



PLAN SURREY 2013

BYLAW NO. 18020
20 OCTOBER 2014

OFFICIAL COMMUNITY PLAN



City of Surrey

Official Community Plan

Bylaw No. 18020

(Adopted October 20, 2014)

Unless otherwise indicated, photographs and sketches in this document are used for reference or illustrative purposes only and can be changed from time to time without requiring an Official Community Plan Amendment.

Copies of this Bylaw may be viewed on our website at www.surrey.ca/ocp or at:

City of Surrey 13450 -104 Avenue Surrey, BC V3T 1V8

Consolidated Amendment Date: **February 10, 2025**

THIS IS A CONSOLIDATED BYLAW PREPARED BY THE CITY OF SURREY FOR CONVENIENCE ONLY.

The City of Surrey does not warrant that the information contained in this consolidation is current. It is the responsibility of the person using this consolidation to ensure that it accurately reflects current bylaw provisions.

A list of amendments adopted by Council can be found on the preceding pages.

CITY OF SURREY

BYLAW No. 18020

A By-law to adopt an Official Community Plan for the City of Surrey

WHEREAS pursuant to the provisions of Section 472 of the *Local Government Act*, being Chapter 1 of the Revised Statutes of British Columbia, as amended from time to time, Council may, by bylaw, adopt a Community Plan as an Official Community Plan;

AND WHEREAS the goals of the Official Community Plan reflect the future vision for Surrey;

AND WHEREAS the City Council deemed it necessary for orderly, sustainable, economical growth and development, to adopt an Official Community Plan;

AND WHEREAS the Official Community Plan was prepared with input from the general public through Open Houses, Public Information Meetings, Stakeholder Workshops, Focus Groups, and On-line and Written Communication;

AND WHEREAS pursuant to the provisions of Section 477 of the said *Local Government Act*, in preparing the Official Community Plan, and after First Reading, City Council has given consideration, in sequence, to its most recent Financial Plan, the *Agricultural Land Commission Act*, Waste Management Plan and Economic Strategy Plan to ensure consistency between them and the Official Community Plan;

AND WHEREAS pursuant to the provisions of Section 475 of the said *Local Government Act*, Council gave notice to adjoining municipalities, prescribed government agencies and Metro Vancouver;

NOW THEREFORE the City Council of the City of Surrey, ENACTS AS FOLLOWS:

1. This Bylaw may be cited for all purposes as the “Surrey Official Community Plan Bylaw, 2013, No. 18020”
2. The “Surrey Official Community Plan By-law, 1996, No. 12900” and all amendments and resolutions related thereto are hereby repealed.
3. Council hereby adopts as its Official Community Plan the document entitled “PlanSurrey 2013: Official Community Plan”, a copy of which is attached hereto as “Schedule A” and made part of this bylaw.


PASSED FIRST READING on the 16th day of December, 2013

PASSED SECOND READING on the 16th day of December, 2013

PUBLIC HEARING HELD thereon the 31st day of March, 2014

PASSED THIRD READING AS AMENDED, on the 20th day of October, 2014.

RECONSIDERED AND FINALLY ADOPTED, signed by the Mayor and Clerk, and sealed with the Corporate Seal on the 20th day of October, 2014.



MAYOR



CLERK

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
December 1, 2014	18204	Map	Figure 3: General Land Use Designations
December 15, 2014	17788	Map	Figure 3: General Land Use Designations
January 12, 2015	18265	Map	Figure 3: General Land Use Designations
February 2, 2015	17868	Map	Figure 3: General Land Use Designations
March 23, 2015	17981	Map	Figure 3: General Land Use Designations
“	18376	Map	Figure 3: General Land Use Designations
“	18272	Map	Figure 3: General Land Use Designations
“	18152	Map	Figure 3: General Land Use Designations
“	18333	Map	Figure 3: General Land Use Designations
April 27, 2015	18423	Text	Tables and Figures
“	“	Text	Land Uses and Densities
“	“	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 5: Suburban Density Exception Areas
“	“	Map	Figure 6—Deleted
“	“	Text	Theme B: B6 General
“	“	Text	Theme C: C2 Walking
“	“	Text	Theme D: D1 General; D4 Energy Supply
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 63: Secondary Plan Areas
“	“	Text	Implementation II (a): Secondary Plans
“	“	Text	Implementation II (d): Development Permits
“	“	Text	Implementation II (e): Development Approval
“	“	Text	Implementation II (f): Master Plans and Strategies
“	“	Text	Implementation III: OCP Monitoring and Review
“	“	Text	Implementation V: Definitions
“	“	Text	DP2: Hazard Lands Development Permit Area
“	“	Text	DP4: Farm Protection Development Permit Area
May 25, 2015	18447	Map	Figure 3: General Land Use Designations
July 13, 2015	18405	Map	Figure 3: General Land Use Designations
July 27, 2015	18267	Map	Figure 3: General Land Use Designations
September 28, 2015	18491	Map	Figure 3: General Land Use Designations
November 2, 2015	18498	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 63: Secondary Plan Areas
November 16, 2015	18373	Map	Figure 3: General Land Use Designations
November 16, 2015	18431	Map	Figure 3: General Land Use Designations
July 25, 2016	18713	Map	Figure 3: General Land Use Designations
July 25, 2016	18496	Map	Figure 3: General Land Use Designations

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
July 25, 2016	18657	Map	Figure 3: General Land Uses and Densities
September 12, 2016	18316	Map	Figure 3: General Land Uses and Densities
September 12, 2016	18784	Text	Implementation II (d): Development Permits
“	“	Text	Implementation II (d): DP3 Sensitive Ecosystems
“	“	Map	Figure 67: Green Infrastructure Development
“	“	Map	Figure 68: Streamside Development Permit Area
“	“	Text	DP3: Sensitive Ecosystems
October 24, 2016	18546	Map	Figure 3: General Land Uses and Densities
December 5, 2016	18312	Map	Figure 3: General Land Uses and Densities
February 6, 2017	18683	Map	Figure 3: General Land Uses and Densities
February 20, 2017	18610	Map	Figure 3: General Land Uses and Densities
March 6, 2017	18468	Map	Figure 3: General Land Uses and Densities
“	18613	Map	Figure 3: General Land Uses and Densities
April 3, 2017	18748	Map	Figure 3: General Land Uses and Densities
“	18651	Map	Figure 3: General Land Uses and Densities
May 8, 2017	18928	Map	Figure 3: General Land Uses and Densities
“	“	Map	Regional Context Statement: Figure 51: Regional Growth Strategy Land Use Designations
May 29, 2017	18787	Text	Tables and Figures
“	“	Text	Introduction
“	“	Map	Figure 3: General Land Uses and Densities
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Text	Urban Designation
“	“	Text	Multiple Residential Designation
“	“	Text	Commercial Designation
“	“	Map	Figure 12: Fleetwood Town Centre Densities
“	“	Text	Add Table 7a: Land Use Designation Exceptions
“	“	Text	Theme A: A2 Accommodating Higher Density
“	“	Text	Theme B: B3 Land Use and Density
“	“	Map	Figure 34: Greenways Network
“	“	Map	Theme D; D2 Hazard Lands
“	“	Map	Figure 38: Steep Slops Hazard Map
“	“	Map	Figure 42: Major Employment Areas
“	“	Text	Theme F: F3 Affordable Housing
“	“	Map	Figure 63: Secondary Plan Areas
“	“	Text	Implementation: DP1
“	“	Text	Implementation: DP2
“	“	Map	Figure 65: Steep Slope Hazard Land
“	“	Text	Implementation: DP3

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
May 29, 2017 (cont.)	18787 (cont.)	Map	Figure 67: Green Infrastructure Development Permit Area
"	"	Text	Implementation: DP4
"	"	Text	DP1: Form and Character Development Permit Area and Guidelines
"	"	Text	DP2: Hazard Lands Development Permit Area and Guidelines
"	"	Text	DP4: Farm Protection Development Permit Area and Guidelines
June 12, 2017	18650	Text	Tables and Figures
"	"	Map	Figure 24: Water Distribution Infrastructure
"	"	Map	Figure 25: Stormwater Infrastructure
"	"	Map	Figure 26: Sanitary Sewer Infrastructure
"	"	Text	Theme C, C1 Energy
"	"	Map	Add Figure 26a: District Energy Systems
June 26, 2017	19003	Map	Figure 3: General Land Use Designations
July 10, 2017	18894	Map	Figure 3: General Land Use Designations
July 24, 2017	19213	Map	Figure 3: General Land Use Designations
September 11, 2017	19206	Map	Figure 3: General Land Use Designations
October 2, 2017	18889	Map	Figure 3: General Land Use Designations
October 2, 2017	18892	Map	Figure 3: General Land Use Designations
October 2, 2017	19243	Map	Figure 3: General Land Use Designations
"	"	Text	Figure 7a: Land Use Designation Exceptions
"	"	Map	Figure 42: Major Employment Areas
October 2, 2017	19064	Map	Figure 3: General Land Use Designations
October 23, 2017	19363	Text	Implementation: DP1: Form and Character
October 23, 2017	19366	Map	Figure 3: General Land Use Designations
October 23, 2017	19126	Map	Figure 3: General Land Use Designations
October 23, 2017	18655	Map	Figure 3: General Land Use Designations
October 23, 2017	18774	Map	Figure 3: General Land Use Designations
November 6, 2017	18885	Map	Figure 3: General Land Use Designations
November 20, 2017	19208	Map	Figure 3: General Land Use Designations
December 4, 2017	19181	Map	Figure 3: General Land Use Designations
January 22, 2018	19155	Map	Figure 3: General Land Use Designations
January 22, 2018	19252	Map	Figure 3: General Land Use Designations
February 19, 2018	18923	Map	Figure 3: General Land Use Designations
March 12, 2018	19062	Map	Figure 3: General Land Use Designations
March 12, 2018	18370	Map	Figure 3: General Land Use Designations
March 12, 2018	19328	Map	Figure 3: General Land Use Designations
March 12, 2018	18276	Map	Figure 3: General Land Use Designations
April 23, 2018	18833	Text	Suburban Designation
April 23, 2018	19038	Map	Figure 3: General Land Use Designations

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
May 7, 2018	15785	Map	Figure 3: General Land Use Designations
May 7, 2018	19519	Text	Theme A: Growth Management
“	“	Text	Theme B: Centres, Corridors and Neighbourhoods
“	“	Map	Figure 22: Frequent Transit Corridors
May 7, 2018	19338	Map	Figure 3: General Land Use Designations
May 28, 2018	19324	Map	Figure 16: Central Business District Densities
June 25, 2018	19008	Map	Figure 3: General Land Use Designations
October 1, 2018	19364	Text	Land Use Designations: Densities
“	“	Text	Multiple Residential Designation
“	“	Map	Figure 3: General Land Use Designations
“	“	Text	Multiple Residential Land Use Designation
“	“	Map	Figure 23: Finer Grained Road Network
“	“	Text	Theme B: B5 Memorable Features, Views and Viewpoints
“	“	Text	Theme B: B5 Memorable Features, Natural Heritage
“	“	Map	Figure 34: Greenways Network
“	“	Map	Figure 36: EMS Green Infrastructure Network (GIN)
“	“	Map	Figure 37: Fish Watercourse Classifications
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 43: Agricultural Lands
“	“	Map	Figure 65: Steep Slope Hazard Lands Development Permit Area
“	“	Map	Figure 67: Green Infrastructure Development Permit Area
“	“	Map	Figure 68: Streamside Development Permit Area
“	“	Map	Figure 69: Farming Protection Area
“	“	Text	DP1: Development Permit Guidelines: Form and Character
“	“	Text	DP1.1: Form and Character, Common Guidelines: All Development Types
“	“	Text	DP1.1: Form and Character, Building Form, Building Massing
“	“	Text	DP3: Sensitive Ecosystems, C. Development Guidelines, Streamside
“	“	Text	DP3: Sensitive Ecosystems, D. Submission Requirements—Ecosystem Development Plan
“	“	Text	DP3: Sensitive Ecosystems, E. Submission Requirements—Impact Mitigation Plan

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
October 1, 2018	19364	Text	DP4: Farming Protection, C. Setbacks and Buffers
October 1, 2018	19627	Map	Figure 16: Central Business District Densities
October 1, 2018	19383	Map	Fig 3: General Land Use Designations
October 1, 2018	19372	Map	Figure 16: Central Business District Densities
October 1, 2018	19611	Map	Figure 16: Central Business District Densities
December 3, 2018	18716	Map	Figure 3: General Land Use Designations
December 3, 2018	19067	Map	Figure 3: General Land Use Designations
December 3, 2018	18717	Map	Figure 3: General Land Use Designations
December 3, 2018	19568	Map	Figure 3: General Land Use Designations
January 14, 2019	19547	Map	Figure 3: General Land Use Designations
January 14, 2019	19578	Map	Figure 3: General Land Use Designations
February 11, 2019	19298	Map	Figure 16: Central Business District Densities
February 25, 2019	19765	Text	DP1: Development Permit Guidelines: Form and Character
March 11, 2019	19296	Map	Figure 3: General Land Use Designations
March 11, 2019	"	Map	Figure 42: Major Employment Areas
April 1, 2019	19481	Map	Figure 3: General Land Use Designations
April 15, 2019	19645	Map	Figure 3: General Land Use Designations
April 29, 2019	19801	Map	Figure 9: Centres and Rapid Transit Areas and Corridors
"	"	Text	Theme A: A1 Growth Priorities
"	"	Text	Theme A: A2 Accommodate Higher Density
"	"	Map	Figure 18: Centres and Frequent Transit Areas and Corridors
"	"	Text	Theme B: B3 Transit Corridors
"	"	Map	Figure 21: Frequent Transit Development Types
"	"	Map	Figure 22: Frequent Transit Networks
"	"	Text	Theme C: C2 Transportation
"	"	Map	Figure 28: Frequent Transit Networks
April 29, 2019	19483	Map	Figure 3: General Land Use Designations
April 29, 2019	19541	Map	Figure 3: General Land Use Designations
July 22, 2019	19787	Map	Figure 3: General Land Use Designations
November 6, 2019	19650	Map	Figure 3: General Land Use Designations
November 18, 2019	19946	Text	DP1: Development Permit Guidelines: Form and Character
December 2, 2019	19487	Map	Table 7A: Land Use Designation Exceptions
December 16, 2019	19996	Map	Figure 3: General Land Use Designations
"	"	Map	Figure 4: Secondary Plan Areas
"	"	Map	Figure 11: Cloverdale Town Centre Densities
"	"	Map	Figure 42: Major Employment Areas
"	"	Map	Figure 63: Secondary Plan Areas

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
January 13, 2020	19509	Map	Figure 3: General Land Use Designations
March 9, 2020	19747	Map	Figure 3: General Land Use Designations
March 9, 2020	19633	Map	Figure 3: General Land Use Designations
March 9, 2020	19887	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 42: Major Employment Areas
April 6, 2020	19886	Text	Table 7A: Land Use Designation Exceptions
April 6, 2020	20026	Text	Theme D: D4Energy, Emissions and Climate Resiliency
“	“	Text	Implementation, V. Definitions
May 4, 2020	20008	Text	Urban Land Use Designation
“	“	Text	Multiple Residential Land Use Designation
“	“	Map	Figure 42: Major Employment Areas
“	“	Text	DP1: Development Permit Guidelines: Form and Character
“	“	Text	DP3: Development Permit Guidelines: Sensitive Ecosystems
June 15, 2020	19672	Map	Figure 3: General Land Use Designations
July 13, 2020	20118	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 14: Newton Town Centre Densities
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 63: Secondary Plan Areas
July 27, 2020	20109	Text	Land Uses and Densities
September 14, 2020	19863	Map	Figure 3: General Land Use Designations
“	“	Text	Figure 7A: Land Use Designation Exceptions
October 19, 2020	20062	Map	Figure 3: Land Use Designation Exceptions
October 19, 2020	20166	Map	Figure 3: Land Use Designation Exceptions
October 19, 2020	19692	Map	Figure 3: Land Use Designation Exceptions
November 23, 2020	19870	Map	Figure 3: Land Use Designation Exceptions
November 23, 2020	20006	Map	Figure 16: Central Business District Densities
December 7, 2020	20137	Map	Figure 3: Land Use Designation Exceptions
December 21, 2020	19751	Map	Figure 3: General Land Use Designations
“	“	Text	Table 7A: Land Use Designation Exceptions

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ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
December 21, 2020	20231	Text	Theme A: A3 Sensitive Infill
"	"	Text	Theme F: F3 Affordable Housing
January 11, 2021	19827	Map	Figure 16: Central Business District Densities
February 22, 2021	20277	Map	Figure 16: Central Business District Densities
February 22, 2021	20133	Map	Figure 3: General Land Use Designations
April 12, 2021	20046	Map	Figure 3: General Land Use Designations
May 10, 2021	17597	Map	Figure 3: General Land Use Designations
May 10, 2021	19590	Map	Figure 3: General Land Use Designations
May 10, 2021	19799	Map	Figure 3: General Land Use Designations
May 10, 2021	19844	Map	Figure 3: General Land Use Designations
May 31, 2021	20340	Map	Figure 3: General Land Use Designations
"	"	Map	Figure 4: Secondary Plan Areas
"	"	Map	Figure 42: Major Employment Areas
"	"	Map	Figure 63: Secondary Plan Areas
June 14, 2021	19636	Map	Figure 3: General Land Use Designations
June 28, 2021	20113	Map	Figure 16: Central Business District Densities
June 28, 2021	20292	Text	Commercial Designation
"	"	Text	Urban Designation
"	"	Map	Figure 4: Secondary Plans
"	"	Text	DP 1.1
July 12, 2021	19922	Map	Figure 3: General Land Use Designations
July 12, 2021	20254	Map	Figure 3: General Land Use Designations
July 12, 2021	20301	Map	Figure 3: General Land Use Designations
July 26, 2021	20237	Map	Figure 3: General Land Use Designations
July 29, 2021	20150	Map	Figure 3: General Land Use Designations
September 27, 2021	20363	Map	Figure 3: General Land Use Designations
October 18, 2021	20104	Map	Figure 16: Central Business District Densities
October 18, 2021	20192	Map	Figure 16: Central Business District Densities
November 8, 2021	20009	Map	Figure 16: Central Business District Densities
November 22, 2021	19867	Map	Figure 3: General Land Use Designations
December 6, 2021	19391	Map	Figure 3: General Land Use Designations
December 6, 2021	19938	Map	Figure 3: General Land Use Designations
December 20, 2021	20160	Map	Figure 16: Central Business District Densities

BYLAW AMENDMENTS

ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
January 17, 2022	20086	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 3: General Land Use Designations
March 7, 2022	20141	Map	Figure 16: Central Business District Densities
March 7, 2022	20393	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 51: Regional Growth Strategy Land Use
“	“	Map	Figure 63: Secondary Plan Areas
“	“	Map	Figure 43: Agricultural Lands
March 7, 2022	20554	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 15: Semiahmoo Town Centre
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 63: Secondary Plan Areas
March 28, 2022	20170	Map	Figure 3: General Land Use Designations
March 28, 2022	20190	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 42: Major Employment Areas
July 25, 2022	20658	Text	Central Business District Designation
“	“	Map	Figure 3: General Land Use Designations
“	“	Map	Figure 4: Secondary Plan Areas
“	“	Map	Figure 16: Central Business District Densities
“	“	Map	Figure 42: Major Employment Areas
“	“	Map	Figure 63: Secondary Plan Areas
“	“	Text	Multiple Residential Designation
“	“	Text	Commercial Designation
“	“	Text	Mixed Employment Designation
“	20368	Map	Figure 3: General Land Use Designations
“	20649	Map	Figure 16: Central Business District Densities
October 3, 2022	20565	Text	Table 7A: Land Use Designation Exceptions
	20245	Map	Figure 3: General Land Use Designations
	20594	Map	Figure 3: General Land Use Designations
	20611	Map	Figure 3: General Land Use Designations
	20044	Map	Figure 3: General Land Use Designations

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ADOPTION DATE	BYLAW NO.	TYPE	DETAILS
November 14, 2022	20266	Map	Figure 3: General Land Use Designations
December 12, 2022	19608	Map	Figure 3: General Land Use Designations
January 16, 2023	19932	Map	Figure 3: General Land Use Designations
February 13, 2023	20382	Map	Figure 3: General Land use Designations
March 6, 2023	19842	Map	Figure 3: General Land Use Designations
April 17, 2023	20678	Map	Figure 3: General Land Use Designations
May 1, 2023	20297	Map	Figure 3: General Land Use Designations
May 15, 2023	20935	Map	Figure 3: General Land Use Designations
	"	Map	Figure 4: Secondary Plan Areas
	"	Map	Figure 42: Major Employment Areas
	"	Map	Figure 63: Secondary Plan Areas
June 5, 2023	20380	Map	Figure 3: General Land Use Designations
June 19, 2023	20661	Map	Figure 3: General Land Use Designations
	20454	Map	Figure 3: General Land Use Designations
	20451	Map	Figure 3: General Land Use Designations
July 10, 2023	20604	Map	Figure 3: General Land Use Designations
July 24, 2023	20371	Map	Figure 3: General Land Use Designations
	20725	Text	Table 7A: Land Use Designation Exceptions
October 16, 2023	20544	Map	Figure 3: General Land use Designations
October 30, 2023	20705	Map	Figure 3: General Land use Designations
	"	Map	Figure 16: General Land use Designations
	"	Map	Figure 42: General Land use Designations
November 20, 2023	20695	Map	Figure 3: General Land use Designations
	"	Text	Table 7A: Land Use Designation Exception
	21086	Map	Figure 3: General Land use Designations
	"	Map	Figure 4: General Land use Designations
	"	Map	Figure 13: General Land use Designations
	"	Map	Figure 42: General Land use Designations
	"	Map	Figure 63: General Land use Designations
December 4, 2023	21104	Map	Figure 4: General Land use Designations
	"	Map	Figure 5: General Land use Designations
	"	Map	Figure 9: General Land use Designations
	"	Map	Figure 63: General Land use Designations
	"	Map	Figure 67: General Land use Designations
December 18, 2023	20651	Map	Figure 16: General use Designations
	20970	Map	Figure 3: General use Designations
January 29, 2024	20966	Map	Figure 15: Semiahmoo Town Centre
	"	Text	Table 7A: Land Use Designation Exceptions
February 26, 2024	20616	Map	Figure 3: General Land use Designations
	"	Map	Figure 5: General Land use Designations








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April 8, 2024	20547	Map	Figure 3: General Land use Designations
	"	Text	Table 7A: Land Use Designation Exceptions
	21183	Map	Figure 4: General Land use Designations
	"	Map	Figure 63: General Land use Designations
April 22, 2024	20958	Text	Table 7A: Land Use Designation Exceptions
	21215	Map	Figure 3: General Land use Designations
May 6, 2024	20404	Map	Figure 3: General Land use Designations
	"	Map	Figure 42: General Land use Designations
June 10, 2024	19701	Map	Figure 3: General Land use Designations
	21275	Text	Table 7A: Land Use Designation Exceptions
June 24, 2024	20843	Text	Table 7A: Land Use Designation Exceptions
July 8, 2024	20941	Map	Figure 3: General Land use Designations
July 22, 2024	19674	Map	Figure 3: General Land use Designations
September 23, 2024	20992	Map	Figure 3: General Land use Designations
	21389	Text	Implementation
	"	Text	DPI Form and Character
November 4, 2024	20923	Text	Table 7A: Land Use Designation Exceptions
January 27, 2025	20833	Map	Figure 3: General Land use Designations
	"	Map	Figure 42: Major Employment Areas
February 10, 2025	21533	Text	Downtown: Land Uses and Densities
	"	Map	Figure 16: Downtown Densities
	21206	Map	Figure 3: General Land use Designations
	20894	Map	Figure 3: General Land use Designations
	"	Text	Table 7A: Land Use Designation Exceptions
	21096	Map	Figure 3: General Land use Designations
		Text	Table 7A: Land Use Designation Exceptions

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INTRODUCTION



ROLE OF THE OFFICIAL COMMUNITY PLAN

Introduction

An Official Community Plan (OCP) is comprised of objectives and policies that set out a local government's long-term plan for community development. In British Columbia, authority for adopting an OCP is granted through the *Local Government Act* (LGA). Part 26 of the LGA provides the framework for preparing and adopting an OCP and outlines both required and optional content.

The LGA (Section 473) requires an OCP to show the approximate location and extent of future residential, commercial, industrial, agricultural and institutional land uses. It also requires illustrating where sand and gravel resources are located, where transportation, sewer and water infrastructure exists and is intended to be installed, where schools and public facilities are planned and where there are restrictions on the use of land that is environmentally sensitive and/or subject to hazardous conditions. An OCP must also contain policy statements on the provision of affordable and rental housing, housing for persons with special needs and targets and policies relating to the reduction of community-wide greenhouse gas (GHG) emissions. Lastly, a Regional Context Statement, indicating how OCP policies are consistent with a region's Regional Growth Strategy (RGS), is a required element of an Official Community Plan.

The LGA enables governments to include policies related to a wide range of issues including: transportation, agriculture, the natural environment, parks and recreation, social services and social development. The LGA also stipulates that a local government is not obligated to implement or enact the content contained in an OCP, however, by-laws or policies adopted after an OCP is completed must be consistent with it, otherwise the OCP must be amended.

PlanSurrey 2013 is established by by-law as the Official Community Plan for Surrey. This OCP has been prepared and adopted in accordance with the LGA, including requirements regarding public and agency consultation.

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The Sustainability Charter

In 2008, the City of Surrey endorsed the first *Sustainability Charter* as a guiding document intended to direct the corporate operations of the City as well as the evolution of the community towards sustainability. In 2016, an updated *Sustainability Charter 2.0* was endorsed by Council to continue to function as an overarching policy document focusing on a vision for Surrey that articulates a thriving, green, inclusive city, organized around eight community themes for a more holistic way of looking at sustainability and the interconnectedness of community systems.

The plans and policies contained in this Official Community Plan are set within the overall framework of the *Sustainability Charter* and are consistent with its principles and goals. The OCP sets out an overall vision of a sustainable future for Surrey over the next 30 years in terms of urban development, the provision of civic facilities and amenities, transportation and infrastructure, environmental stewardship, economic development, social and cultural development.

Figure 1: Sustainability Charter Structure



Goals within Surrey's Sustainability Charter are categorized according to eight overlapping community themes. Each theme has an overarching goal, followed by the Desired Outcomes the community wants to see looking ahead to the year 2058.

Official Community Plan Layout

The OCP is organized into the following sections:

Planning Context

sets out the major planning challenges that provide the context for the plans and policies contained in the OCP.

Vision

describes the vision of the City's future and guides the OCP's specific plans and policies.

Land Uses and Densities

shows the location of and describes the intent of the various OCP land use designations, along with the general densities and development considerations associated with them.

Themes, Objectives and Policies

contains planning objectives and policies on a range of community development subjects, organized around six themes. The first three themes relate to the physical structure of the City: Growth Management, Centres, Corridors and Neighbourhoods and Infrastructure and Facilities. The last three themes relate to the sustainable development and management of Ecosystems, Economy and Society and Culture.

Regional Context Statement

contains the Regional Context Statement which describes the relationship between the OCP and the Metro Vancouver *Regional Growth Strategy*, as required by the *Local Government Act*.

Implementation

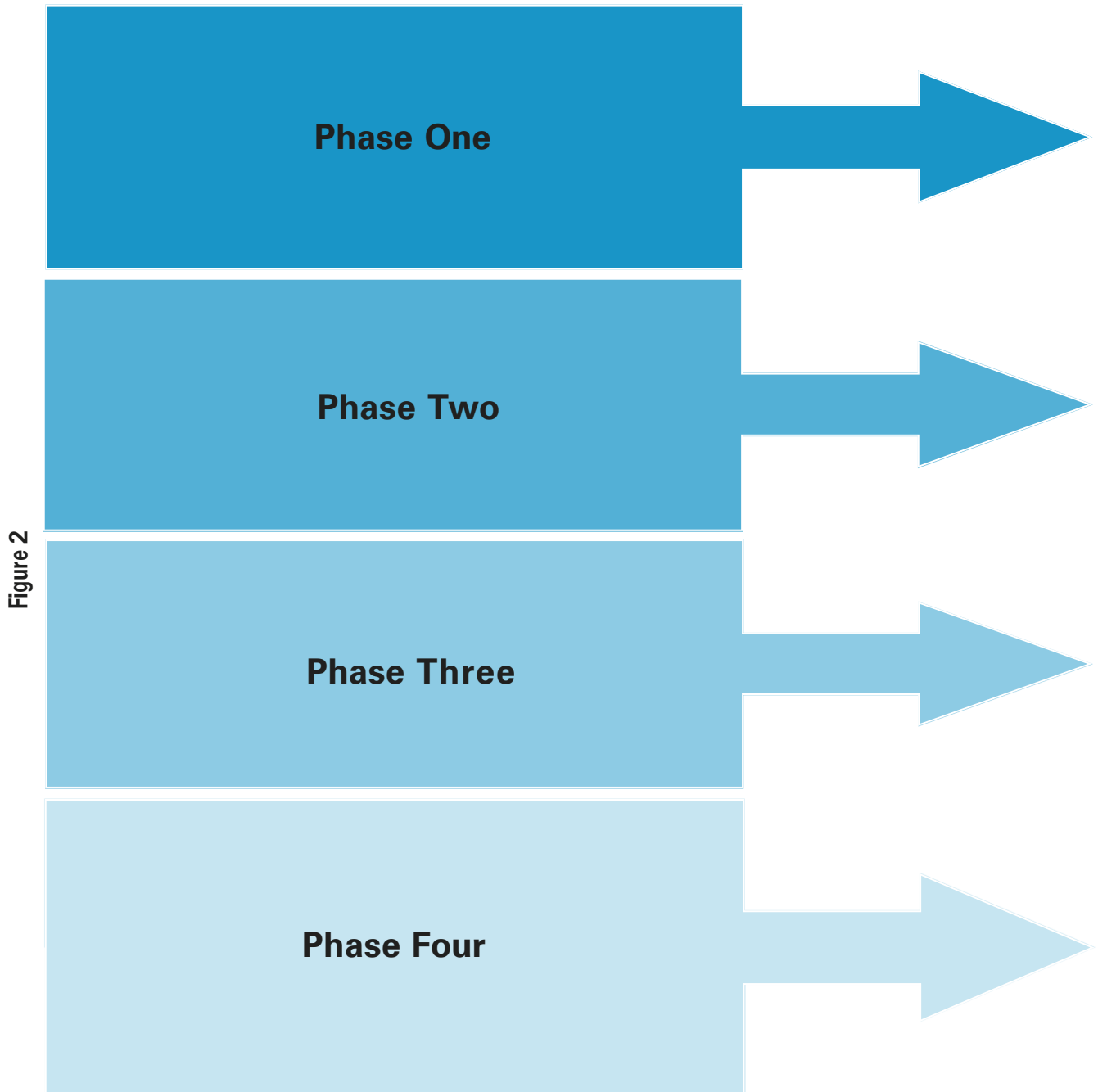
indicates how the objectives of the OCP are implemented, including the relationship of the OCP to other City plans and strategies; the definition, justification and requirements related to Development Permits; the authorization of Temporary Use Permits; and the monitoring of how well the OCP is directing the future growth and evolution of Surrey.



Surrey vistas.

PlanSurrey 2013: OCP Process

PlanSurrey 2013: The Official Community Plan was prepared through a process of community consultations including direction provided by an extensive list of City advisory committees, community associations and stakeholder groups. Specialized technical assistance was also provided by internal City staff on as as-needed basis throughout the entirety of the process. The chronology of the PlanSurrey 2013 process is outlined in Figure 2 below:



In accordance with *Local Government Act* Section 477 PlanSurrey 2013: Official Community Plan, has also been prepared in conjunction with the following documents:

- ⇒ City of Surrey Financial Plan
- ⇒ Metro Vancouver Integrated Liquid Waste Resource Management Plan, and
- ⇒ Metro Vancouver Integrated Solid Waste and Resource Management Plan.

BACKGROUND RESEARCH AND ISSUES ANALYSIS

- Consulted with City Advisory Committees
- Consulted with Key Community Stakeholders
- Consulted with all City Departments
- Held Community Open Houses in North, Central and South Surrey to review OCP vision and issues
- Reported to Council on findings

ALTERNATIVES IDENTIFICATION AND EVALUATION

- City Council workshop/City staff workshop
- Met with City Advisory Committees
- Hosted an Intergovernmental Agency Meeting
- Hosted Technical Working Group Meetings
- Updated Report to Council

PLAN DRAFTED

- Draft Plan Prepared
- Realigned Existing OCP into new format
- Maps upgraded/Development Permit Areas overhauled
- Circulated draft OCP to Process Participants for Review and Comment
- Met with Stakeholders and City Advisory Committees

FINALIZE THE PLAN

- Held City-wide Public Open House to Review OCP Draft
- Sent Final Update Report to Council
- Held Public Hearing and By-law Readings
- Received Metro Vancouver Approval of Regional Context Statement
- Received Final Adoption, Finalized All Edits, Printed and Circulated Final Document





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PLANNING CONTEXT

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TRANS LINK

PLANNING CHALLENGES

PlanSurrey 2013: The Official Community Plan is intended to guide the development of the City of Surrey over the next 30 years. As such, the Plan responds to both current conditions and to emerging trends. Together, these form the context for planning policies and for evaluating the success of these policies.

In considering current conditions and looking forward to emerging trends and projections, several major challenges have been identified and are addressed in this Official Community Plan.

CHALLENGE #1:

Accommodate Continued Population Growth

Surrey, along with the Metro Vancouver region as a whole, is an attractive place to live. Projections contained in the Metro Vancouver *Regional Growth Strategy* are for over 1 million additional residents moving into the region by the year 2040. This growth is driven by inter-provincial and international migration and stable rates of natural generational replacement. Surrey is expected to receive a substantial portion of this growth due to a relatively affordable cost of housing and a somewhat abundant land base that can accommodate urban development. It is expected that Surrey’s population will continue to increase by approximately 2% per year over the next three decades resulting in a population increase of approximately 267,475 by the year 2041, for a total population projection of 770,200 (see Table 1). This growth will need to be allocated in ways that maximize the efficient use of urban land while minimizing the impacts of change in existing neighbourhoods.

Table 1

Surrey Projected Population Growth and Dwelling Units		
<u>Year</u>	<u>Population</u>	<u>Dwelling Units</u>
2012	502,725	164,935
2017	553,475	184,385
2021	593,600	199,950
2026	639,600	219,450
2031	685,250	238,800
2036	727,750	256,800
2041	770,200	274,900

SOURCE: City of Surrey and Metro Vancouver
NOTE: Dwelling Units refer to occupied units.

CHALLENGE #2:

Match Population Growth with Business Development and Employment Opportunities

Key to supporting a sustainable community is developing a robust and diverse local economy which provides a wide range of jobs close to where people live. Over the past 50 years, as Surrey has transitioned from a rural economy to a suburban “bedroom” community, and then to a more integrated urban community, the balance of local jobs to the size of the labour force has also changed. Currently Surrey has approximately 0.70 jobs for every member of the labour force. A key planning challenge is to move this ratio closer to a balance of 1:1 which becomes more imperative in the context of strong population increases. The benefits of a better balance of local jobs to population include reduced time spent commuting, reduced traffic congestion and greenhouse gas emissions, a more resilient economy, and a diversified tax base reducing the burden of property taxes on residential properties. The goal of the OCP is to accommodate an additional 125,400 jobs over the 30-year planning horizon (see Table 2).

Table 2

Surrey Projected Employment Growth	
<u>Year</u>	<u>Jobs</u>
2012	171,200
2017	195,200
2021	214,200
2026	236,200
2031	257,950
2036	277,450
2041	296,600

SOURCE: City of Surrey and Metro Vancouver

NOTE: Dwelling Unit s refer to occupied units.

CHALLENGE #3:

Reduce Reliance on the Private Automobile

Much of Surrey’s “first wave” of urban growth occurred in the 1970s to 1990s, a period of high automobile use that influenced how communities and neighbourhoods were designed and laid out. Higher fuel prices, increased traffic congestion and an increased understanding of the need to reduce greenhouse gas emissions, as well as the connection between active transportation (walking and cycling) and healthier communities, are now all compelling reasons to reduce people’s reliance on cars for everyday transportation (see Tables 3 and 4). This shift requires decisive and long-term efforts to reorient land use patterns, increase mixed-use development, plan higher density developments in conjunction with frequent public transit, influence individual behavior and invest in cycling and walking infrastructure that allows for convenient alternatives to the automobile.

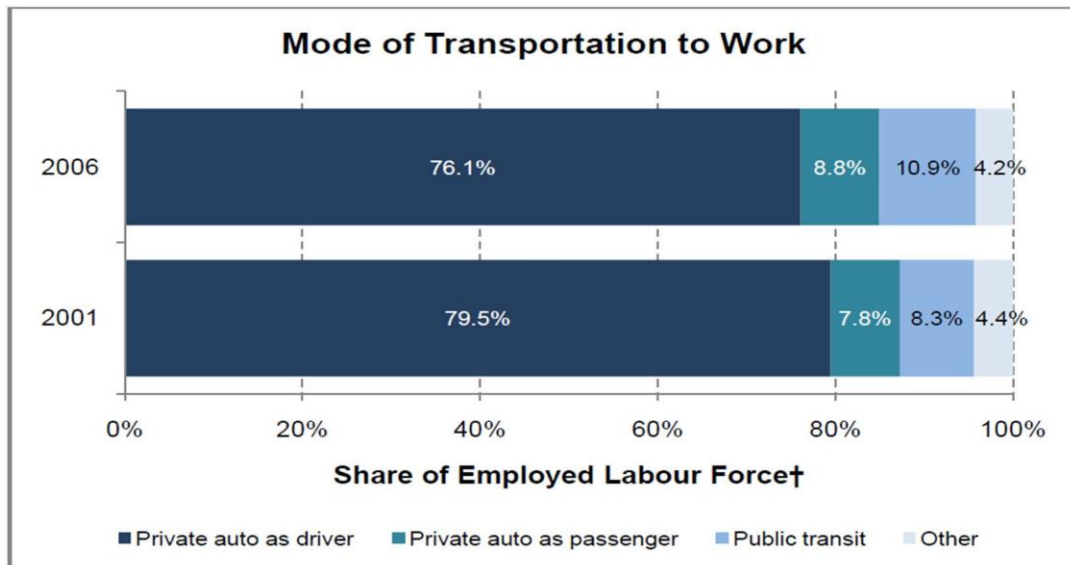
Vehicle-KM Travelled Per Capita (VKT)

Table 3

	2007*	2010*	2040**
Daily VKT	19.16	19.05	18.2
Annual VKT	6,994	6,949	6,643

* 2007, 2010 Data Source: BC Ministry of Environment CEEI Report
 ** 2040 Projection: HBLG Analysis; Based on “Business as Usual” using current plans for projections

Table 4



† Does not include those working at home or working outside Canada.
 SOURCE: Census Canada

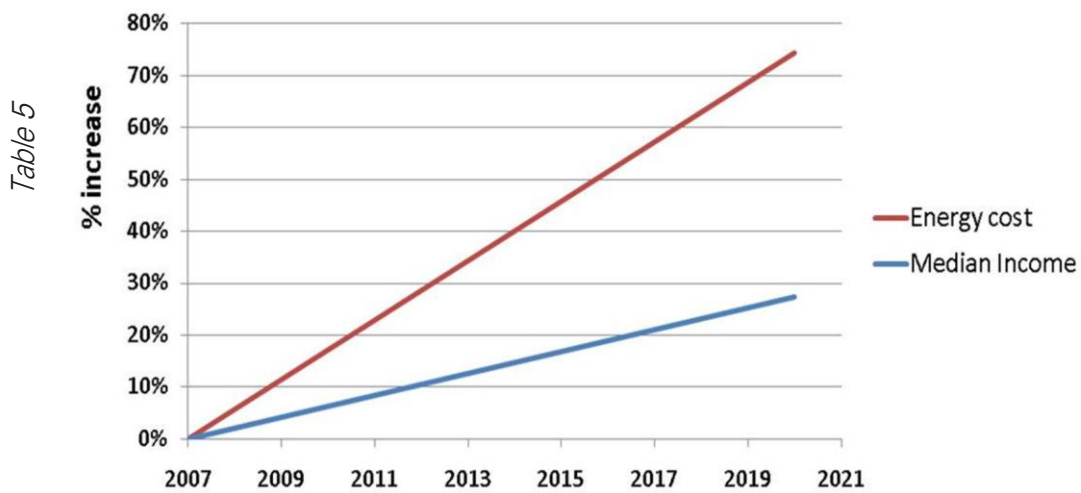
CHALLENGE #4:

Ensure a Resilient City in Response to Rising Energy Costs and A Changing Climate

While the future is uncertain, current trends point to a changing climate characterized by more intense winter precipitation events, lower snow packs in the mountain watersheds that feed the Region and the City’s water system, and longer periods of summer drought. Sea levels are forecast to rise, putting pressure on the dykes and drainage systems protecting Surrey’s floodplains which contain critical infrastructure and a majority of the City’s productive farmland. It is likely that energy costs, including fuel for transportation, as well as gas and electricity for appliances and building heating and cooling, will increase in price, taking up a larger share of household, business and institutional operating budgets (see Table 5).

The OCP addresses resilience to these likely changes by emphasizing a compact urban form which reduces the required extension of infrastructure systems, improves the financial viability of transit and clean, renewable district energy infrastructure, reduces the amount of vehicular travel per capita and avoids development in vulnerable floodplain and escarpment areas.

Projected % Increase in Energy Spending & Family Income in Surrey

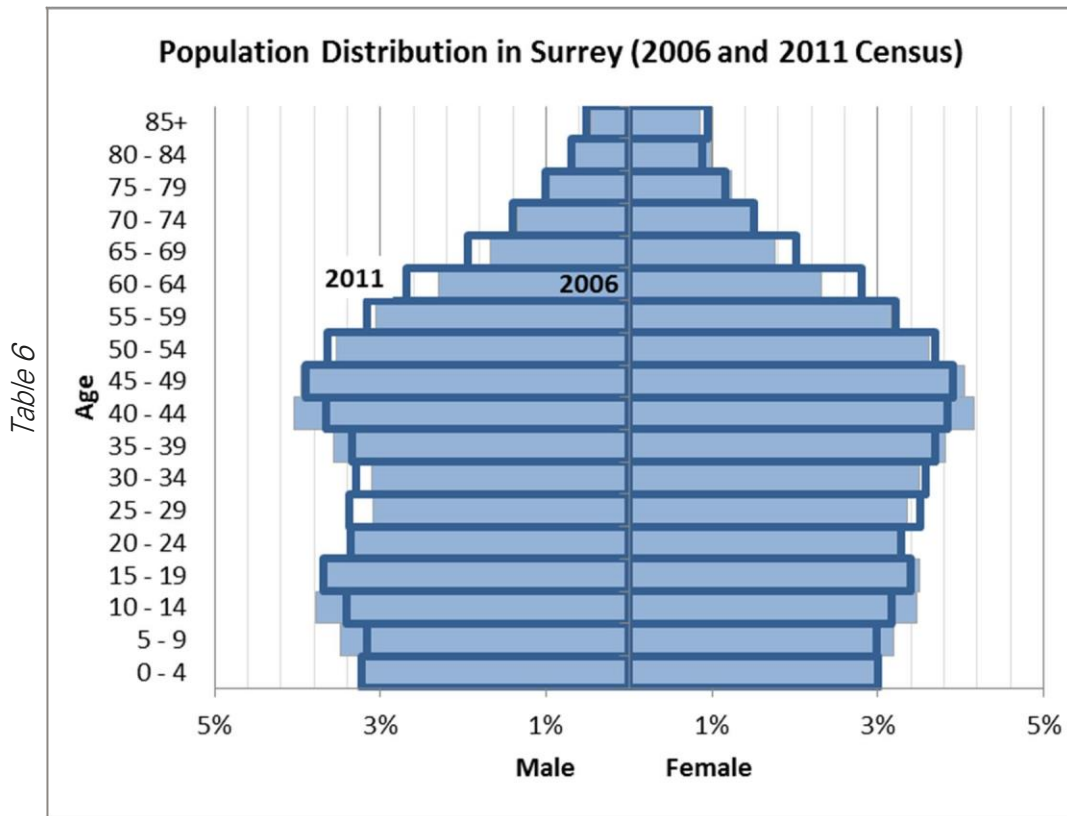


SOURCE: Census Canada

CHALLENGE #5:

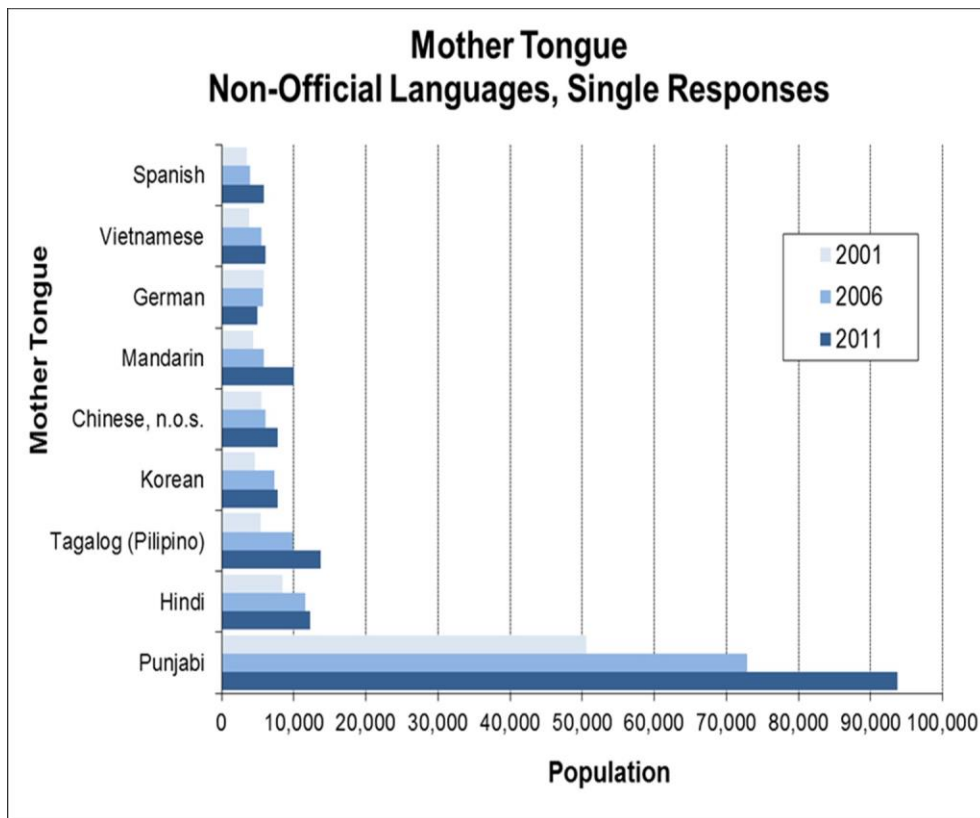
Serve the Increasingly Diverse Needs of Surrey’s Population

Surrey’s urban population is increasingly diverse (see Table 7) and includes groups of people who have unique needs. A growing population of seniors requires new forms of housing or adaptations to existing housing in order to allow them to remain in their neighbourhoods. New Canadians need access to community services in languages they understand and need support as they integrate into Canadian culture and into unfamiliar communities and institutions. As the City grows, additional services helping persons who are homeless or with mental illness or addiction issues are needed. Ensuring that Surrey’s large population of children and youth (see Tables 6) are prepared for and able to engage in learning and healthy lifestyles remains a challenge, particularly among more vulnerable populations including immigrant and refugee families and families living in poverty.



SOURCE: Census Canada

Table 7



SOURCE: Census Canada





VISION



VISION: SURREY 2041

In responding to the Planning Challenges previously identified, Surrey's OCP is guided by a vision for a sustainable city.

This vision includes nine dimensions that, taken together, describe a city that embodies the goal of the *Sustainability Charter*; that is, to *"meet the needs of the present generation in terms of socio-cultural systems, the economy and the environment, while promoting a high quality of life but without compromising the ability of future generations to meet their own needs."*

By implementing the policies and plans in this Official Community Plan:

"The City of Surrey will continually become a **greener**, more **complete**, more **compact** and **connected** community that is **resilient, safer, inclusive, healthier** and more **beautiful**."

These nine "Building Blocks" of the Vision inform the policies within this OCP and serve as the guide by which the effectiveness of the OCP will be measured and monitored.

Surrey 2041 Vision Elements

For the purposes of this Official Community Plan, these nine Vision “ Building Blocks ” include the following considerations:

A **GREENER** Surrey includes:



- A connected network of protected natural ecosystems comprised of urban forests, riparian areas and wetlands, foreshore areas, grasslands and protected farmlands
- Green buildings that reduce energy use, reduce urban stormwater runoff and recycle wastewater
- Green neighbourhoods with a thriving, healthy tree canopy, low-impact rainwater management systems that protect fish habitat, and safe, convenient greenway networks of cycling and walking routes
- Programs and facilities which reduce, reuse and recycle waste
- Integrated and efficient land use and transportation networks that reduce greenhouse gas emissions and air pollution.

A more **COMPLETE** Surrey includes:



- A diversified and balanced economy attracting new and expanded business investment in growing sectors of the economy, adding high-quality employment at a rate that meets or exceeds population growth
- Growing and thriving creative cultural and entertainment industries providing local opportunities for Surrey’s residents and visitors and positioning Surrey as a tourism destination for cultural and entertainment experiences
- A diversified housing stock that meets the changing needs of families and individuals at all levels of income
- A growing post-secondary education sector that provides advanced skills training, research and life-long learning opportunities and that supports economic and socio-cultural development
- A network of community gathering places and centres for building community connections and spaces for celebrations.

A more **COMPACT** Surrey includes:



- A thriving City Centre and Town Centres with a mix of uses and sufficient density of jobs and residences to support public transit and a vibrant urban environment
- An efficient use of urban land so that green spaces are protected and development pressures on agricultural lands are reduced.

A more **CONNECTED** Surrey includes:



- Convenient access for residents to jobs, community amenities and services, close to where they live
- An efficient and connected network of streets, walkways, bike routes and public transit, such as Light Rail Transit, that provides safe, efficient alternative transportation choices throughout the city
- Extensive and connected natural areas, green spaces and farmland, that are defined and protected by clear and stable urban development boundaries.

Surrey 2041 Vision Elements (cont.)

A more **RESILIENT** Surrey includes:



- Community-wide preparation for expected future challenges including the effects of climate change, higher prices for energy and food, and increasing costs to renew public infrastructure
- Sufficient fiscal reserves and human resources to respond effectively to unpredictable future events or emergencies.

A **SAFER** Surrey includes:



- Safe and active neighbourhoods and public spaces that are also perceived as being safe by children, seniors, women and person with disabilities
- Streets, greenways and multi-use pathways that support traffic safety and that reduce accidents and injuries
- Emergency response and emergency management plans that support a safe and protected community.

A more **INCLUSIVE** Surrey includes:



- Community-building opportunities including programs and places that foster neighbourhood connections
- Celebrating Surrey's cultural diversity and growing inter-cultural and inter-generational appreciation
- Access to clean, safe, appropriate and affordable housing for all
- Open access to health, education, social and integration services for everyone including people with lower incomes or with special needs.

A **HEALTHIER** Surrey includes:



- Universal access to healthy local food and access to healthy food locally
- A land use mix and a network of safe cycling and walking routes that support everyday active lifestyles
- Clean air, water and soil that support human health as well as healthy and diverse vegetation and wildlife communities in Surrey.

A more **BEAUTIFUL** Surrey includes:



- Consistent excellence in the architecture and urban design of public and private buildings, parks, plazas and streets
- The protection of natural and cultural heritage elements including significant trees and landscapes and heritage sites and buildings
- Civic beautification initiatives developed in collaboration with businesses, community groups and neighbourhoods
- Abundant, diverse and prominent public art installations and place-making elements that highlight the distinctive character of Surrey's Town Centres and neighbourhoods.



A photograph of a modern, multi-story residential building with a mix of grey and white siding and large windows. In the foreground, there is a wooden pergola structure. The scene is set outdoors with greenery and a clear blue sky. A semi-transparent blue banner is overlaid across the top portion of the image, containing the text 'LAND USES and DENSITIES' in white, bold, sans-serif font.

LAND USES and DENSITIES

BL 19364

Land Use designations defined in this Section and depicted on the General Land Use Designations map (Figure 3) are intended to reflect the goals and objectives of the Official Community Plan and should be read in conjunction with this document's policies and guidelines. Combining the information in this section with the policies contained throughout the Plan provides a balanced reflection of where, when and how development should proceed throughout the city.

This Section outlines the intended character, generalized land uses and expected development densities within each Land Use Designation. Implementation of the information provided here is intended to take place over time through the rezoning of land (in accordance with Surrey's *Zoning By-law*) and through construction (in accordance with other City bylaws and permit processes). Zoning regulations specify permitted land uses and densities on a property-by-property basis and are intended to be generally consistent with the provisions outlined in this Section and with City Centre, Town Centre and Frequent Transit Development Areas as shown on Figure 9.

Densities

Densities within this Section are to be used as a general guide and determined as follows:

- 1) In selected areas, including Urban Centres and Frequent Transit Development Areas (see Figure 9), and where specifically noted in approved Secondary Plans, densities expressed as Floor Area Ratio (FAR) are calculated on a gross site basis, before dedications for roadways or other public purposes are accounted for, and unless otherwise noted. For the purposes of density calculations, undevelopable areas such as riparian areas, parks, or utility corridors, etc., are not included in the gross site area. and
- 2) In all other circumstances, densities expressed as either FAR or Units Per Hectare (Units Per Acre) are calculated on a net site basis, after dedications for roadways or other public purposes are accounted for, unless otherwise noted.

Secondary Plans

Secondary Plans have been initiated and approved by Council for various parts of the city, as shown on Figure 4, Secondary Plan Areas. Secondary Plans include Local Area Plans (primarily for employment areas); Neighbourhood Concept Plans and Infill Area Plans, (primarily for residential neighbourhoods); Town Centre Plans (specific for mixed-use areas found within Surrey's identified Town Centres) and the *Surrey City Centre Plan*. Secondary Plans provide more detailed and specific land use and density plans and set out development requirements (such as amenity fees) that are provided in exchange for achieving a Plan's densities. As Secondary Plans are approved by City Council, the more general *Official Community Plan* Land Use Designations are amended, as needed, to be consistent with the more detailed Secondary Plan. OCP Designation boundaries typically follow parcel boundaries; however, where there is a discrepancy, OCP designation boundaries shall be considered approximate.

Areas outside of Secondary Plans that are scheduled for future urban development are shown on Figure 7, Future Development Areas. These areas are intended to remain as Suburban until such time as Secondary Plans are initiated and approved by Council.

Transit-Oriented Areas

Transit-Oriented Areas (TOAs) have been designated to comply with Provincial requirements. Sites within TOAs are subject to the provisions outlined on pages 60.1 through 60.19.

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Figure 3: General Land Use Designations

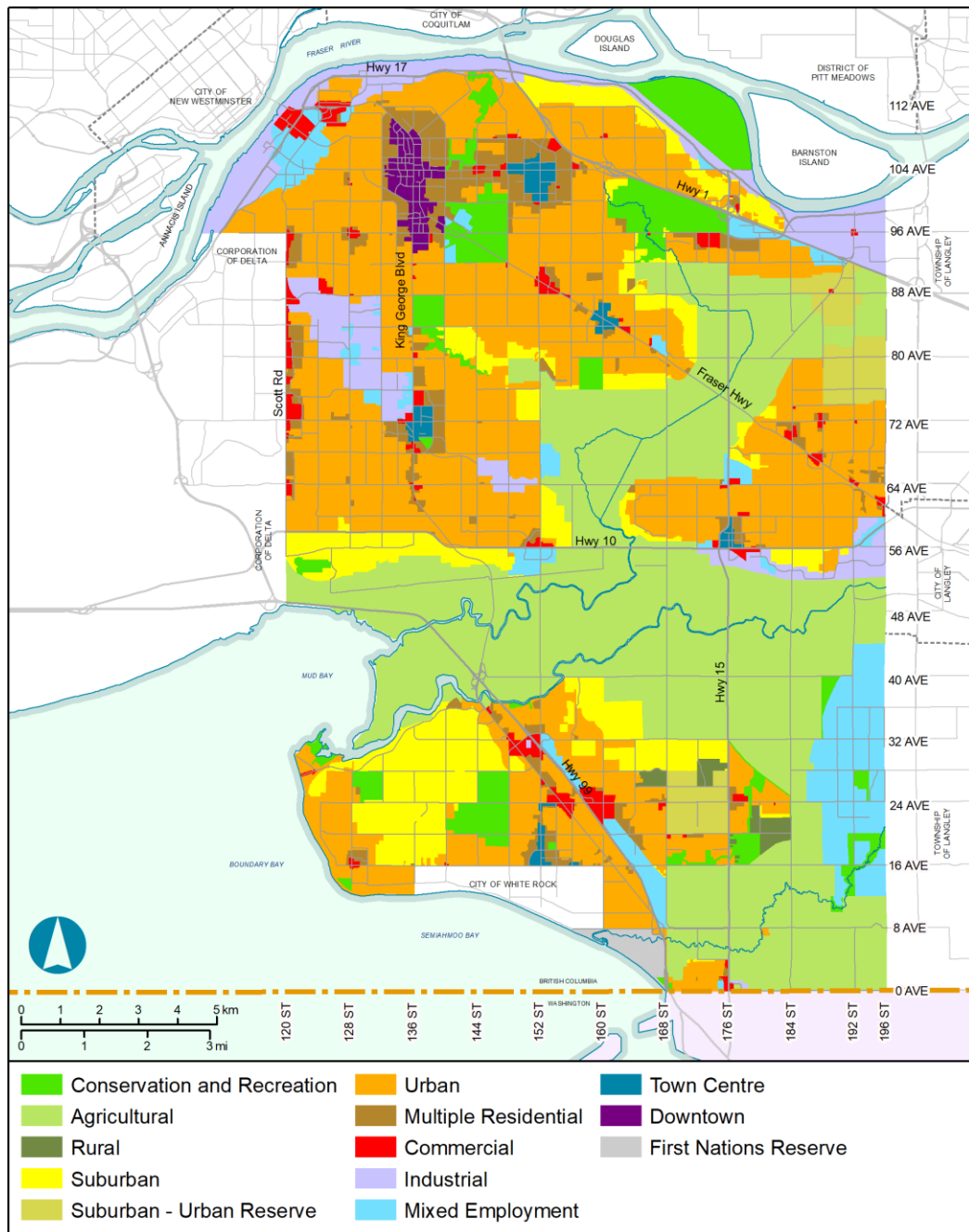
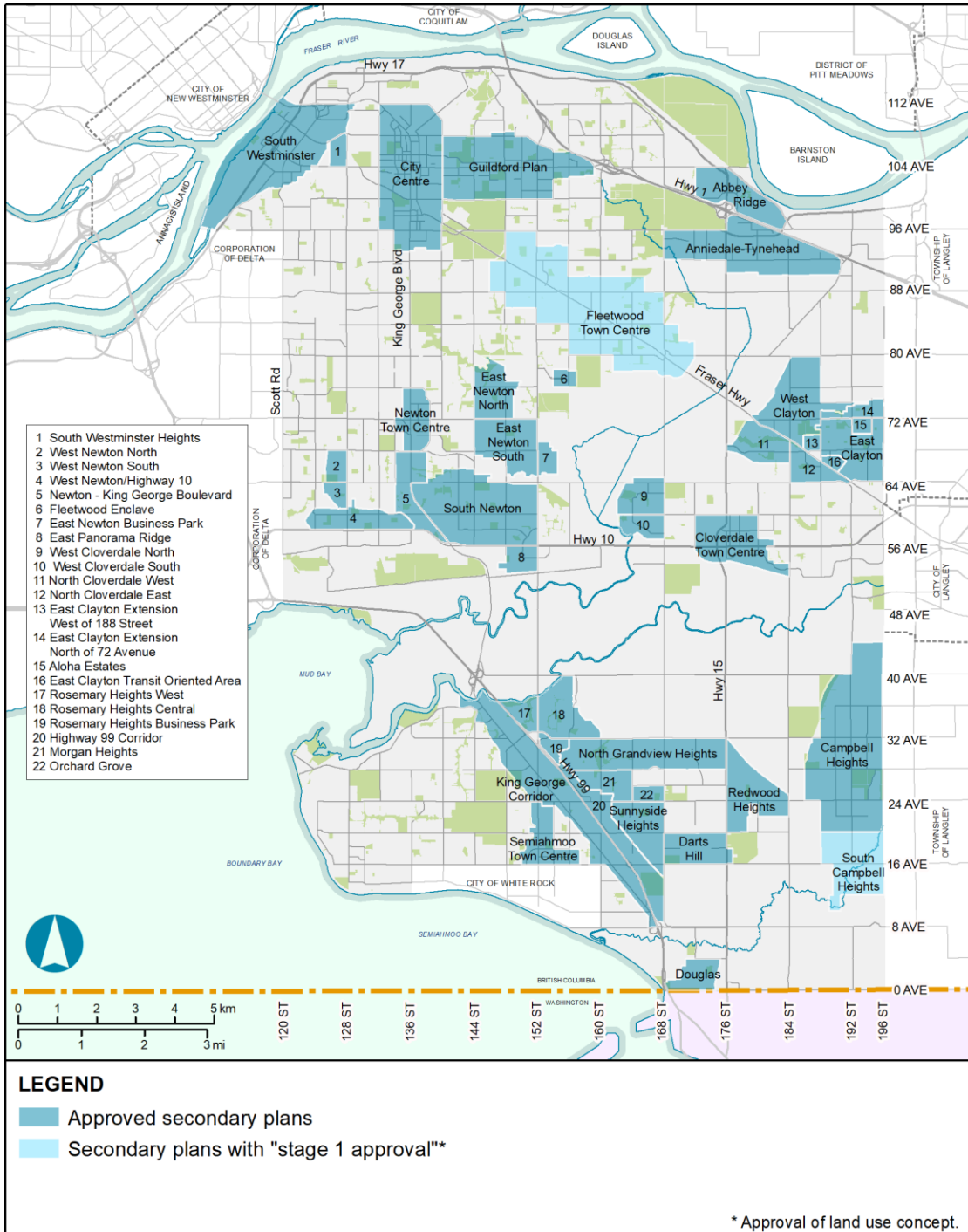


Figure 4: Secondary Plan Areas

- BL 18423
- BL 18498
- BL 18787
- BL 19996
- BL 20340
- BL 20292
- BL 20554
- BL 20393
- BL 20658
- BL 20340
- BL 20658
- BL 20935
- BL 21086
- BL 21104
- BL 21183



Conservation -Recreation

The CONSERVATION-RECREATION designation is intended to protect significant natural ecosystems and extensive outdoor recreation areas and parks.

Lands within this designation include conservation areas, urban forests, public facilities and major Regional and City parks. City parks included in this designation contain significant natural areas with conservation management objectives.

DEVELOPMENT CONSIDERATIONS FOR CONSERVATION-RECREATION:

⇒ There are no specific development considerations applicable to this designation at this time.



*Example of Conservation-Recreation Designation:
Green Timbers Lake in Green Timbers Urban Forest*

Agricultural

The AGRICULTURAL designation is intended to support agriculture, complementary land uses and public facilities. This designation includes lands within the Agricultural Land Reserve (ALR) as well as rural lands outside the ALR that are used for farming and various other complementary uses.

For property located within the Agricultural Land Reserve, uses are permitted in accordance with the *Agricultural Land Commission Act*, the regulations and the orders of the Agricultural Land Commission (ALC) and the *Farm Practices Protection Act*. Non-farm uses require the approval of the ALC.

ALR land exclusion applications are not supported except where there is a demonstrated clear benefit to agriculture within Surrey and where, for every 1 hectare of land removed from Surrey's ALR, a minimum of 2 hectares of land is added into the ALR within Surrey.

For property not located within the Agricultural Land Reserve, uses are restricted to farming, rural residential and those uses compatible with farming.

DEVELOPMENT CONSIDERATIONS FOR AGRICULTURAL:

Subdivision (Property Within ALR):

⇒ Subdivision that creates new lots is discouraged without a demonstrated clear net benefit to agriculture.

Subdivision (Property Outside of ALR):

⇒ Subdivision of land is generally limited to the creation of lots 2 hectares (5 acres) or larger.



Example of Agricultural Designation:
Active farmland

Rural

The RURAL designation is intended to support low-density residential uses and public facilities on large properties that are not serviced by sanitary sewers and are not expected to be serviced in the foreseeable future.

DEVELOPMENT CONSIDERATIONS FOR RURAL:

Lot Sizes:

BL 18423

- ⇒ Lots in RURAL designated areas are generally restricted to a minimum of 0.8 hectares (2 acres) or larger, in size.



*Example of Rural Designation:
Single Family Residential on large lot*

Suburban

BL 18423
BL 18833

The SUBURBAN designation is intended to support low-density residential uses, complementary institutional, agricultural and small-scale commercial uses and public facilities in keeping with a suburban neighbourhood character.

DEVELOPMENT CONSIDERATIONS FOR SUBURBAN:

Densities:

- ⇒ Densities within the SUBURBAN designation may range up to a maximum of 5 units per hectare (2 units per acre).
- ⇒ Densities within the SUBURBAN designation may increase up to 10 units per hectare (4 units per acre).

EXCEPT:

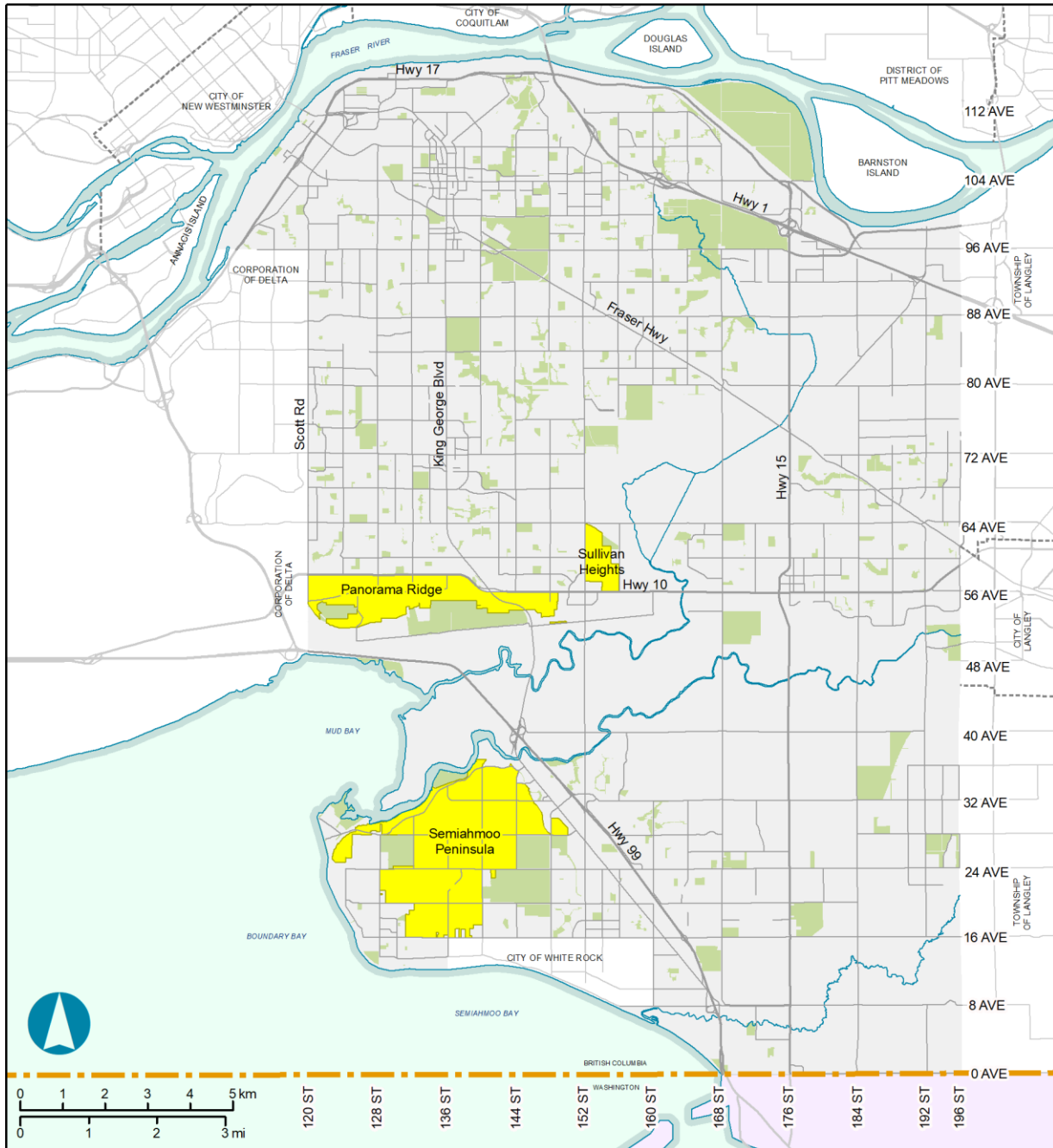
For areas within 200 m of the Agricultural Land Reserve (ALR) edge, and for those SUBURBAN areas shown in Figure 5, densities are a maximum of 5 units per hectare (2 units per acre). For development sites that have portions both within and beyond 200 m of the ALR edge, the allowable density within each portion may be averaged over the entire site area, subject to compatibility with adjacent existing lot sizes.

- ⇒ Densities within the SUBURBAN designation may be calculated on a gross site area where sufficient parkland and/or a community benefit are provided.



*Example of Suburban Designation:
large single family homes widely spaced apart*

Figure 5: Suburban Density Exception Areas



BL 18423
BL 21104
BL 21016

LEGEND

Suburban density exception areas

Note: This map is for general reference only.

Figure 6: Suburban Subdivision Exception Areas

BL 18423

DELETED

Suburban— Urban Re- serve

Land within the SUBURBAN—URBAN RESERVE designation is intended to support the retention of Suburban land uses in areas where future urban development is expected; and is subject to City Council initiation and approval of comprehensive Secondary Plans such as Neighbourhood Concept Plans.

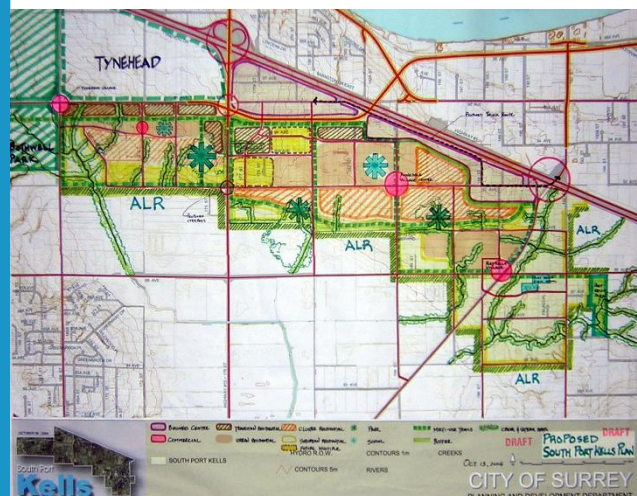
Until a Secondary Plan is approved and until the OCP land use designation is amended to conform to that approved Secondary Plan, land uses within this designation will follow the same requirements as those listed under the SUBURBAN designation.

DEVELOPMENT CONSIDERATIONS FOR SUBURBAN-URBAN RESERVE:

Lot Sizes:

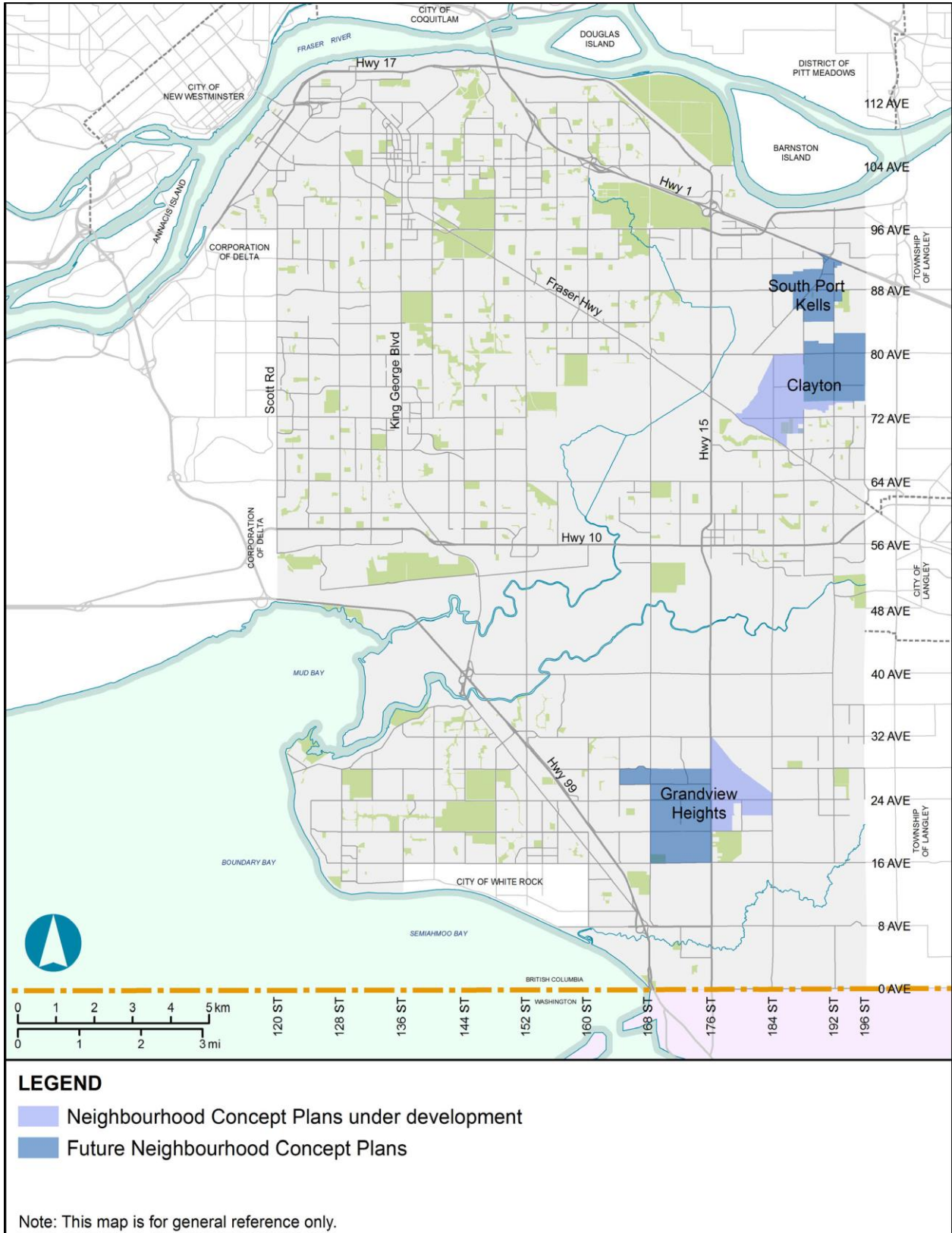
⇒ In order to facilitate the efficient and effective development of planned urban neighbourhoods well into the future—new lots in SUBURBAN-URBAN RESERVE designated areas (see Figure 3 and 7) are generally restricted to a minimum of 2 hectares (5 acres), or larger, in size.

BL 18423



Example of Suburban-Urban Reserve Designation: A Secondary Plan detailing how an area of land designated suburban will eventually evolve and change to an Urban designation

Figure 7: Future Development Areas



Urban

The URBAN designation is intended to support low and medium density residential neighbourhoods. Residential uses within this designation may include a range of forms such as detached and semi-detached houses as well as ground-oriented attached housing including townhouses and row houses.

Other complementary uses included in this land use designation are public facilities, places of worship, small-scale daycare facilities, schools, live-work units and small-scale neighbourhood-serving shops.

DEVELOPMENT CONSIDERATIONS FOR URBAN:

Densities:

- ⇒ Subject to neighbourhood compatibility (see Figure 8), densities within the URBAN designation support up to 37 units per hectare (15 units per acre) for development taking place within established or existing residential neighbourhoods, which may be increased to 49 units per hectare (20 units per acre) in exchange for provision of sufficient community amenities in accordance with approved City Council and Department policies.
- ⇒ Referencing Figure 9, densities within the URBAN designation may range up to 72 units per hectare (30 units per acre) in FTDA's, Urban Centres and sites abutting a Frequent Transit Network and, where specifically noted, in approved Secondary Plan Areas (Figure 4).

Development Permits:

- ⇒ Commercial, multiple unit residential and mixed-use developments within this designation are subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP.

BL 18423
BL 18787
BL 20008
BL 20109
BL 20292



Example of Urban Designation: smaller and narrower single family lots with smaller front yard setbacks and direct access to the street from the front door

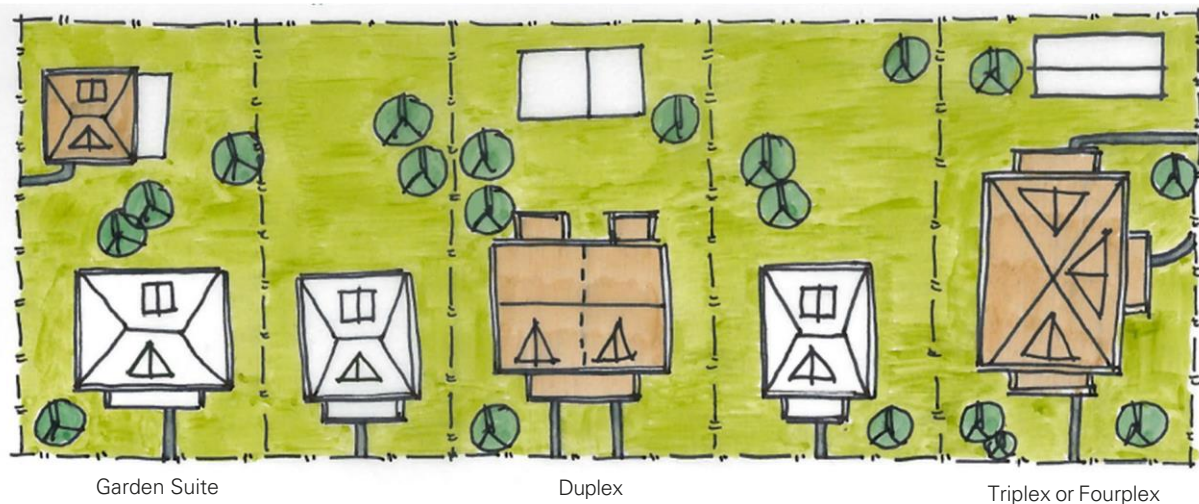
Figure 8: Neighbourhood Compatibility

Typically an issue during times of transition or when working to accommodate higher densities, neighbourhood compatibility is a concept used in the development of cities to promote quality design and to encourage new or infill development to be appropriate and relevant to the existing and established character of a surrounding area or neighbourhood. New development is considered compatible with existing neighbourhoods when its design has taken into consideration, and attempts to replicate, follow or copy, the following elements:

- Number of storeys**
- Lot size**
- Architectural style and features**
- Window style and material**
- Roof design, pitch and material**
- Siding material**
- Setbacks**
- Entry features**

Other neighbourhood impacts such as **retaining views, privacy and tree canopy** should also be examined when determining the neighbourhood compatibility of new development in existing areas.

In general, neighbourhood compatibility refers to the inherent quality, feel, look and style of an area that is dominant *before* new or infill development takes place. In the context of development in Surrey, neighbourhood compatibility should be used to determine appropriate development where there is no existing Secondary Plan in place.



INFILL EXAMPLE: A) USE SIMILAR SETBACKS; B) RETAIN VEHICLE ACCESS POINTS; C) MAINTAIN PRIVACY OF ADJACENT HOMES; D) DEFINE EACH NEW UNIT INDIVIDUALLY BY PROVIDING INDIVIDUAL FRONT ENTRANCE POINTS ; F) PROVIDE FOR APPROPRIATE TRANSITIONS, PARTICULARLY WITH BUILDING HEIGHT, BUILDING DESIGN AND SECOND STOREY SETBACKS.

Multiple Residential

The MULTIPLE RESIDENTIAL designation is intended to support higher-density residential development including local, neighbourhood-serving commercial and community uses. These higher-density neighbourhoods are typically located adjacent to COMMERCIAL, TOWN CENTRE or DOWNTOWN designations to support the vitality of these areas or along Frequent Transit Corridors.

Subject to creating an appropriate interface with adjacent lower-density residential neighbourhoods, residential uses in this designation may include apartment buildings (generally up to 6 storeys), higher-density townhouses (typically with underground or structured parking) and supportive housing community care facilities that are constructed as multiple-family buildings.

Limited commercial and institutional uses may be supported within the MULTIPLE RESIDENTIAL designation in mixed-use development, but commercial uses that have a large number of employees and/or that generate high traffic volumes are not supported. Public facilities are also permitted within the MULTIPLE RESIDENTIAL designation.

DEVELOPMENT CONSIDERATIONS FOR MULTIPLE RESIDENTIAL:

Densities:

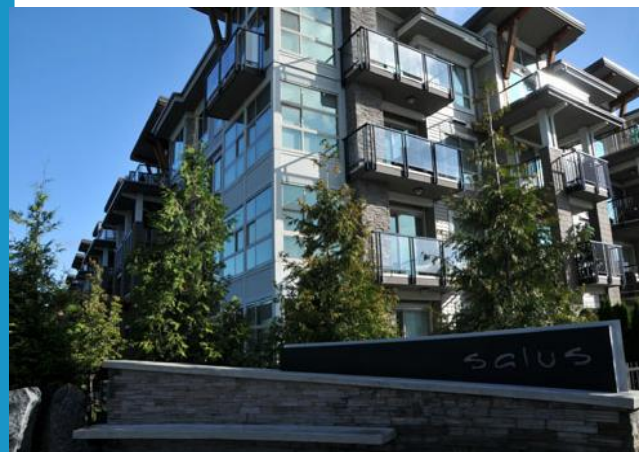
Except as shown in Table 7a, the following densities apply:

- ⇒ Densities within the MULTIPLE RESIDENTIAL designation may range up to 1.5 FAR, subject to an appropriate interface (see Figure 10) with adjacent lower-density residential developments
- ⇒ Referencing Figure 4 and Figure 9, densities within the MULTIPLE RESIDENTIAL designation may range up to 2.5 FAR in Frequent Transit Development Areas, Urban Centres, sites abutting a Frequent Transit Network, within the City Centre Plan area, and where specifically noted in an approved Secondary Plan Area.
- ⇒ Additional bonus densities may be granted in select areas in exchange for the provision of sufficient community amenities in accordance with approved City Council and Department policies.

Development Permits:

- ⇒ Commercial, multiple unit residential and mixed-use development within this land use designation is subject to the issuance of a Development Permit in accordance with DP1 of the Implementation Section of this OCP.

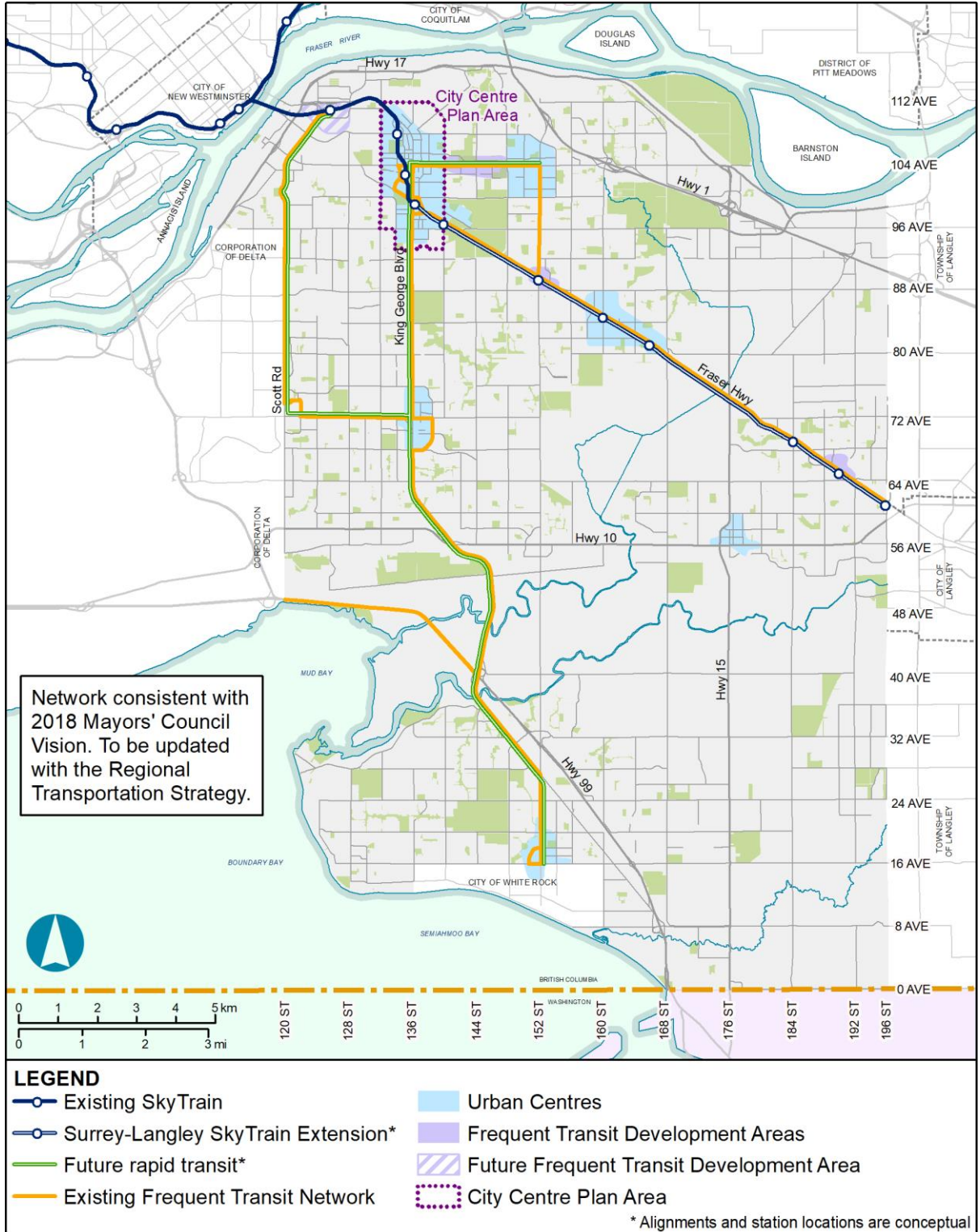
BL 18787
BL 19364
BL 20109
BL 20658
BL 21086



Example of Multiple Residential Designation: Higher density developments including stacked townhouses and condo buildings

Figure 9: Centres and Rapid Transit Areas and Corridors

BL 19801
BL 21104



Commercial

The COMMERCIAL designation is intended to support major commercial developments, including neighbourhood-serving and city-serving retail and office developments. Lands within the COMMERCIAL designation are typically located in neighbourhood centres, along major roads, or in areas adjacent to TOWN CENTRE and DOWNTOWN designations.

Primary uses within the COMMERCIAL designation are retail and stand-alone office uses including institutional offices. Multi-unit residential uses may also be permitted in mixed-use development provided that ground-level uses, with noted exceptions, are exclusively commercial.

Exceptions to ground-level commercial apply only to those areas of the building located away from street frontages; these areas may support residential uses by accommodating indoor amenities or parking (for example) but specifically are not to include dwelling units.

Limited light industrial uses may be permitted within this designation but these developments are subject to being compatible with adjacent land uses. Public facilities are also permitted uses within the COMMERCIAL designation.

DEVELOPMENT CONSIDERATIONS FOR COMMERCIAL:

Densities:

- ⇒ Densities within the COMMERCIAL designation may range up to 1.5 FAR, subject to an appropriate interface (see Figure 10) with adjacent residential areas
- ⇒ Referencing Figure 9, densities within the COMMERCIAL designation may range up to 2.0 FAR for sites abutting a Frequent Transit Network and, where specifically noted, in approved Secondary Plan Areas (Figure 4)
- ⇒ Referencing Figure 9, densities within the COMMERCIAL designation may range up to 2.5 FAR in Frequent Transit Development Areas (FTDA) and in Urban Centres, provided a minimum of 0.5 FAR is used for commercial purposes
- ⇒ Referencing Figure 4, densities within the COMMERCIAL designation within the Guildford Plan area may range up to 3.5 FAR where specifically noted in the plan.
- ⇒ Additional bonus densities may be granted in select areas in exchange for the provision of sufficient community amenities in accordance with approved City Council and Department policies

Development Permits:

- ⇒ Commercial, multiple unit residential, mixed-use and industrial developments within this designation are subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP.

BL 18787
BL 20292
BL 20658
BL 20292
BL 21086



Example of Commercial Designation: Ground-level commercial uses with upper storey multiple residential development.

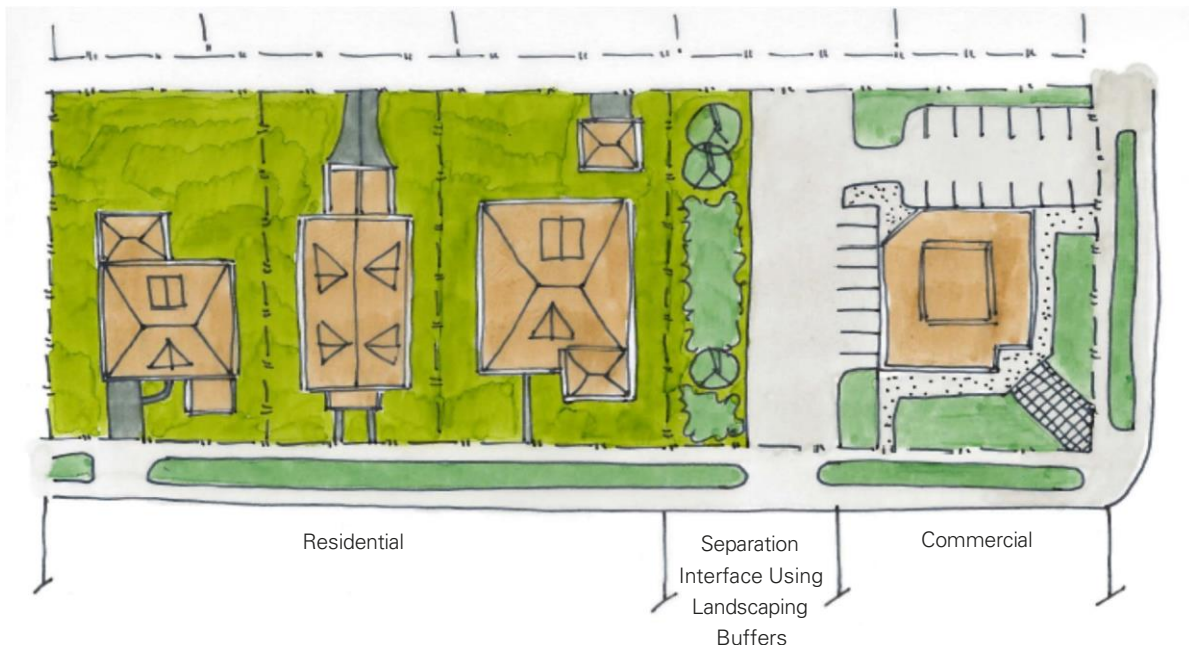
Figure 10: Appropriate Interfaces

Urban environments contain a mixture of different land uses required to support a highly-functioning residential base and workforce. Similar uses are typically located together so as to reduce conflicts that may arise between non-compatible uses. To create a truly healthy, vibrant and pedestrian-oriented city mixing use types is required. Mixing uses can however, result in negative impacts imposed on one group over another by virtue of close proximity. In order to continue to successfully mix uses in an urban environment, efforts are required to ensure the interface between two opposing uses is addressed appropriately to reduce negative impacts. The design of these interface areas, for example, between residential and commercial properties, can greatly influence quality of life and transportation options.

In order to appropriately address issues that arise from mixing uses, special attention should be made to design and locate the following items so as to reduce or eliminate their negative impacts on adjacent properties:

- Loading areas/bays**
- Ventilation units**
- External lighting**
- Driveways (particularly for drive-thrus)**
- Commercial Garbage bins**

Adjusting **setbacks and landscaping** areas can also greatly help in creating effective interfaces between commercial and residential land uses.



Industrial

The INDUSTRIAL designation supports light and heavy industrial land uses, including manufacturing, warehouse, wholesale trade and equipment storage and repair. Accessory uses that operate ancillary to a main industrial use may include limited office uses, a caretaker's residence, and commercial uses that are strictly limited to those that support industrial activities. Public facilities are permitted within the INDUSTRIAL designation but retail uses and stand-alone office uses are generally not supported.

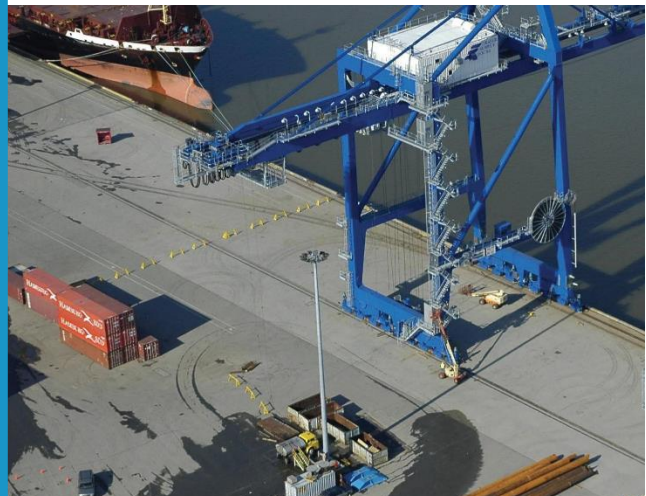
DEVELOPMENT CONSIDERATIONS FOR INDUSTRIAL:

Densities:

⇒ INDUSTRIAL densities may range up to approximately 1.0 FAR, subject to appropriate site planning to support the efficient operation of industrial operations.

Development Permits:

⇒ Development is subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP. Industrial developments within this designation may be subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP.



Example of Industrial Designation:
Fraser-Surrey Docks

BL 18423
BL 20658

Mixed Employment

The MIXED EMPLOYMENT designation is intended to support a mix of industrial, commercial, business and office uses that are not suited for locations within Town Centres or commercial centres. Commercial use examples may include business parks and, in select locations, large-scale retail outlets with warehousing requirements for furniture, building and landscaping supplies, outdoor storage and vehicle and equipment servicing, as shown in an approved Secondary Plan. Office use examples include those related to industrial uses, construction trades, equipment assembly, repair and testing, and large assembly uses. Public facilities are permitted within the MIXED EMPLOYMENT designation.

Industrial uses are encouraged to locate in MIXED EMPLOYMENT designated areas, except heavy industry.

Residential uses are not permitted in this land use classification except for accessory caretaker units.

Commercial uses that do not require extensive outdoor storage areas or large format floor space, or that are not related to industrial uses are encouraged to locate in the COMMERCIAL, TOWN CENTRE or DOWNTOWN land use designated areas. Notwithstanding the overall intent of the MIXED EMPLOYMENT designation, large

-format retail commercial uses are not permitted, except as shown in an approved Secondary Plan.

DEVELOPMENT CONSIDERATIONS FOR MIXED EMPLOYMENT:

Densities:

- ⇒ Development within the MIXED EMPLOYMENT designation may range up to 1.0 FAR and up to 1.5 FAR within Frequent Transit Development Areas and in Urban Centres, as shown in Figure 9.
- ⇒ Development densities are subject to designing an appropriate interface with adjacent lower-density residential neighbourhoods.

Development Permits:

- ⇒ Development is subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP. Industrial developments within this designation may be subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP.



Example of Mixed Employment:
Morgan Creek Business Park

Town Centre

The TOWN CENTRE designation is intended to support the development of each of Surrey's five Town Centres outside of the City Centre (Guildford, Fleetwood, Newton, Cloverdale, Semiahmoo) as the primary commercial, institutional and civic hearts of their communities.

Land uses within the TOWN CENTRE designation include public facilities, institutional and civic uses, commercial uses, stand-alone and mixed office and retail developments, mixed-use commercial and multiple family residential developments and stand-alone multiple family residential development subject to policies in approved Town Centre Plans.

Industrial and lower-density residential uses, including detached and semi-detached housing, are discouraged within the TOWN CENTRE designation.

Each of the five Town Centres included in this designation has its own unique character that should be reinforced through high quality urban design in public and private sector development and in the design of public space.

DEVELOPMENT CONSIDERATIONS FOR TOWN CENTRE:

Densities:

- ⇒ Densities within the TOWN CENTRE designation differ according to each unique Town Centre as shown in Figures 11 through 15.
- ⇒ Densities within the TOWN CENTRE designation are calculated on a gross site area basis and may exceed the densities shown in Figure 11 through 15 where sites are adjacent to existing or planned rapid transit station locations (shown in Figure 9).
- ⇒ Additional bonus densities may be granted in exchange for the provision of sufficient community amenities in accordance with approved City Council and Department Policies.

Development Permits:

- ⇒ Commercial, multiple unit residential and mixed-use developments within this designation are subject to the issuance of a Development Permit, in accordance with DP1 of the Implementation Section of this OCP.



Example of Town Centre: Semiahmoo
Town Centre, 152 Street

Figure 11: Cloverdale Town Centre Densities

BL 18787
BL 19996

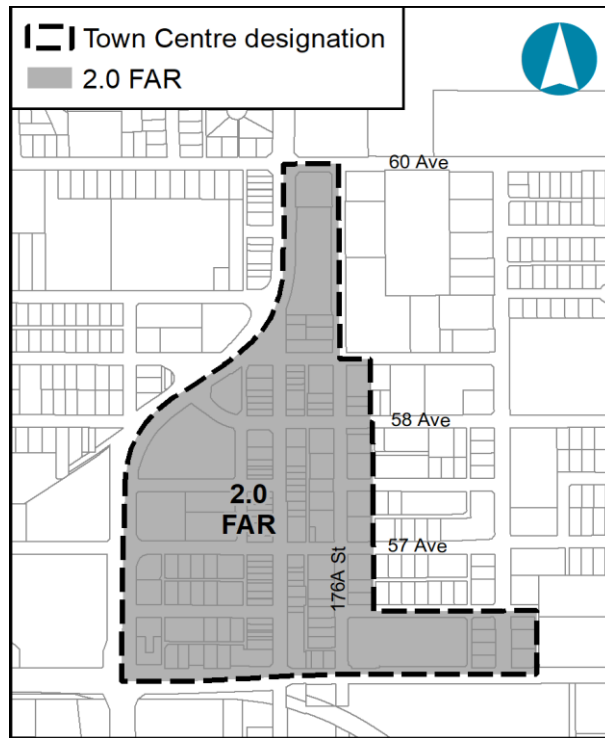


Figure 12: Fleetwood Town Centre Densities

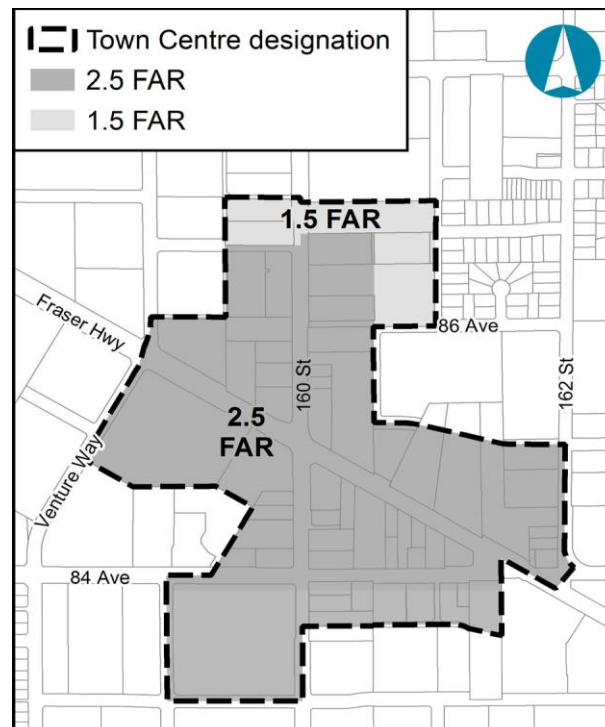


Figure 13: Guildford Town Centre Densities

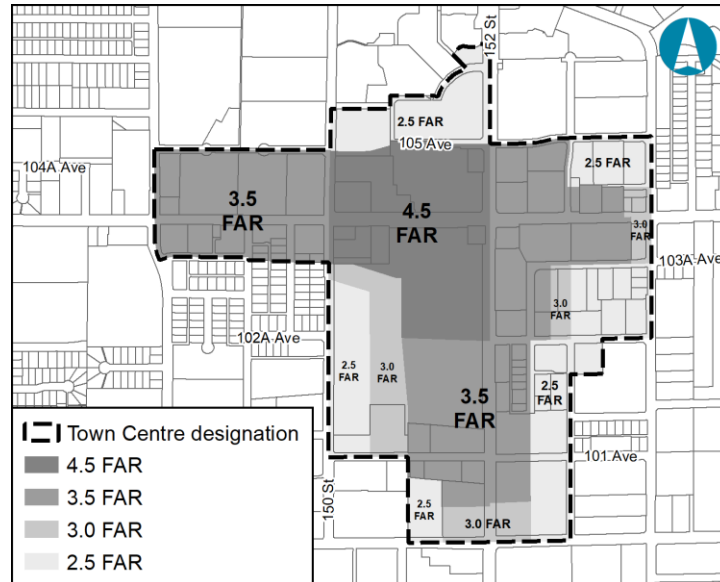


Figure 14: Newton Town Centre Densities

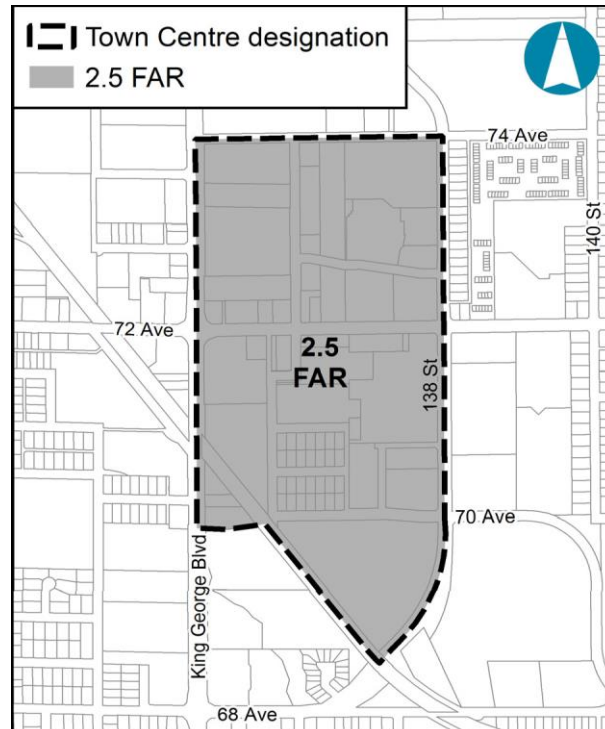
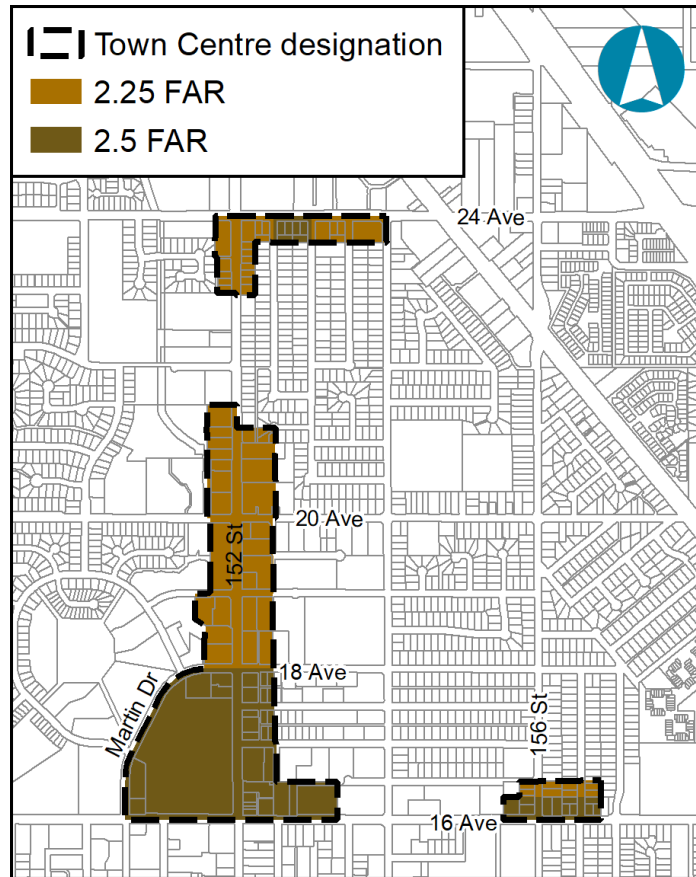


Figure 15: Semiahmoo Town Centre Densities

BL 20554
BL 20966



BL 21533

Downtown

The DOWNTOWN designation is intended to support the continued development of Surrey City Centre as the primary commercial, civic, institutional, transit and high-density residential centre for Surrey and as the primary metropolitan centre for the “South of the Fraser” area of Metropolitan Vancouver. Urban design of public and private sector development including buildings, streets, plazas and gathering spaces, is intended to be of the highest quality to provide a vibrant, pedestrian-friendly and highly attractive environment and to support investment and activity in the City Centre.

Lands within this designation are intended for the highest density development in Surrey in order to support a vibrant commercial and civic centre and to support high-capacity rapid transit services. This includes commercial, retail and office developments, major institutional and civic developments, public facilities and mixed-use commercial and residential developments.

Stand-alone, high-density residential apartment development may be permitted in selected areas within this designation as identified in the *Surrey City Centre Plan*.

Example of Central Business District Designation:
Central City Office Tower



DEVELOPMENT CONSIDERATIONS FOR DOWNTOWN:

Densities:

- ⇒ Densities within the DOWNTOWN designation vary by location as shown in Figure 16.
- ⇒ Densities within DOWNTOWN are calculated on gross site area before dedications for public purposes such as streets, parks or plazas.
- ⇒ Within Central Business District Area 1, as shown in Figure 16, the density of 7.5 FAR is comprised of 3.5 FAR non-residential and 4.0 FAR residential. If the non-residential floor space is delivered in a standalone building of at least 200,000 square feet and 15 storeys, non-residential may be reduced to 3.0 FAR and residential increased to 4.5 FAR.
- ⇒ Within Central Business District Area 2, as shown in Figure 16, the density of 7.5 FAR is comprised of 2.0 FAR non-residential and 5.5 FAR residential. If the non-residential floor space is delivered in a standalone building of at least 100,000 square feet and 10 storeys, non-residential may be reduced to 1.5 FAR and residential increased to 6.0 FAR.
- ⇒ Additional bonus densities may be granted in accordance with applicable bylaws.

Development Permits:

- ⇒ Commercial, multiple-unit residential and mixed-use developments within the DOWNTOWN designation are subject to the issuance of a Development Permit in accordance with DP1 of the Implementation Section of this OCP.

Figure 16: Downtown Densities

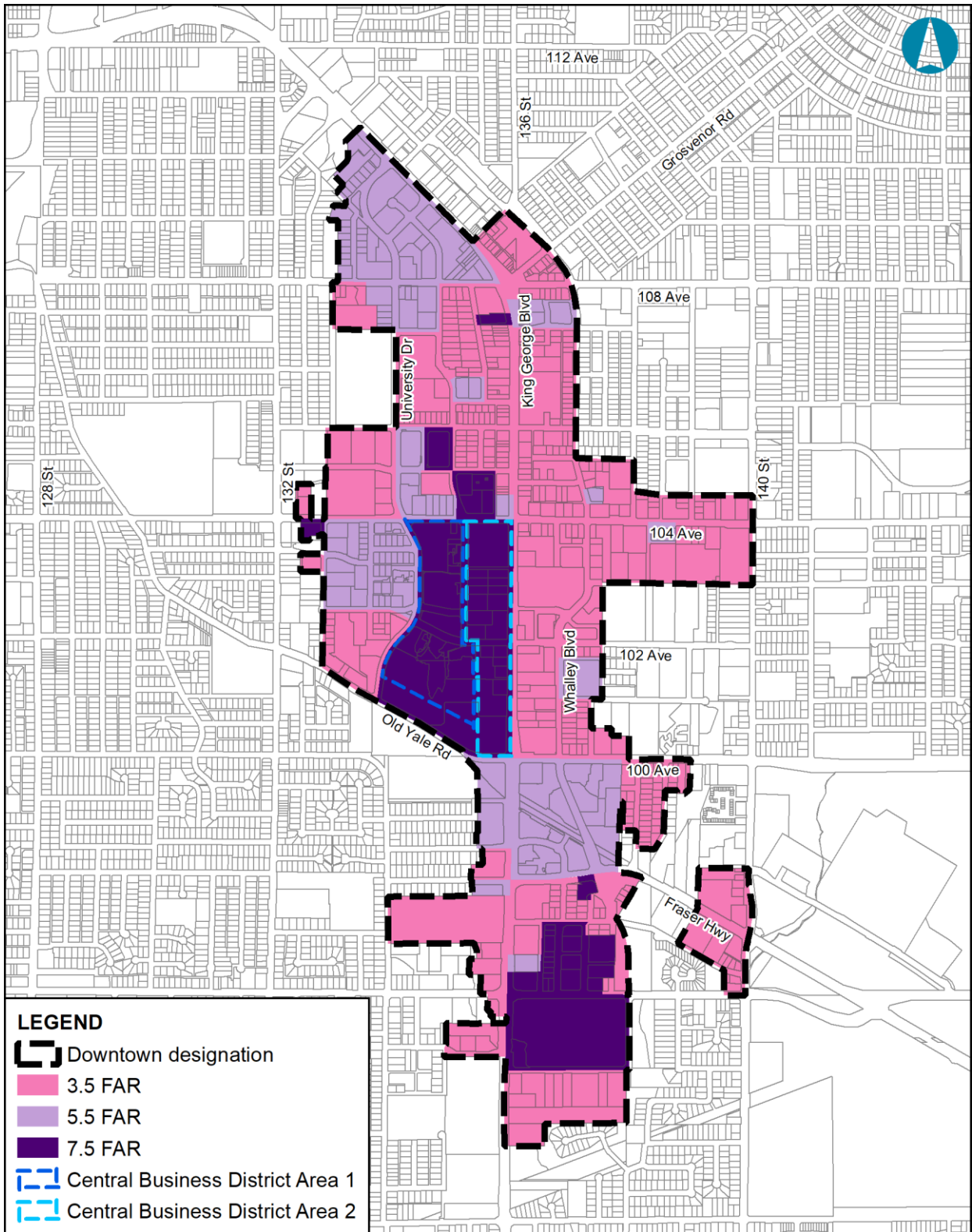


Table 7a: Land Use Designation Exceptions

The following use or density exceptions are permitted for the following properties, or any lots created from these properties, in the identified Land Use Designation, on a site-specific basis only:

<u>BYLAW NO.</u>	<u>LAND USE DESIGNATION</u>	<u>SITE SPECIFIC PROPERTY</u>	<u>SITE SPECIFIC PERMISSION</u>
19243	Multiple Residential	12661—110 Avenue (Lot 1, Section 17, Blk. 5N, Range 2W, NWD, Plan EPP39522)	Density permitted up to 2.0 FAR (gross calculation)
18716	Multiple Residential	14150 Green Timbers Way (Blk. H, Sec. 36, Blk. 5 N, Range 2W, NWD, Except Plan BCP51681); and 9900 140 Street (Blk. I, Sec. 36, Blk. 5 N, Range 2W, NWD, Except Plan BCP51681)	Density permitted up to 2.5 FAR
19487	Multiple Residential	17505 and 17515 64 Avenue (Lot 4 and 5, except part Dedicated Road on Plan BCP15284, Sec. 18, Twp. 8, NWD, Plan 14338); and Portion of 6455 176 Street (Lot 6, except part on Plan BCP10827, Sec. 18, Twp. 8, NWD, Plan 24144)	Density permitted up to 1.86 FAR
19886	Multiple Residential	14706—104 Avenue (Lot 151, Sec. 30, Blk 5N, Range 1W, NWD, Plan 50505, Except Part in Plan BP33452)	Density permitted up to 2.8 FAR (gross calculation)
19863	Multiple Residential	8109, 8123 and 8135 King George Boulevard	2.52 FAR
19751	Multiple Residential	Portion of 8205 King George Boulevard	2.83 FAR
20565	Mixed Employment	6611 – 152A Street (Lot 4 Sec. 14 Township 2 NWD Plan BCP49715)	1.3 FAR (net calculation)
20725	Mixed Employment	15290 – 66 Avenue (Lot 5 Section Township 2 NWD Plan 1442)	1.3 FAR (net calculation)
20695	Multiple Residential	14744 - 106 Avenue (Lot 137 Section 19 Block 5 North Range 1 West NWD Plan 41515) 14754 - 106 Avenue (Lot 136 Section 19 Block 5 North Range 1 West NWD Plan 41515) 14764 - 106 Avenue (Lot 135 Section 19 Block 5 North Range 1 West NWD Plan 41515) 14774 - 106 Avenue (Portion of Lot 134 Section 19 Block 5 North Range 1 West NWD Plan 41515)	Density permitted up to 2.38 FAR

- BL 18787
- BL 19243
- BL 18716
- BL 19487
- BL 19886
- BL 19863
- BL 19751
- BL 20565
- BL 20695
- BL 20966
- BL 20547
- BL 20958
- BL 20843
- BL 20923
- BL 20894
- BL 21096

<u>BYLAW NO.</u>	<u>LAND USE DESIGNATION</u>	<u>SITE SPECIFIC PROPERTY</u>	<u>SITE SPECIFIC PERMISSION</u>
20966	Multiple Residential	<p>2350 – 152A Street Lot 78 Section 14 Township 1 NWD Plan 13327</p> <p>2360 – 152A Street South Half Lot 31 Section 14 Township 1 NWD Plan 8492</p> <p>2355 – 153 Street Lot 79 Section 14 Township 1 NWD Plan 13327</p> <p>2365 – 153 Street The South Half Lot 30 Section 14 Township 1 NWD Plan 8492</p>	Density permitted up to 2.48 FAR (net calculation)
20547	Multiple Residential	<p>8293 King George Boulevard Lot 67 Section 29 Township 2 NWD Plan 51521</p> <p>8345 – 135A Street Lot 71 Section 29 Township 2 NWD Plan 57624</p> <p>That 54.3 sq. m. portion of closed road</p>	Density permitted up to 2.21 FAR
20958	Multiple Residential	<p>11054 – 132 Street Lot 1, Except Part Dedicated Road on Plan LMP41027, Section 15 Block 5 North Range 2 West NWD Plan 8791</p> <p>11066 – 132 Street Lot 2, Except Part Dedicated Road on Plan LMP41027, Section 15 Block 5 North Range 2 West NWD Plan 8791</p> <p>11080 – 132 Street Lot 3, Except Part Dedicated Road on Plan LMP41027, Section 15 Block 5 North Range 2 West NWD Plan 8791</p> <p>11037 Ravine Road Lot 80 Except: Parcel H (Bylaw Plan 87021) Section 15 Block 5 North Range 2 West NWD Plan 34840</p> <p>11049 Ravine Road Lot 5 Section 15 Block 5 North Range 2 West NWD Plan 9739</p> <p>11057 Ravine Road Lot 4 Section 15 Block 5 North Range 2 West NWD Plan 9739</p> <p>11069 Ravine Road Lot 3 Section 15 Block 5 North Range 2 West NWD Plan 9739</p>	Density permitted up to 2.7 FAR

BYLAW NO.	LAND USE DESIGNATION	SITE SPECIFIC PROPERTY	SITE SPECIFIC PERMISSION
20843	Multiple Residential	<p>10620 – 138A Street Lot 74 Section 23 Block 5 North Range 2 West NWD Plan 49290</p> <p>10628 – 138A Street Lot 81 Section 23 Block 5 North Range 2 West NWD Plan 57467</p> <p>10619 – 139 Street Lot 75 Section 23 Block 5 North Range 2 West NWD Plan 49290</p> <p>10631 – 139 Street Lot 82 Section 23 Block 5 North Range 2 West NWD Plan 57467</p>	Density permitted up to 3.1 FAR
20923	Multiple Residential	<p>14518 - 104A Avenue Lot "C" Section 19 Block 5 North Range 1 West NWD Plan 17992</p> <p>14528 - 104A Avenue Lot 124 Section 19 Block 5 North Range 1 West NWD Plan 41196</p> <p>14538 - 104A Avenue Lot 125 Section 19 Block 5 North Range 1 West NWD Plan 41196</p> <p>14548 - 104A Avenue Lot 126 Section 19 Block 5 North Range 1 West NWD Plan 41196</p>	Density permitted up to 2.35 FAR
20894	Multiple Residential	<p>7112 – 128 Street South Half Lot 1; Except: Firstly: Part Subdivided by Plan 16116 and Secondly: Part Subdivided by Plan 18420; Thirdly: Part on Plan BCP7030 Section 17 Township 2 NWD Plan 1183</p>	Density permitted up to 2.00 FAR (net calculation)
21096	Multiple Residential	<p>7790 King George Boulevard Lot 60 Section 21 Township 2 NWD Plan 46548</p>	Density permitted up to: 2.75 FAR (Block 4) 2.35 FAR (Block 5) 2.49 FAR (Block 6)
		<p>7850 King George Boulevard Parcel "B" North West Quarter Section 21 Township 2 NWD Plan 55111</p>	Density permitted up to: 2.78 FAR (Block 1) 2.79 FAR (Block 2) 2.23 FAR (Block 3)

1 TRANSIT-ORIENTED AREAS

- 1.1 The *Local Government Act* requires municipalities to designate Transit-Oriented Areas (TOAs) around existing and planned transit stations. TOAs are areas within a prescribed distance of a transit station, within which minimum allowable densities and heights are applicable. The *Local Government Transit-Oriented Areas Regulation* prescribes the applicable transit stations as well as distances that define the geographic extent of TOAs and classes of land within TOAs, called tiers. The regulation also establishes minimum allowable densities and heights within TOAs and associated applicability criteria. Unless otherwise provided in relation to Transit-Oriented Areas under this Part of the Official Community Plan, the definitions in the *Local Government Act* and regulations thereunder apply to this Part.

2 TRANSIT STATIONS

- 2.1 Transit-Oriented Areas are designated for the following twelve SkyTrain stations:
- (a) **Scott Road Station** located above the Scott Road on-ramp to King George Boulevard north of 110 Avenue (see Figure 16a);
 - (b) **Gateway Station** located on the east side of University Drive at 108 Avenue (see Figure 16b);
 - (c) **Surrey Central Station** located on the west side of City Parkway between 102 Avenue and Central Avenue (see Figure 16c);
 - (d) **King George Station** located at 9904 King George Boulevard (see Figure 16d);
 - (e) **Green Timbers Station** located at the northeast corner of Fraser Highway and 140 Street (see Figure 16e);
 - (f) **152 Street Station** located at the northwest corner of Fraser Highway and 152 Street (see Figure 16f);
 - (g) **Fleetwood Station** located at the northeast corner of Fraser Highway and 160 Street (see Figure 16g);
 - (h) **Bakerview-166 Street Station** located at the northwest corner of Fraser Highway and 166 Street (see Figure 16h);
 - (i) **Hillcrest-184 Street Station** located at the northwest corner of Fraser Highway and 184 Street (see Figure 16i);
 - (j) **Clayton Station** located on the north side of Fraser Highway between 189 Street and 191A Street (see Figure 16j);
 - (k) **Willowbrook Station** located at the northeast corner of Fraser Highway and 196 Street in the City of Langley (see Figure 16k); and

(l) **Columbia Station** located at 425 Columbia Street in the City of New Westminster (see Figure 16l).

2.2 Transit-Oriented Areas are designated for the following three bus exchanges:

(a) **Guildford Mall Exchange** located on 104 Avenue between 150 Street and 152 Street (see Figure 16m);

(b) **Newton Exchange** located at 13736 72 Avenue (see Figure 16n); and

(c) **Scottsdale Exchange** located at 7414 120 Street (see Figure 16o).

3 EXTENT OF TRANSIT-ORIENTED AREAS

3.1 For the purpose of defining TOAs, the *Local Government Transit-Oriented Areas Regulation* prescribes the following distances which apply according to the type of transit station:

(a) 800 metres for a SkyTrain station; and

(b) 400 metres for a bus exchange.

3.2 For each transit station, a TOA encompasses those areas within the prescribed distance of the transit station. The prescribed distance is measured horizontally from the coordinates identified for each transit station in the regulation.

3.3 Where the boundary of a TOA bisects a lot, the lot is deemed to be wholly within the TOA.

4 TRANSIT-ORIENTED AREA TIERS

4.1 For the purpose of setting minimum allowable densities and heights within TOAs, multiple classes of land, called tiers, are established within TOAs, according to the type of transit station and the distance from the transit station. Minimum densities and heights vary between tiers as described in Section 5 below.

4.2 For SkyTrain stations, three tiers are established as follows:

(a) Tier 1, encompassing lands up to 200 metres from the SkyTrain station;

(b) Tier 2, encompassing lands more than 200 metres and up to 400 metres from the SkyTrain station; and

(c) Tier 3, encompassing lands more than 400 metres and up to 800 metres from the SkyTrain station.

4.3 For bus exchanges, two tiers are established as follows:

(a) Tier 4, encompassing lands up to 200 metres from the bus exchange; and

(b) Tier 5, encompassing lands more than 200 metres and up to 400 metres from the bus exchange.

4.4 Where a lot falls within more than one tier, the tier allowing the greatest density and height (of the relevant tiers) applies to that lot.

4.5 Where a lot falls within different tiers with respect to more than one TOA, the tier allowing the greatest density and height (of the relevant tiers) applies. This may occur where TOAs overlap or are near one another.

5 MINIMUM ALLOWABLE DENSITY FRAMEWORK

5.1 The Minimum Allowable Density Framework (MD Framework) establishes minimum densities and heights that must be allowed, subject to Section 6.2, in relation to land within a TOA that is zoned to permit a residential use (either standalone or combined with other uses). The MD Framework applies on the basis of each lot's zoning classification as of the date of the designation of a relevant TOA. The figures on pages 60.5 to 60.19 reflect zoning classifications prior to the introduction of a bylaw amending the Official Community Plan to insert this Transit-Oriented Areas section.

The figures may not reflect any subsequent Zoning Bylaw amendments granted final adoption prior to final adoption of the Official Community Plan amendment bylaw. The status of a lot with respect to the MD Framework should be confirmed with the City of Surrey Planning & Development Department.

5.2 The minimum allowable density is expressed as a Floor Area Ratio (FAR), as defined in the Zoning Bylaw, which represents the ratio of a building's gross floor area to the area of the lot on which the building is constructed. The minimum allowable height is expressed in terms of the number of storeys (i.e. floors) wholly or partly above ground. According to the MD Framework, municipalities must not reject projects on the basis of density where the proposed density is up to the minimum allowable density or on the basis of height where the proposed height is up to the minimum allowable height. The following minimum allowable densities and heights, subject to Section 6.2, are established for Tiers 1 through 5:

Tier	Minimum Allowable Density	Minimum Allowable Height
Tier 1	5.0 FAR	20 storeys
Tier 2	4.0 FAR	12 storeys
Tier 3	3.0 FAR	8 storeys
Tier 4	4.0 FAR	12 storeys
Tier 5	3.0 FAR	8 storeys

5.3 The MD Framework applies in conjunction with applicable land use designations in the Official Community Plan and approved secondary plans. The minimum allowable density is inclusive of all uses and tenures which may be permitted or required by such land use designations. For clarity, the minimum density that must be allowed within a TOA may be required to be comprised of specific uses including, without limitation, residential, commercial, office, and

institutional uses. Each specific use may also be allocated a density of use. In addition, all or a portion of a residential use may be required as rental.

- 5.4 Within TOAs where a land use designation in the Official Community Plan or approved secondary plan allows for a level of density or height below the minimum allowable density and height, as specified in Section 5.2, the latter prevails, subject to Section 6.2.
- 5.5 Within TOAs, where an approved secondary plan supports residential uses on a lot not zoned to permit such uses, and where a rezoning application consistent with the secondary plan is proposed, the MD Framework applies, subject to Section 6.2. In such event, an update to the applicable TOA map is not required. Instead, TOA maps will be updated on a periodic basis to reflect any such rezoning applications that are approved.
- 5.6 For the purposes of Capital Project Contributions, as set out in Schedule G of the Zoning Bylaw, the density permitted by a land use designation in the Official Community Plan or approved secondary plan remains the density up to which the Capital Project Contribution applies. For the purposes of Community Specific Capital Project Contributions, also as set out in Schedule G of the Zoning Bylaw, the density permitted by a land use designation in the Official Community Plan or approved secondary plan remains the density above which the Community Specific Capital Project Contribution applies. For clarity, the minimum allowable density, as specified in Section 5.2, does not impact the calculation of community amenity contributions.

6 EXEMPTIONS AND LIMITATIONS

- 6.1 As described above, the MD Framework applies to land within a TOA that is zoned to permit a residential use. However, a lot zoned to permit a residential use that is ancillary to an agricultural or industrial use and where no other residential use is permitted is exempt from the MD Framework.
- 6.2 The MD Framework within a TOA is subject to the following:
 - (a) Compliance with all Federal legislation, as amended, including without limitation, the *Fisheries Act* and the *Railway Safety Act*, and regulations under those Acts;
 - (b) Compliance with all Provincial legislation, as amended, including without limitation, the *Local Government Act*, the *Community Charter*, the *Land Title Act*, the *Riparian Areas Protection Act*, the *Transportation Act*, the *Railway Act*, the *British Columbia Railway Act*, and the *Agricultural Land Commission Act*, and regulations under those Acts;
 - (c) Compliance with all City of Surrey bylaws, policies, and regulations, as amended, including without limitation, the Official Community Plan Bylaw, Hazard Lands Development Permit Guidelines, Sensitive Ecosystem Development Permit Guidelines, the Surrey Zoning Bylaw, the Surrey Subdivision and Development Bylaw, the Surrey Sign Bylaw, the Surrey Building Bylaw, and the Surrey Tree Protection Bylaw;
 - (d) Compliance with subdivision requirements of the Approving Officer;

- (e) Compliance with heritage designation bylaws and heritage revitalization agreements except that a heritage designation bylaw adopted by December 7, 2023 may not restrict or prohibit small housing density; and
- (f) Compliance with statutory and restrictive covenants registered in the Land Title Office in favour of the City.

Figure 16a: Scott Road Station Transit-Oriented Area

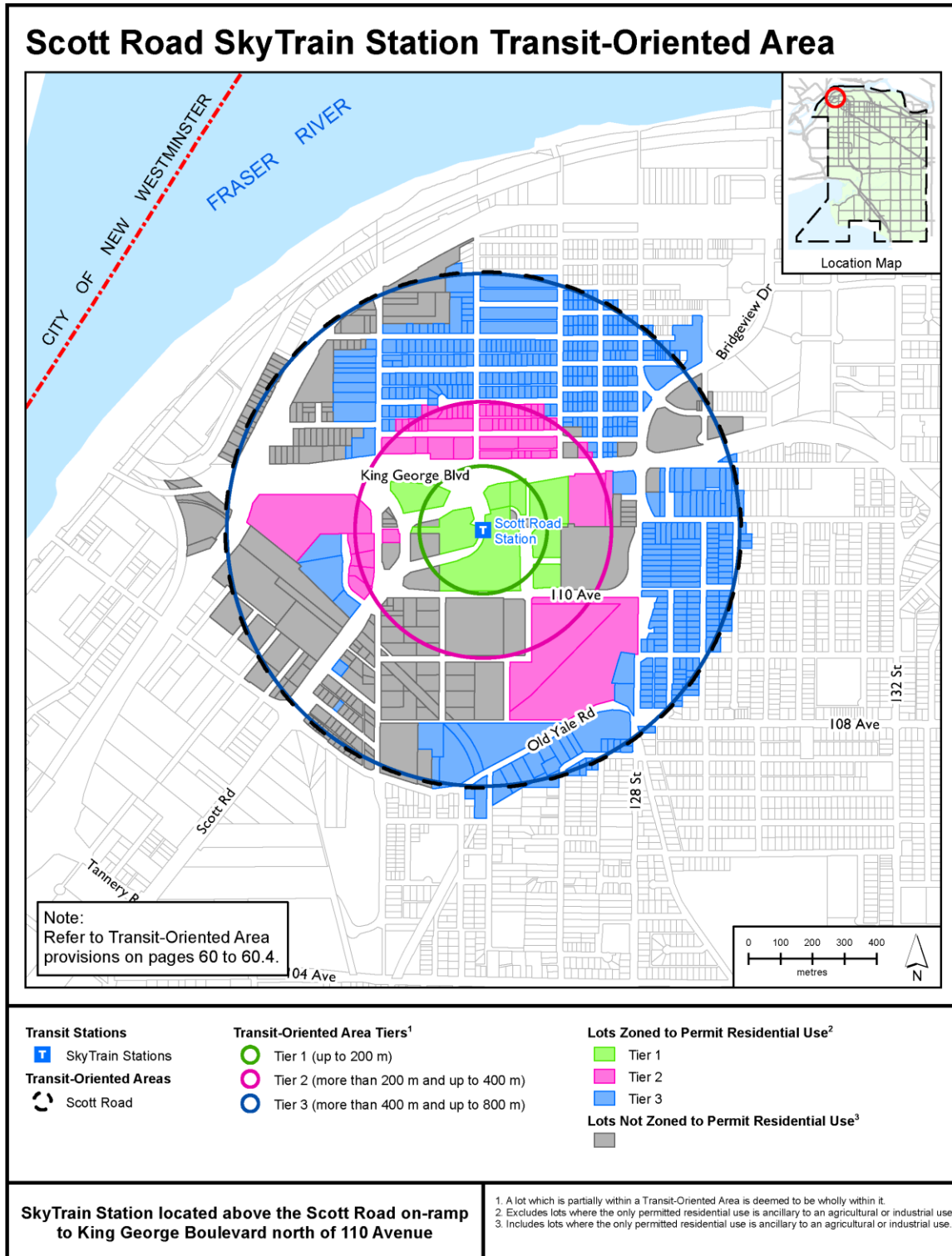


Figure 16b: Gateway Station Transit-Oriented Area

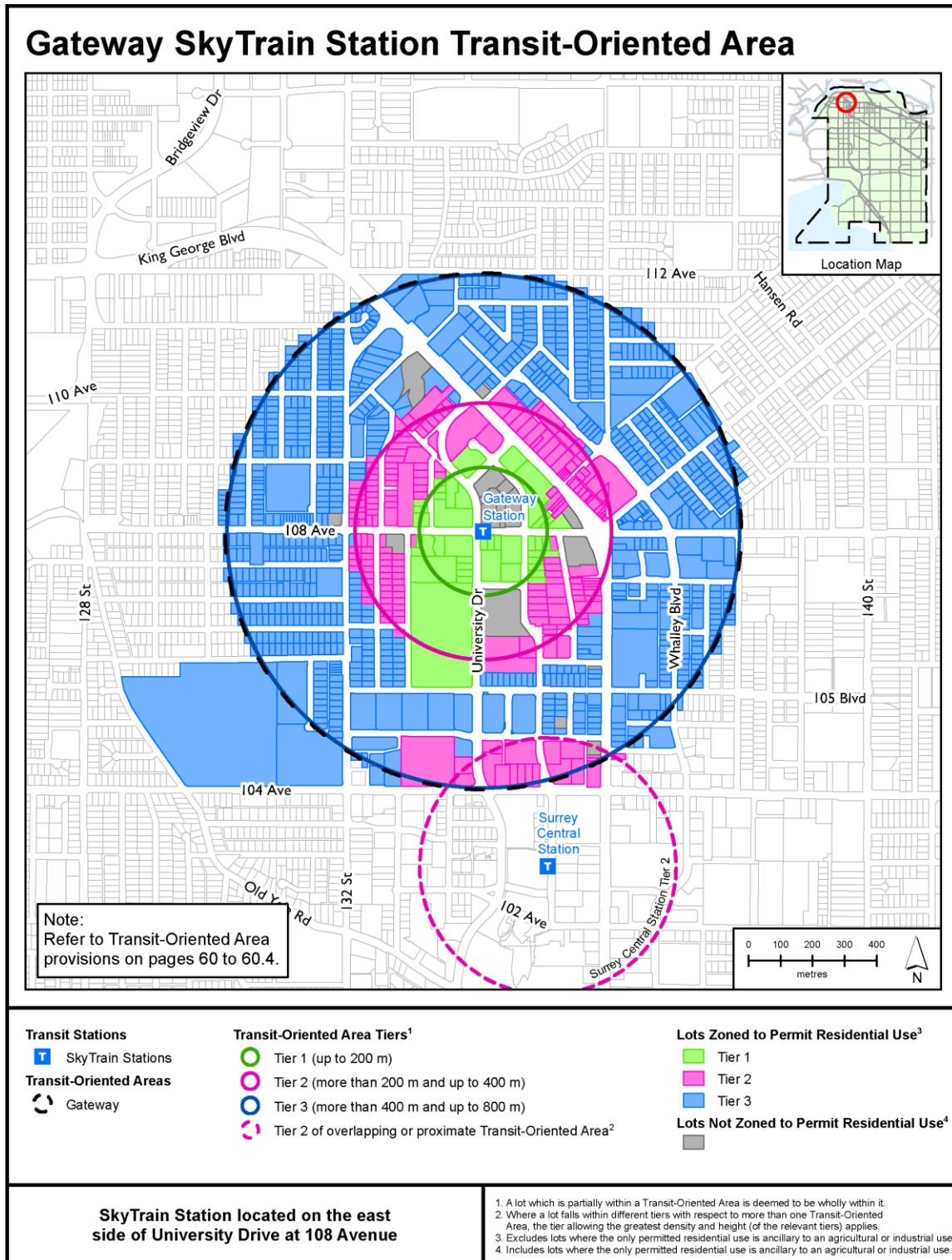


Figure 16c: Surrey Central Station Transit-Oriented Area

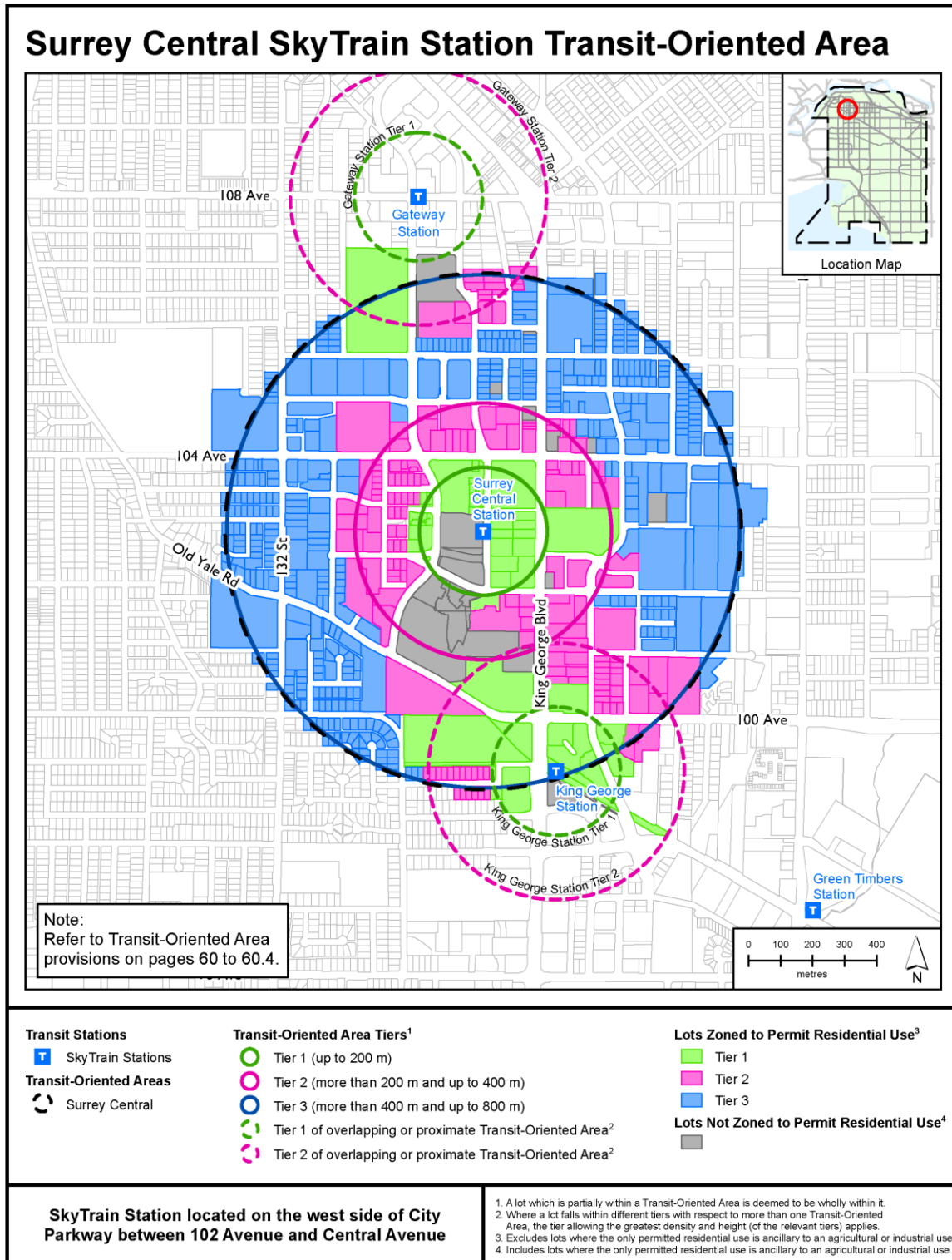


Figure 16d: King George Station Transit-Oriented Area

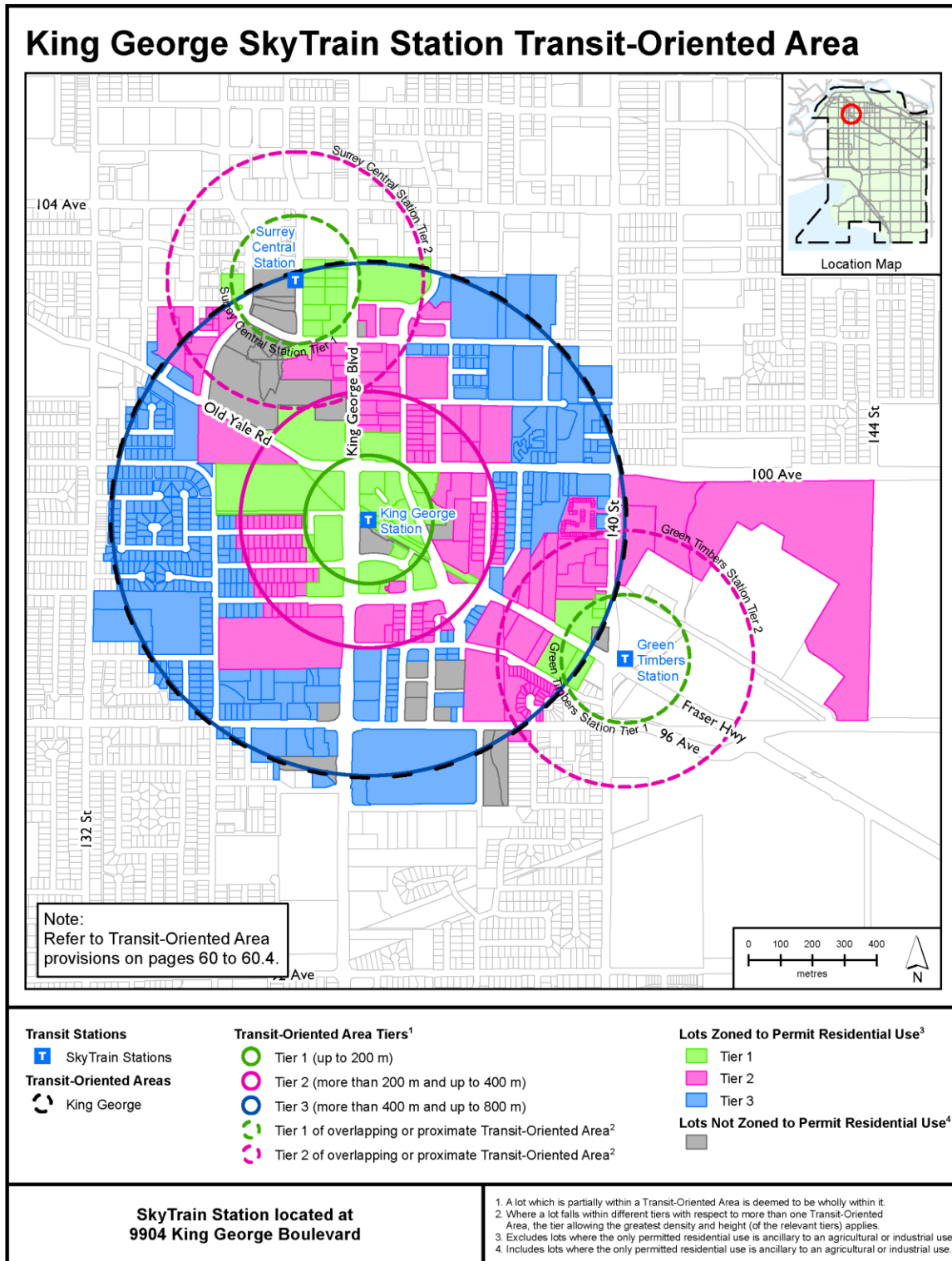


Figure 16e: Green Timbers Station Transit-Oriented Area

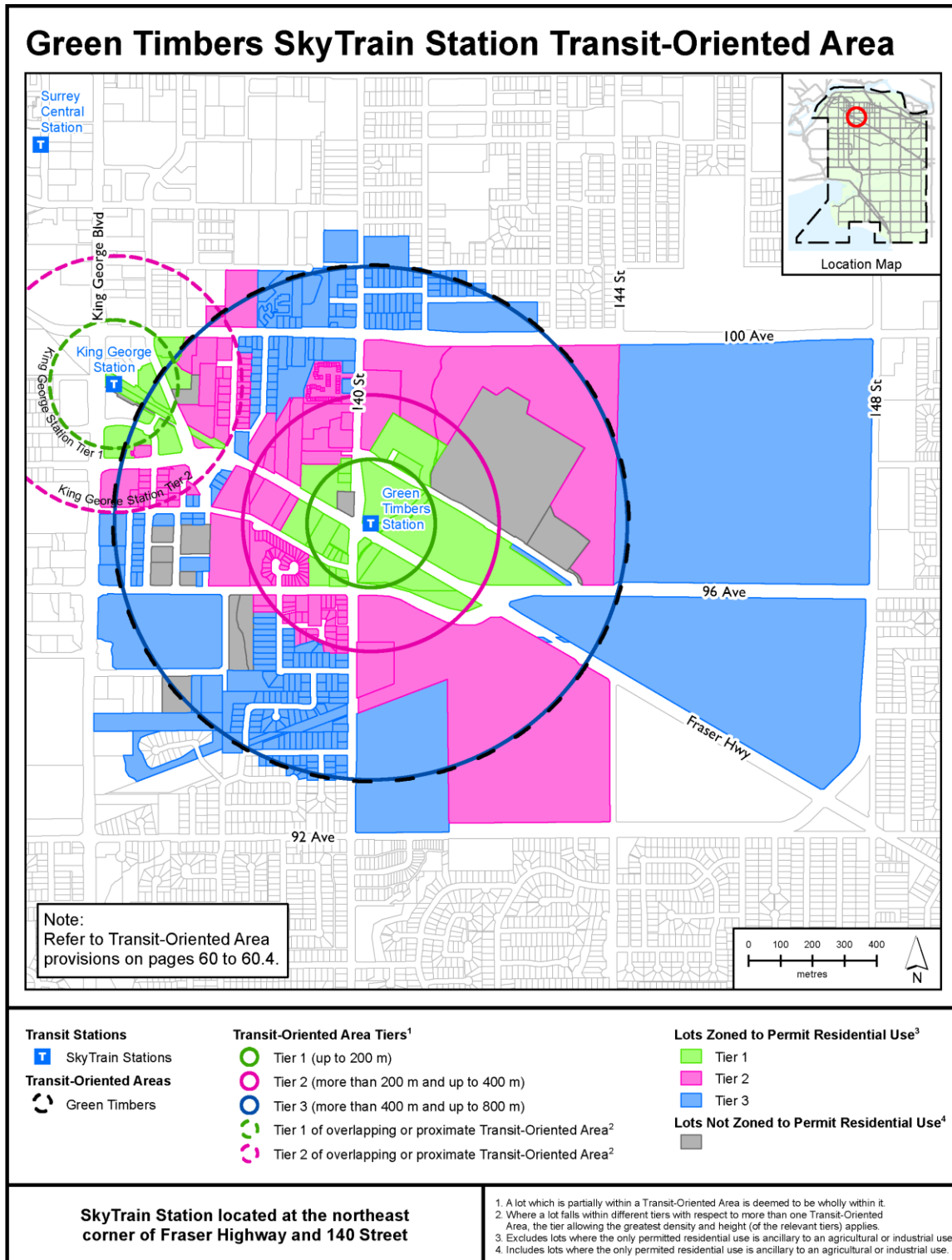


Figure 16f: 152 Street Station Transit-Oriented Area

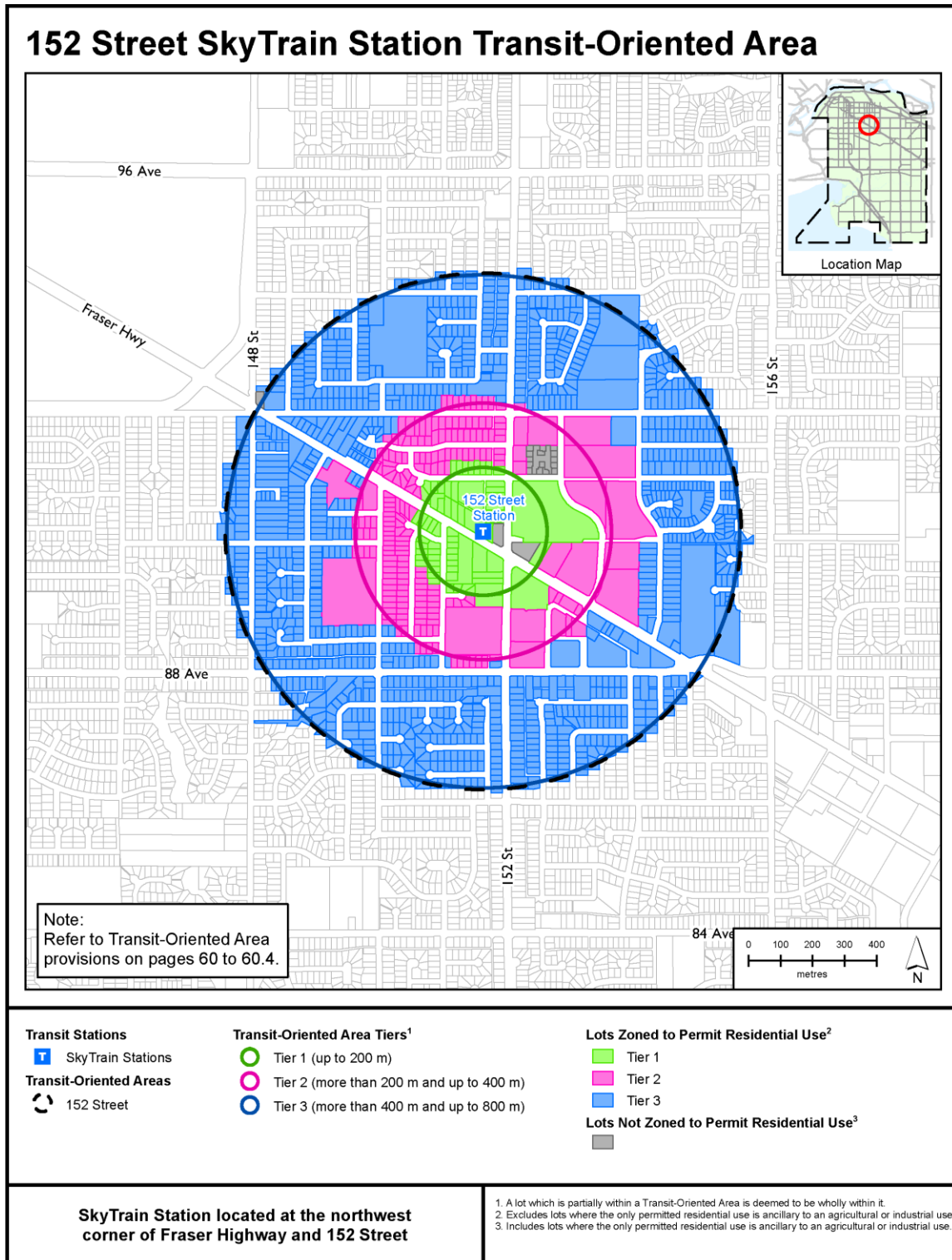


Figure 16g: Fleetwood Station Transit-Oriented Area

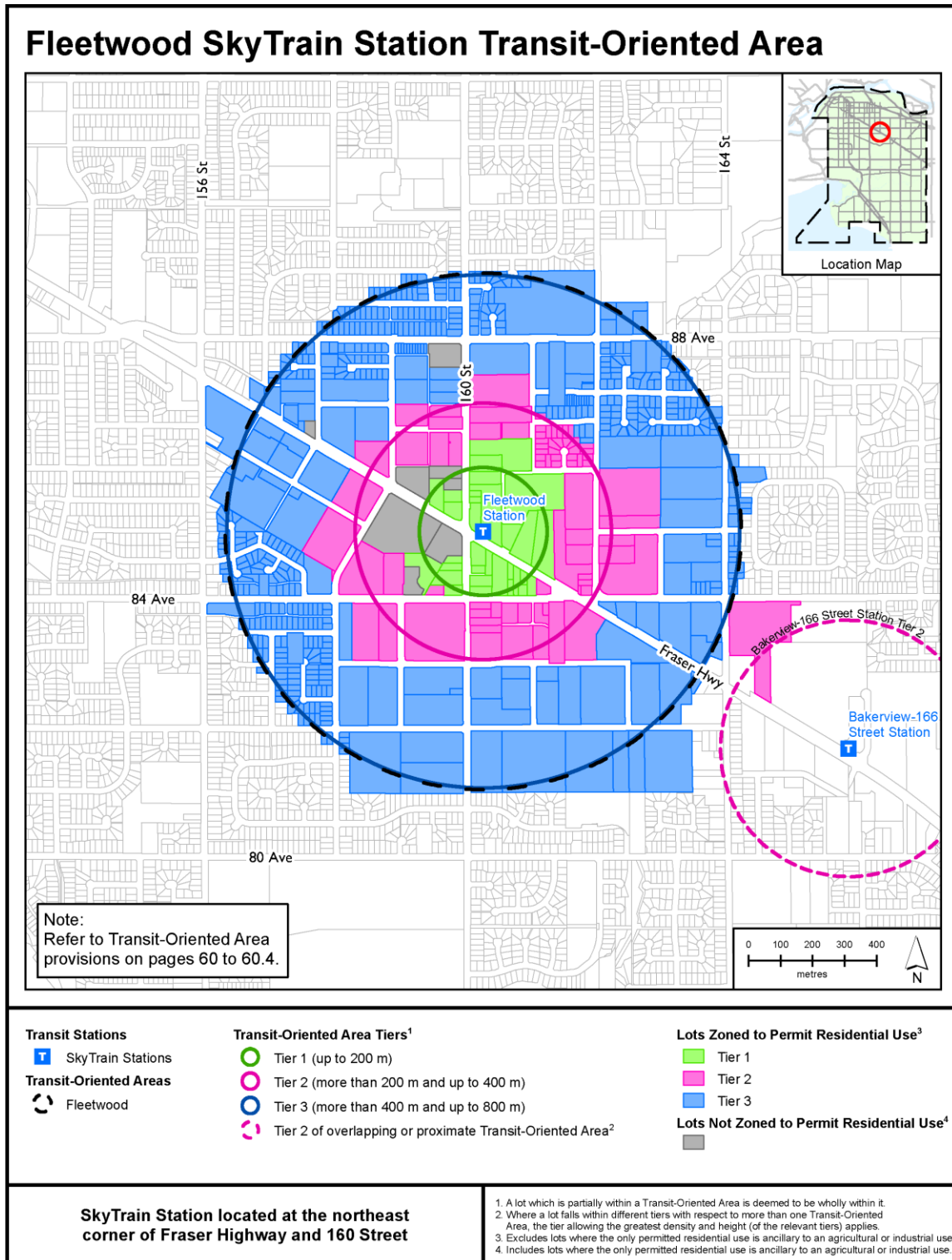


Figure 16h: Bakerview-166 Street Station Transit-Oriented Area

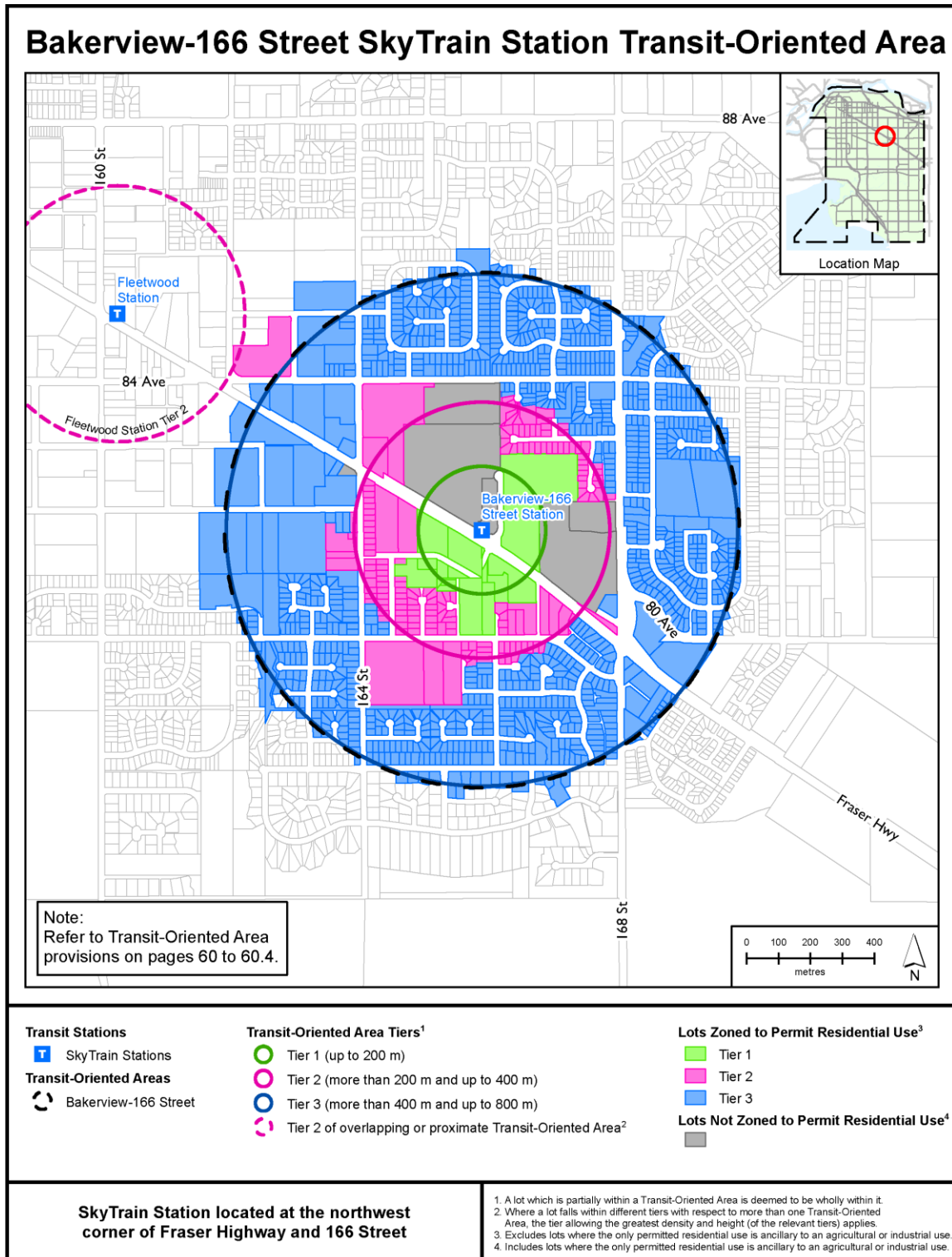


Figure 16i: Hillcrest-184 Street Station Transit-Oriented Area

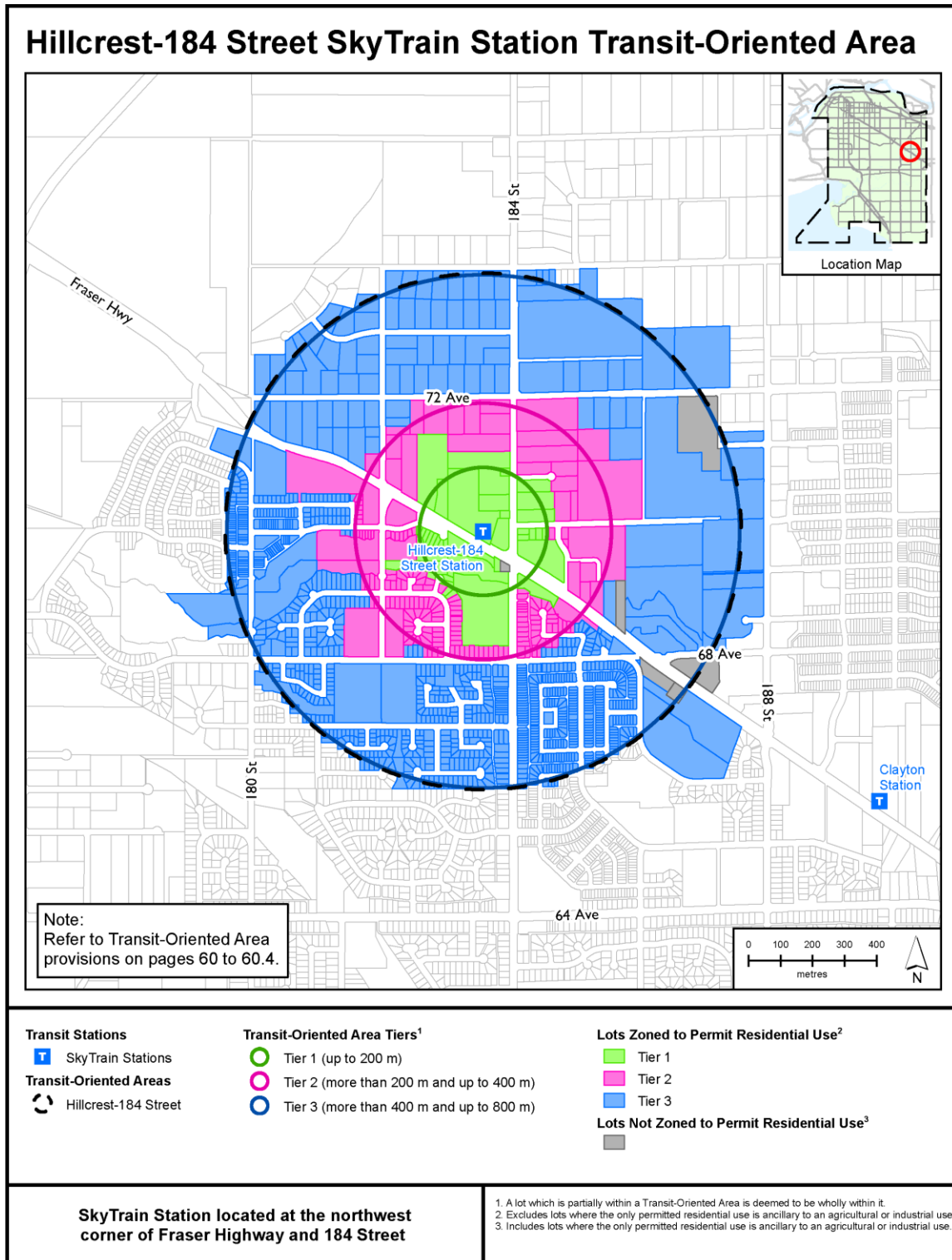


Figure 16j: Clayton Station Transit-Oriented Area

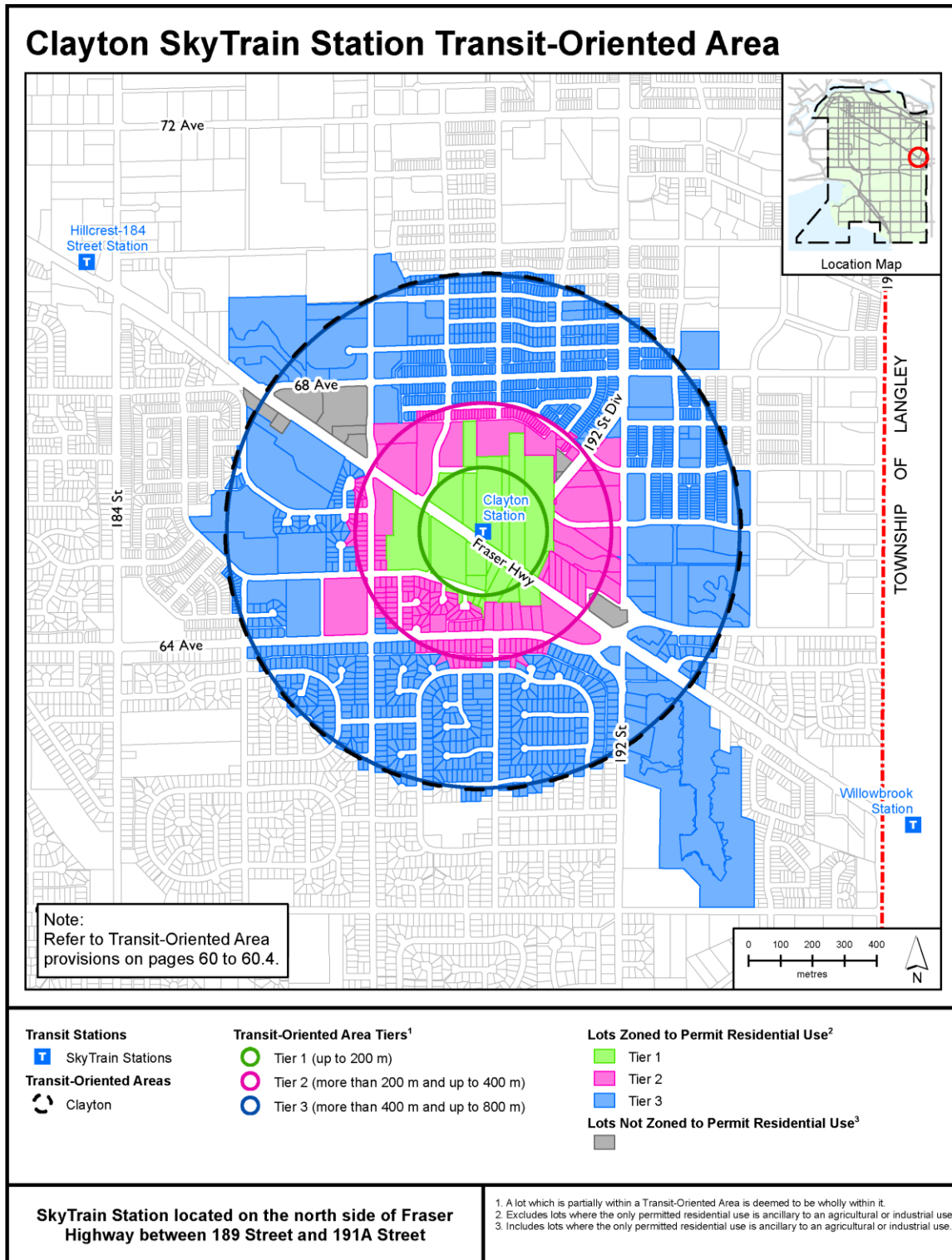


Figure 16k: Willowbrook Station Transit-Oriented Area

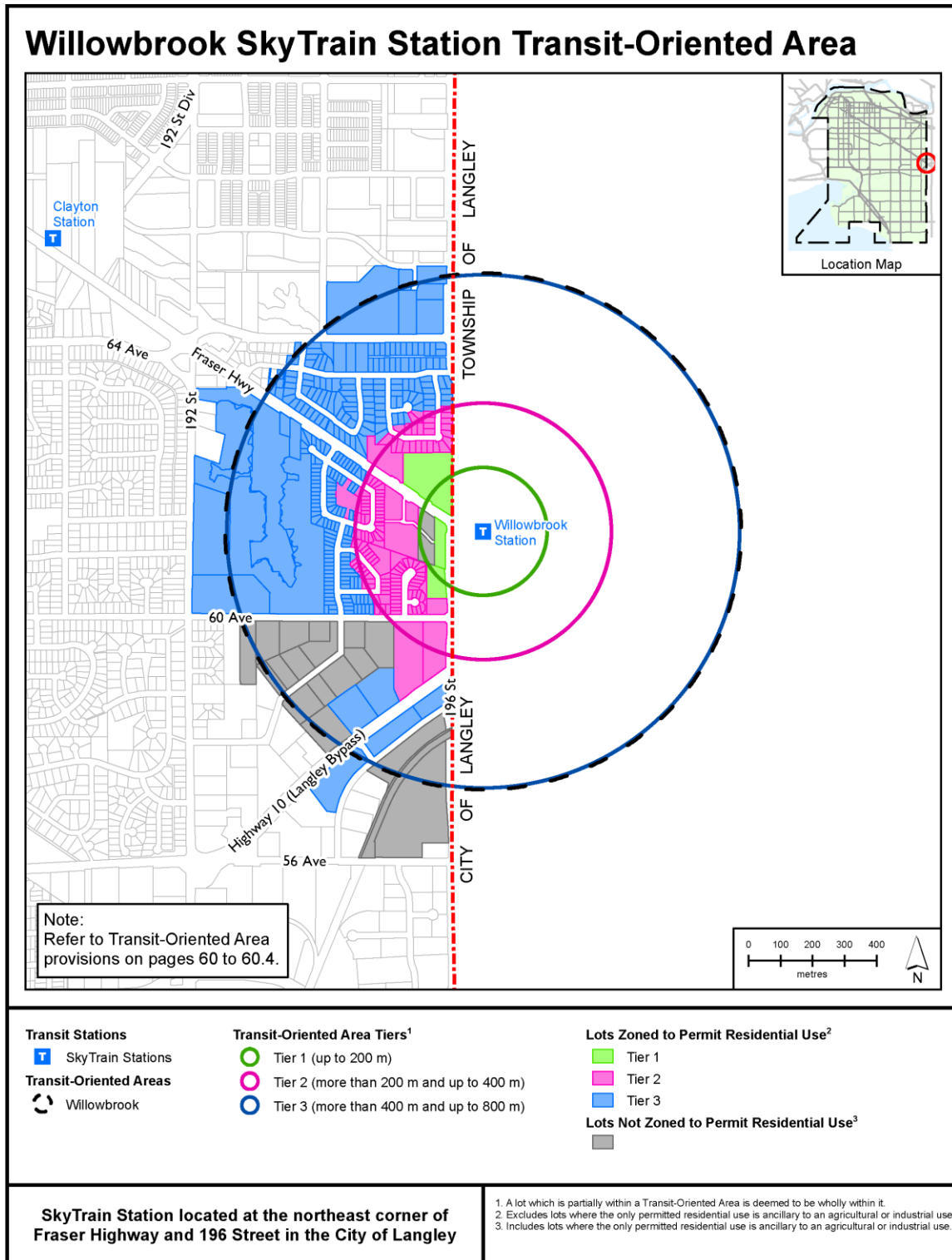


Figure 16l: Columbia Station Transit-Oriented Area

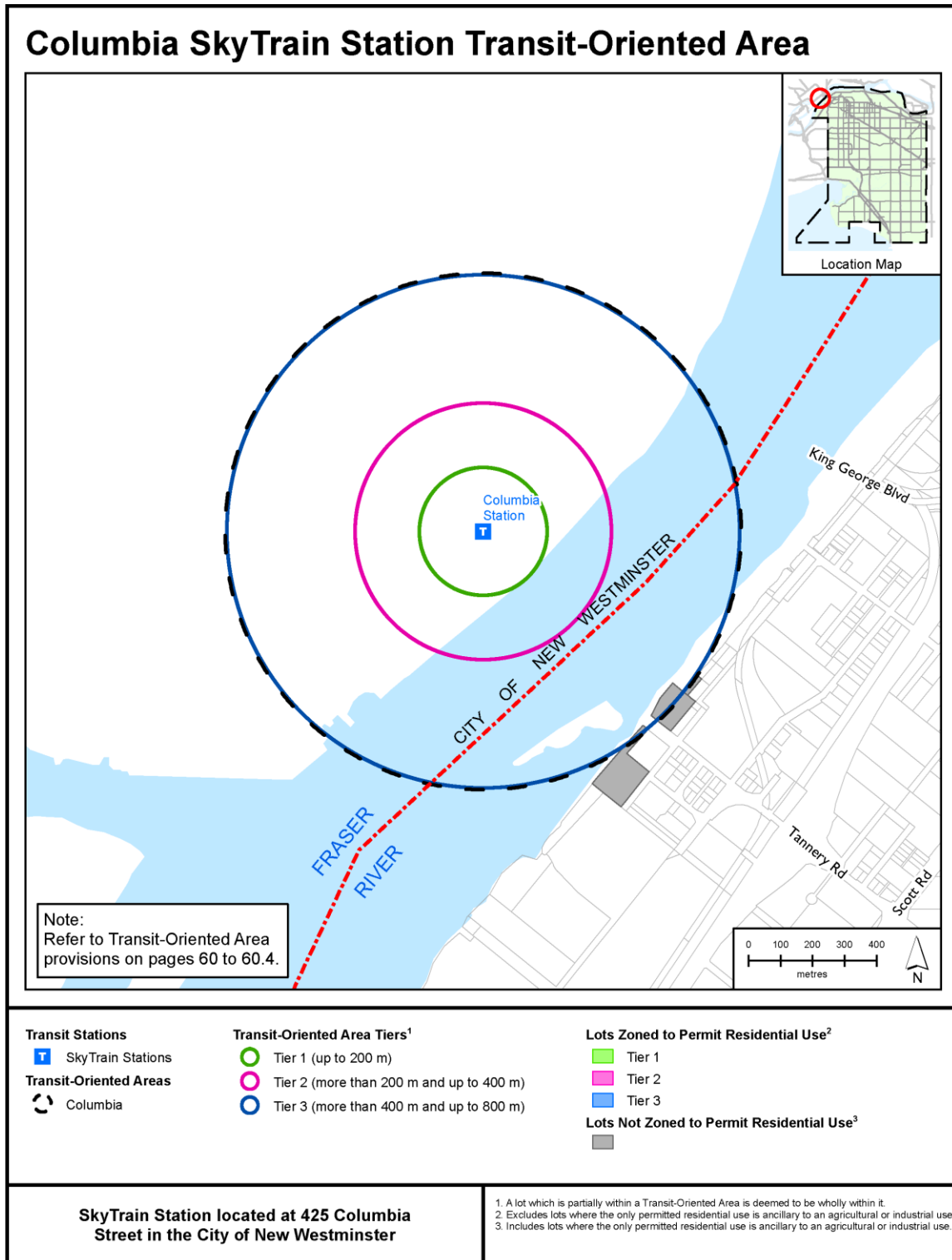


Figure 16m: Guildford Mall Exchange Transit-Oriented Area

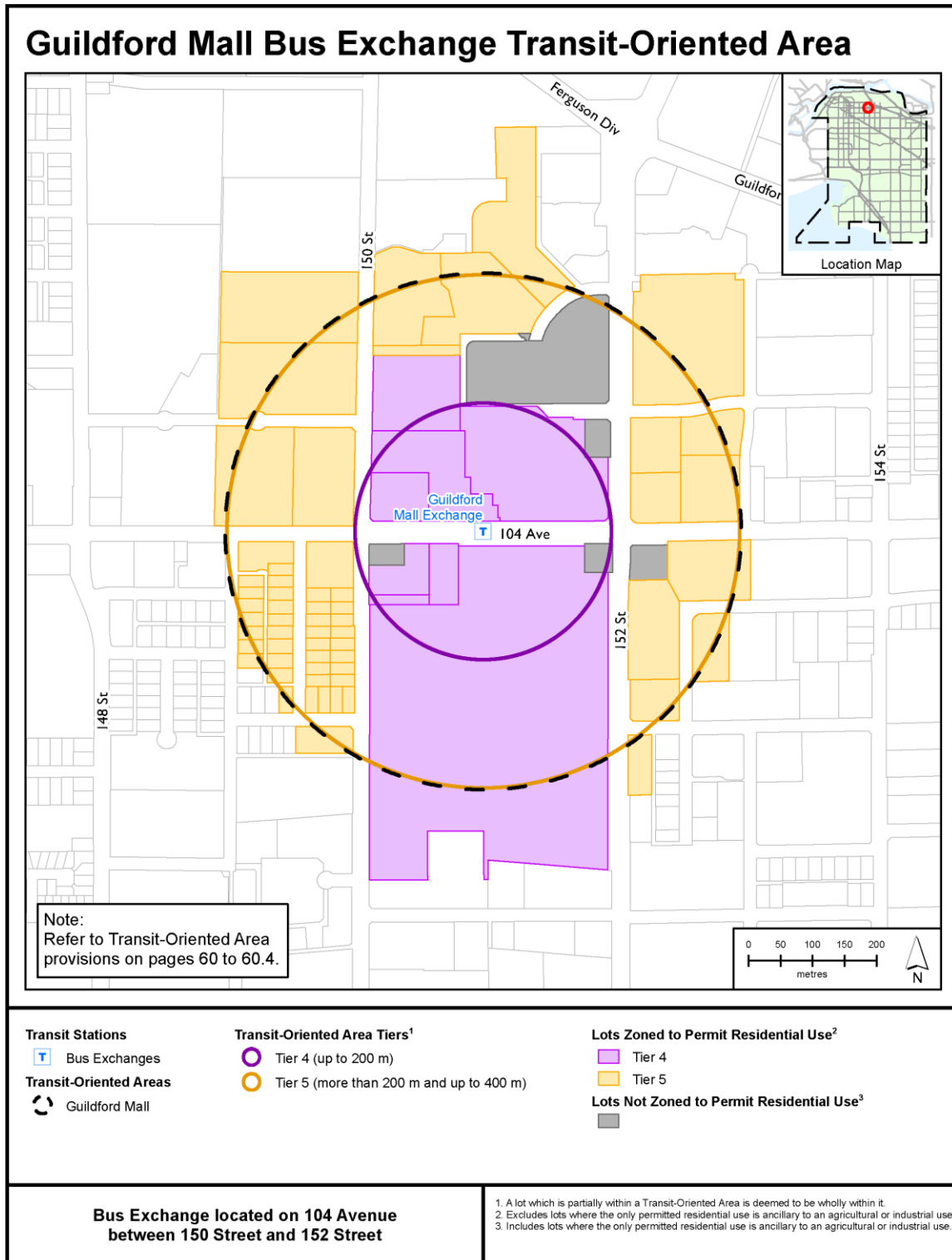


Figure 16n: Newton Exchange Transit-Oriented Area

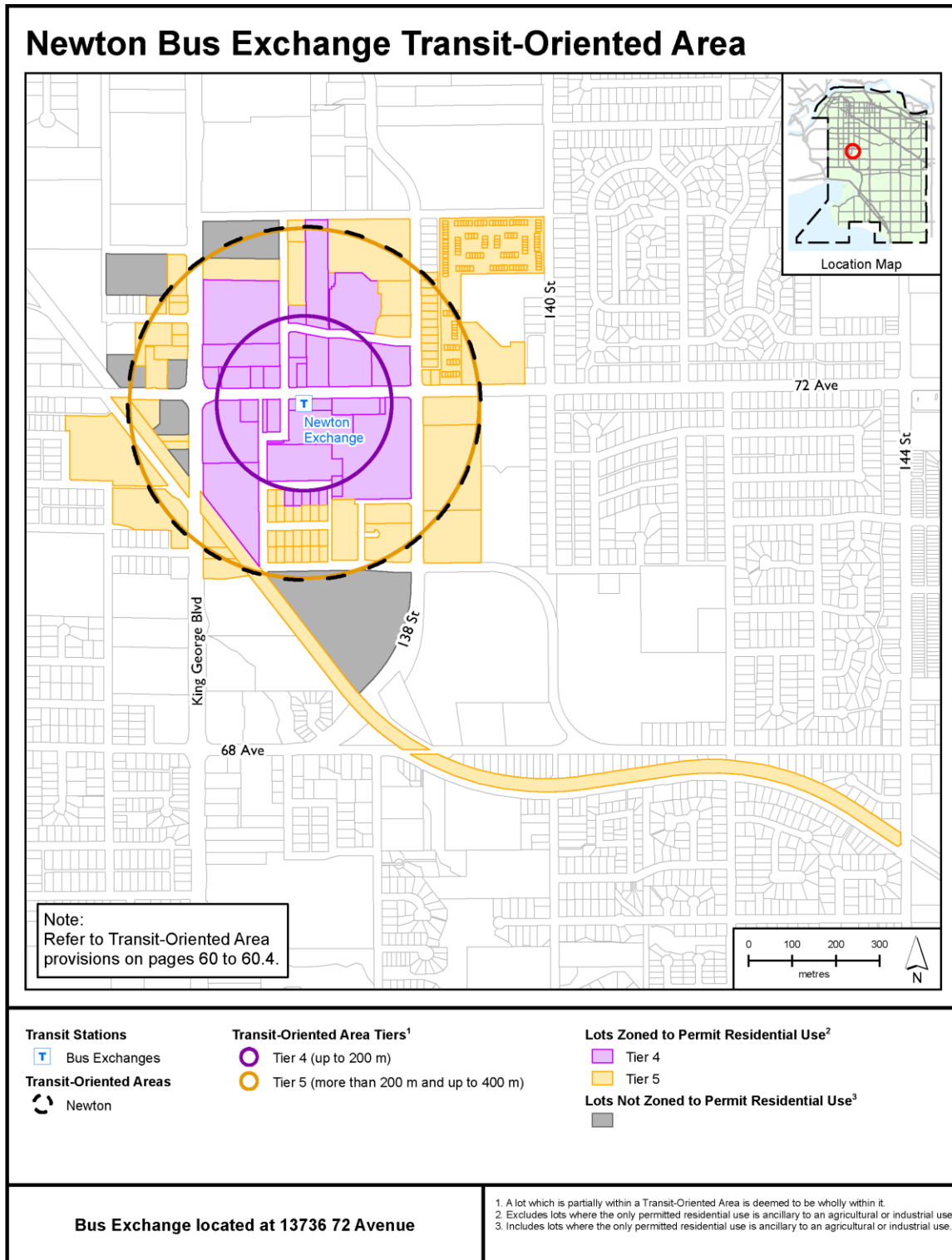
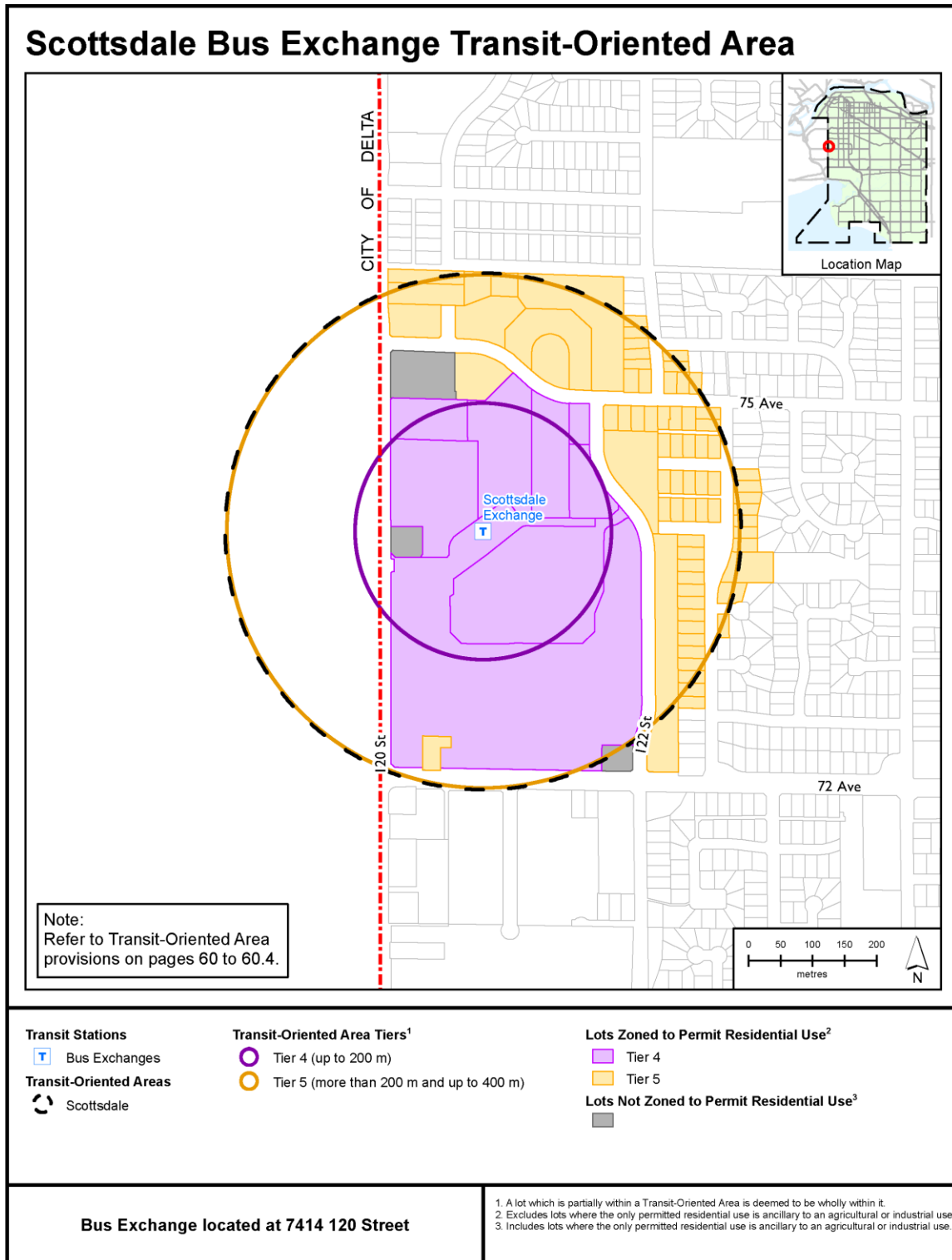


Figure 16o: Scottsdale Exchange Transit-Oriented Area





POLICIES





THEME A: GROWTH MANAGEMENT



THEME A: Introduction



The City of Surrey has experienced significant and steady growth in both population and employment over the past several decades. It is expected that this level of growth will continue through the 30-year planning horizon of this Official Community Plan fueled by strong regional in-migration and by the attractiveness and relative availability and affordability of land for urban development.

Current population projections for Surrey indicate the population growing to 770,200 by the year 2041. This represents an increase of approximately 296,600 people over the next 30 years. Employment is expected to reach 296,600 jobs by the year 2041, an increase of approximately 125,400 over total jobs available today. Managing and directing this expected growth in the most sustainable manner possible is a key goal of this Official Community Plan.

The objectives in Theme A that guide the City's growth management policies include directing high-density residential and commercial growth to the City Centre, Town Centres and Frequent Transit Corridors (see Figure 22), ensuring the efficient and integrated development of new neighbourhoods and encouraging sensitive infill development in appropriate locations.

THEME A: Objectives

1. Establish priorities for accommodating population and employment growth.
2. Direct higher-density growth into Surrey's City Centre, Town Centres and Frequent Transit Corridors.
3. Carefully plan new neighbourhoods for the efficient and sensitive use of urban land.
4. Encourage infill development that is compatible with existing neighbourhoods.

THEME A: GROWTH MANAGEMENT

Direct and Manage Population and
Employment Growth Sustainably

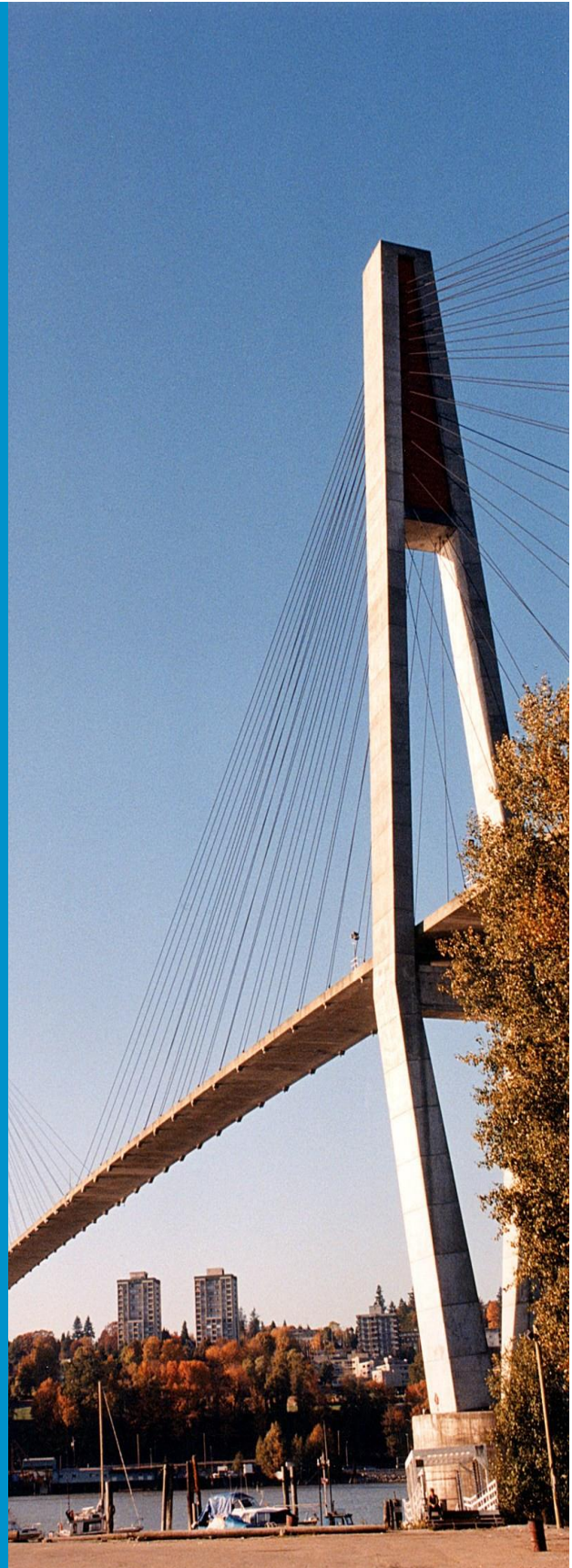
Sections

A1 Growth Priorities

A2 Accommodating Higher
Density

A3 Sensitive Infill

A4 Efficient New
Neighbourhoods



A1

Growth Priorities

BL 19519
BL 19801

Establish priorities for accommodating population and employment growth

A planned, compact form of urban development will accommodate Surrey's projected residential and business growth in a manner that: encourages the efficient use of public infrastructure and community amenities; reduces urban sprawl and pressure on agricultural land and natural habitat; and reduces energy consumption and greenhouse gas emissions. By establishing growth management priorities, the City is able to guide its growth in the most sustainable manner possible and encourage compact and efficient development.

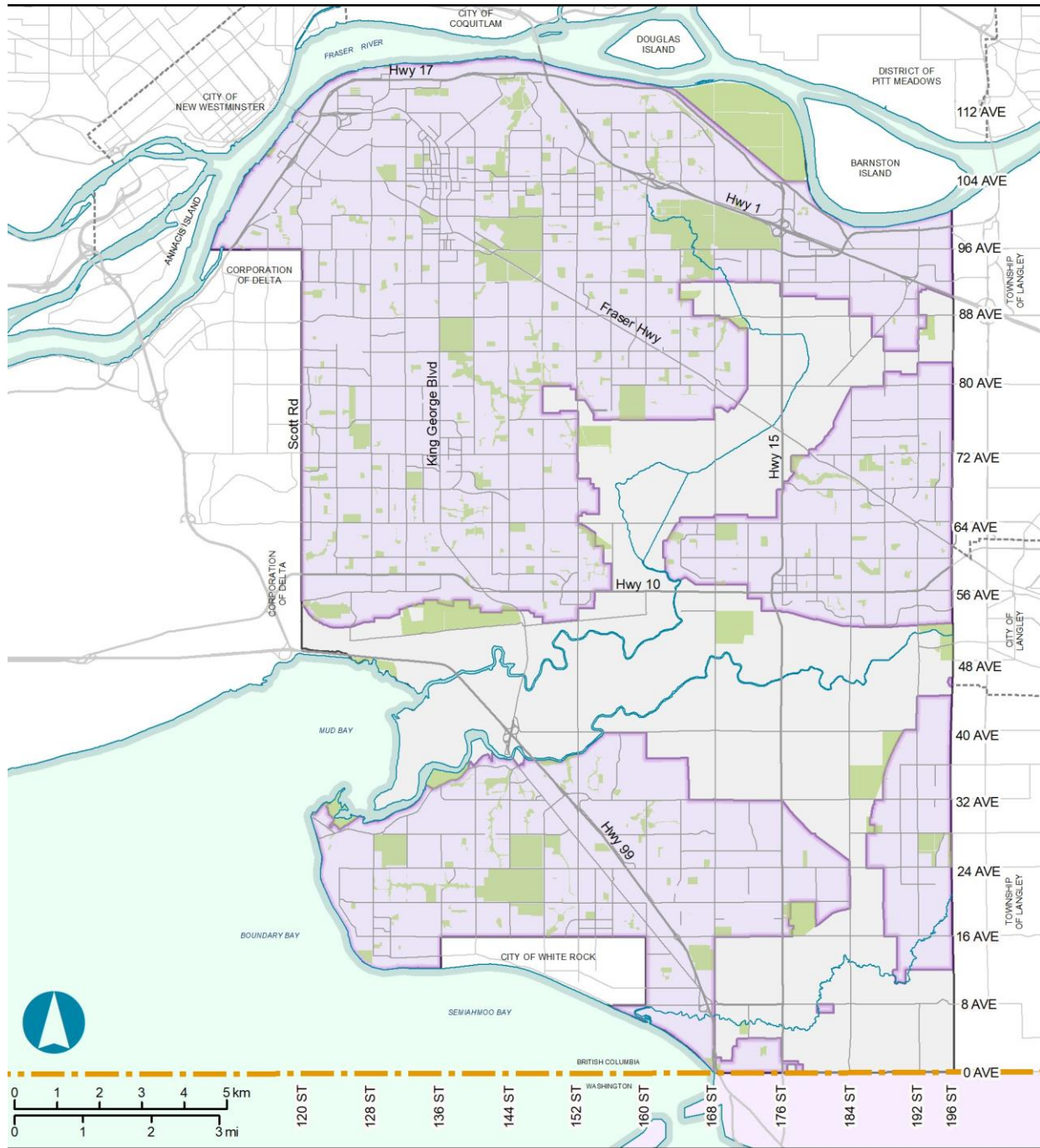
In order to meet Objective A1, the following policies apply:

A1 POLICIES: General


- A1.1 Support compact and efficient land development that is consistent with the Metro Vancouver *Regional Growth Strategy* (RGS) (2011).
- A1.2 Ensure that urban development occurs within the Urban Containment Boundary shown in Figure 17.
- A1.3 Accommodate urban land development according to the following order of growth management priorities:
 - a. City Centre, Town Centre, Urban Centre, Frequent Transit Development Area and Skytrain Corridor Planning Area locations well-served by local services, infrastructure and transit
 - b. Vacant or under-developed commercial, mixed-use and multi-family locations in existing urban areas, particularly along transit corridors and areas well-served by existing community amenities and infrastructure
 - c. Serviced infill areas and redevelopment sites in appropriate locations within existing residential neighbourhoods, when developed compatibly with existing neighbourhood character (see Figure 8)
 - d. Comprehensively-planned new neighbourhoods within approved Secondary Plan areas.
- A1.4 Strongly discourage applications for urban expansion into the Agricultural Land Reserve (ALR), consistent with policies outlined in Section E3 of this Official Community Plan.
- A1.5 Use the provision of municipal services and utilities as a means of supporting the growth management priorities, land use policies and community goals outlined in this Official Community Plan.
- A1.5a Support Frequent Transit Corridors, Frequent Transit Development Areas (FTDA) and Skytrain Corridor Planning Areas (Figure 22) as priority development areas for Surrey to accommodate an increased proportion of density and growth.

Figure 17: Surrey Urban Containment Boundary

BL 20393



LEGEND

 Metro Vancouver Urban Containment Boundary

BL 18787
BL 19519
BL 19801

A2 Accommodating Higher Density

Direct higher-density development into Surrey’s City Centre, Town Centres and Frequent Transit Corridors

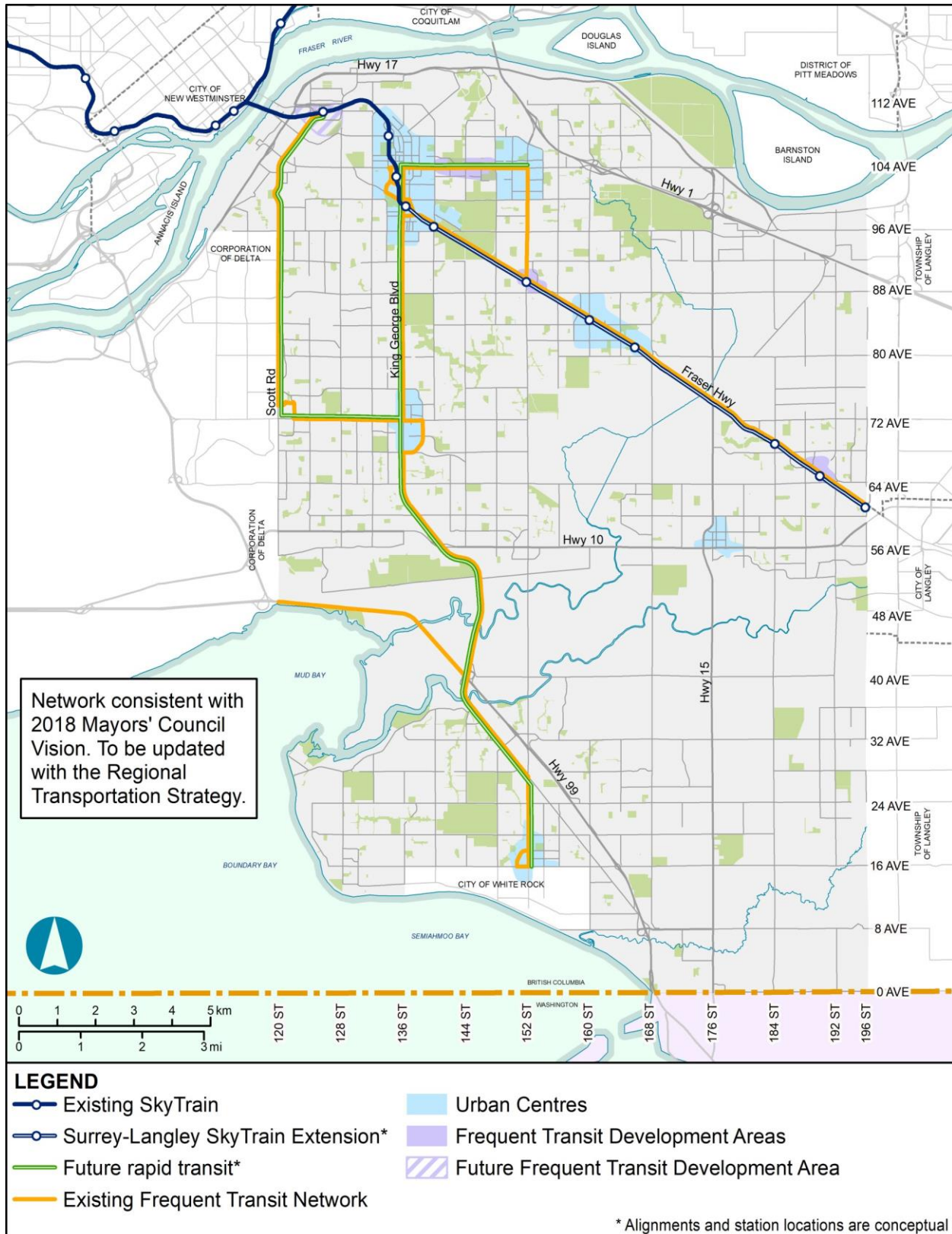
As Surrey responds to the challenges of urban growth, climate change and demographic shifts, it will become increasingly important to align land uses and development densities with significant and high-quality public transit investments. Directing higher-density residential and commercial development into Surrey’s City Centre, Town Centres and key locations along Frequent Transit Corridors creates opportunities for creating more vibrant and walkable centres, enhanced transit services and a critical mass of high-quality spaces and amenities.

In order to meet Objective A2, the following policies apply:

A2 POLICIES: General

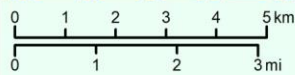
- A2.1 Direct residential and mixed-use development into Surrey’s City Centre, Town Centres, Urban Centres, Skytrain Corridor Planning Areas along Frequent Transit Corridors (see Figure 18) and in approved Secondary Plan areas, at densities sufficient to encourage commercial development and transit service expansion.
- A2.2 Direct major, regional-serving, trip-generating commercial and retail centres and major institutional uses and facilities (e.g. health care, post-secondary education, government, recreation and culture) to locate within City Centre and along Frequent Transit Development Areas (FTDA) and not within Mixed Employment, or other areas.
- A2.3 Direct community-serving commercial and retail centres and institutional uses to locate within Town Centres in order to maximize their accessibility to the public.
- A2.4 Direct stand-alone office uses to locate within City Centre, Town Centres and in appropriate Frequent Transit Corridors where they are accessible by transit. Limit large-scale office development in employment areas where there is no easily accessible transit.
- A2.5 Concentrate high-rise buildings taller than six storeys to within Surrey’s City Centre and Town Centres, consistent with approved Secondary Plans.
- A2.6 Support the redevelopment of Frequent Transit Corridors that fall outside of Town Centres to a higher-density; concentrate these developments within Frequent Transit Development Areas (FTDA), Skytrain Corridor Planning Areas and within 400 metres of existing or planned Rapid Transit stops, as shown in Figures 18 and 22.
- A2.6a Ensure redevelopment along Frequent Transit Corridors, Frequent Transit Development Areas (FTDA) and Skytrain Corridor Planning Areas (Figure 22) are required to be sufficient enough to support rapid transit infrastructure investments.

Figure 18: Centres and Frequent Transit Areas and Corridors



BL 19801

Network consistent with 2018 Mayors' Council Vision. To be updated with the Regional Transportation Strategy.



LEGEND

- Existing SkyTrain
- Surrey-Langley SkyTrain Extension*
- Future rapid transit*
- Existing Frequent Transit Network
- Urban Centres
- Frequent Transit Development Areas
- Future Frequent Transit Development Area

* Alignments and station locations are conceptual

A3

Sensitive Infill

BL 20231

Encourage infill development that is compatible with existing neighbourhoods

Infill development includes “filling in” vacant sites as well as replacing aging or under-utilized development within established neighbourhoods. Infill development, particularly in areas adjacent to urban centres or major transit corridors, is an important component of accommodating Surrey’s growth in a sustainable manner; however, the nature, scale and character of infill development must be carefully managed and designed so that it contributes positively to an established neighbourhood context.

In order to meet Objective A3, the following policies apply:

A3 POLICIES: General

- A3.1 Permit gradual and sensitive residential infill (see Figure 19) within existing neighbourhoods, particularly in areas adjacent to Town Centres, neighbourhood centres and transit corridors, in order to support significant transit improvements, utilize existing transportation infrastructure and implement improvements to the public realm.
- A3.2 Encourage the development of remaining vacant lands in urban neighbourhoods to utilize existing infrastructure and amenities and to enhance existing neighbourhood character and viability.
- A3.3 Require redevelopment and infill development to contribute to neighbourhood connectivity and walkability and to enhance public open spaces and greenspaces within existing neighbourhoods.
- A3.4 Retain existing trees and natural and heritage features in existing neighbourhoods, where possible, in order to preserve neighbourhood character and ecology.
- A3.5 Support infill development that is appropriate in scale and density to its neighbourhood context and that uses compatible design to reinforce neighbourhood character. Specifically, support including secondary suites into Single Family Zones where the size, depth and width of a lot supports the additional parking requirements of a secondary suite.
- A3.6 Encourage innovative housing and buildings including affordable units, energy-efficient and water-conserving designs and innovative waste reduction and sustainability features that are compatible with the scale and context of existing neighbourhoods.

A3 POLICIES: General (cont.)

- A3.7 Encourage local neighbourhood commercial centres and associated local gathering places to appropriately locate within existing neighbourhoods in order to increase walking and cycling options and contribute to neighbourhood character.
- A3.8 Seek partnerships with other governments and public and non-profit agencies, including School District No. 36, to enhance community facilities and meeting places within existing neighbourhoods.
- A3.9 Conduct neighbourhood planning processes with local residents to determine the appropriate density, scale, transition design, transportation improvements, style and character of infill development within each neighbourhood.



Appropriate design, material selection and building scale are all elements of good infill development.

Figure 19: Defining Sensitive Infill

Many planning documents, including this Official Community Plan, identify sensitive infill as an important element in accommodating future growth within existing residential urban neighbourhoods. Infill development is the process of developing vacant or under-used parcels within existing urban areas that are already largely developed. Defining “sensitive” can be difficult as it is community-specific and often reflects the values, design elements and visual identity held by existing residents. Although these values and identifiable qualities can change over time, generally speaking, sensitive infill uses the following development elements:

1. Density Increases—tend to be small and not overly dramatic (e.g. allowing multiple secondary suites or small clustered townhouse developments in single family neighbourhoods rather than large mass, multi-storey buildings)
2. Housing Forms—tend to be similar in style and form (e.g. developing larger family homes with multiple units in a single family neighbourhood rather than small apartment buildings).
3. Designs and Materials—tend to be similar in style with consistency in building materials (e.g. continued use of similar roof pitches, windows, colour palettes and finishing materials).
4. Site Transitions—tend to transition building massing on the edges of development (e.g. stepping-down building edges to meet the heights of adjacent existing structures).



Before and after graphics showing sensitive infill development in a single family neighbourhood. Image credit: Ron Walkey, from City of Surrey Sensitive Urban Infill Design Charrette Project Report

A4 Efficient New Neighbourhoods

Carefully plan new neighbourhoods for the efficient and sensitive use of urban land

New suburban neighbourhoods are anticipated in approved General Land Use Plans for specific areas of Surrey. These areas are within the Urban Containment Boundary set out in the Metro Vancouver *Regional Growth Strategy* and are an important part of the Region's growth management strategy. As land for new urban areas becomes more limited, it is increasingly important that development is carefully planned to be compact, to use urban land and public infrastructure efficiently, to preserve agriculture, and to encourage healthy lifestyles and transportation choices.

In order to meet Objective A4, the following policies apply:

A4 POLICIES: General

A4.1 Plan and develop new neighbourhoods with an emphasis on compact forms of development that:

- effectively utilize land, public infrastructure and City resources
- enhance neighbourhood quality
- reduce development pressures on agricultural and environmentally sensitive lands.

A4.2 Encourage the full and efficient build-out of existing planned urban areas in order to:

- achieve planned capacities
- balance residential and business development
- support high-quality public transit investments
- use infrastructure efficiently
- provide housing options
- create local commerce and workplaces
- provide amenities for residents.

A4.3 Prevent urban development as well as the extension of City services that would encourage subdivision in rural and suburban areas, except in accordance with approved Secondary Plans, or in Local Improvement Areas.



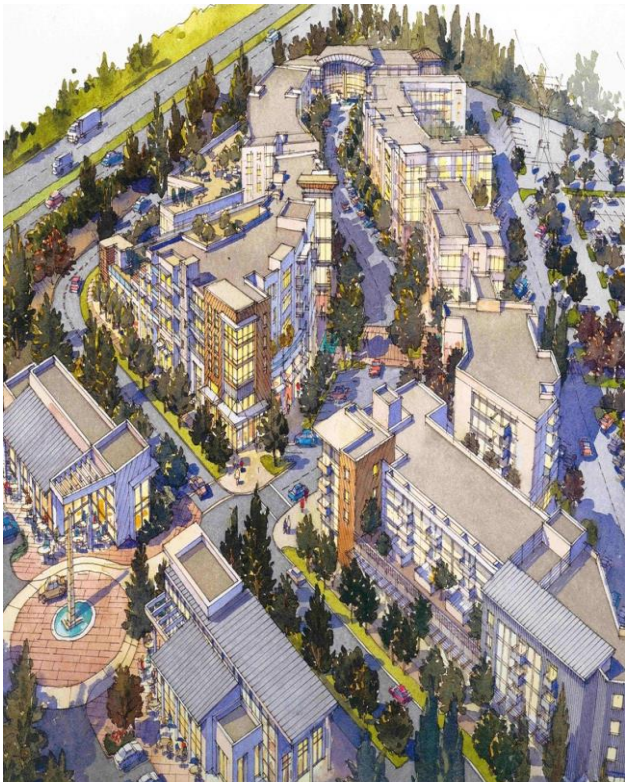
New residential development in Surrey.

A4 POLICIES: Secondary Plan Areas

- A4.4 Plan new urban neighbourhoods, in a sequential manner, through Neighbourhood Concept Plan (NCP) processes in areas with approved General Land Use Plans, including the Grandview Heights, Clayton and South Port Kells areas, as identified in Figure 7.
- A4.5 Conduct appropriate background studies in proposed NCP areas that examine issues and topics such as environmental and heritage resources, integrated stormwater management, servicing and traffic impacts, market analysis and other relevant information to ensure their effective consideration of the Neighbourhood Concept Plan process.
- A4.6 Support the initiation of new Neighbourhood Concept Plan processes where the following criteria can be demonstrated:
- existing approved NCPs within the subject General Land Use Plan area can accommodate less than five years of projected residential growth capacity
 - there is a demonstrated demand for new housing in a specific area of the City
 - there is demonstrated support for initiating an NCP, by affected land owners, consistent with the procedures outlined in the Implementation Section of this Official Community Plan
 - major infrastructure systems may be reasonably extended, including transit, in a logical and cost effective manner to service the proposed NCP area.
- A4.7 Engage the community in local planning processes through public meetings and Citizen Advisory Committees (CAC) made up of citizens representing the interests of local land-owners, surrounding neighbourhoods and the community-at-large.

A4.8 Integrate servicing and financial strategies into Secondary Plans to ensure there are sufficient infrastructure services and development revenues to support the planned land uses and community amenities that are required for new development, on a “development pays” basis.

A4.9 Receive and process land development applications for rezonings, Development Permits or subdivisions only after Stage 1 of a Neighbourhood Concept Plan is approved. Final approval of development applications that are consistent with the Neighbourhood Concept Plan will only be considered after Stage 2 of the NCP is approved and only as is consistent with the procedures outlined in the Implementation Section of this Official Community Plan.



Digital rendering of a proposed development within an Neighbourhood Concept Area.



**THEME B: CENTRES, CORRIDORS and
NEIGHBOURHOODS**



THEME B: Introduction



As a large and geographically diverse city, Surrey is made up of six communities focused around their own urban centres (City Centre and five Town Centres) with individual and distinct neighbourhoods and local focal points.



Surrey is committed to strengthening its City Centre and each Town Centre as the primary focus for social life, culture and commerce within each of their surrounding communities. This Official Community Plan recognizes that each Town Centre is unique and encourages each to develop their own distinctive character and community function. In addition, Surrey's City Centre is positioned as the primary social, cultural and commercial core for all of Surrey and the entire "South of the Fraser" region.



Surrey's urban centres are shaped and connected by Frequent Transit Corridors which are appropriate locations for development intended to mutually support the viability of public transit and active transportation.



The objectives contained in Theme B provide guidance on the design, development and maintenance of sustainable and liveable local neighbourhoods. Local neighbourhoods are designed to be both internally focused towards local parks, schools and amenities, while also being linked into a city-wide network of parks, greenways and transportation corridors.

THEME B: Objectives

1. Strengthen Surrey's City Centre as a dynamic, attractive and complete metropolitan core.
2. Develop each Town Centre as the distinctive social, cultural and commercial centre of its community.
3. Support transit-oriented development along major corridors linking urban centres and employment areas.
4. Build complete, walkable and green neighbourhoods.
5. Retain and enhance memorable natural and built features and places.
6. Encourage beautiful and sustainable urban design.

THEME B: CENTRES, CORRIDORS and NEIGHBOURHOODS

Strengthen Surrey's City Centre, Town Centres and Neighbourhoods into a Connected Network of Vibrant, Sustainable and Liveable Places

Sections

- B1 Dynamic City Centre
- B2 Distinctive Town Centres
- B3 Transit Corridors
- B4 Healthy Neighbourhoods
- B5 Memorable Features
- B6 Urban Design



B1

Dynamic City Centre

Strengthen Surrey’s City Centre as a dynamic, attractive and complete Metropolitan Core

Surrey’s vision for its City Centre is of a Metropolitan Core planned as a primary focus for employment, services, higher-density housing, commercial, cultural, entertainment, mixed and institutional uses that is supported by an integrated rapid transit system.

A strong, positive image will act as a catalyst for attracting greater attention to Surrey as a place to invest and do business, which will benefit the Town Centres and the City as a whole. Locating higher-order public buildings, amenities and services in the downtown area, including Surrey’s City Hall, signals confidence in the City Centre’s future and will help leverage the private sector investments that provide components of a quality downtown experience for residents, workers and visitors.

In order to meet Objective B1, the following policies apply:

B1 POLICIES: General

- B1.1 Develop City Centre as the primary commercial centre for Surrey and as the Metropolitan Core for the entire ‘South of Fraser’ region. Support the highest densities of residential, commercial and mixed-use developments within City Centre, as shown within the *Surrey City Centre Plan*, as amended.
- B1.2 Build on existing strengths and amenities such as views, transportation connections, major public investments, public institutions, adjacent green spaces and natural and cultural heritage to provide a competitive business environment and distinctive and attractive Sense of Place for Surrey’s City Centre.
- B1.3 Concentrate the highest density of residential and mixed-use development within Surrey’s City Centre in areas near existing and planned rapid transit stations.
- B1.4 Densities for commercial, multi-family residential and mixed-use developments in Surrey’s City Centre (see Land Use and Density Section) may be calculated based on gross site area where the City has requested land dedication for pathways, roadways, green spaces, amenities or utility installations.



New Civic Precinct, Surrey City Centre

B1 POLICIES: Land Uses

- B1.5 Locate major institutional, cultural, social service and civic facilities in Surrey’s City Centre such as libraries, recreation facilities, visual and performing art spaces, entertainment spaces, health-related service providers and high density government offices.
- B1.6 Support the expansion of the professional and knowledge-based sectors by encouraging post-secondary educational institutions, and any secondary businesses associated with them, to locate and expand within Surrey’s City Centre.
- B1.7 Support the expansion of a medical and health precinct in Surrey’s City Centre adjacent to and around Surrey Memorial Hospital. Support the growth of new businesses and services associated with the medical and health professions.
- B1.8 Develop and expand facilities designed for conventions and entertainment in Surrey’s City Centre to support the tourism, hospitality and hotel industries.
- B1.9 Encourage the growth of major retail and office development within Surrey’s City Centre that serves the region and capitalizes on transit access and a central location.
- B1.10 Support the placement of local-serving entertainment facilities within Surrey’s City Centre, particularly movie theatres and indoor recreation venues.
- B1.11 Provide adequate and attractive public spaces and facilities to support major festivals, events and public gatherings.

B1 POLICIES: Parks and Greenways

- B1.12 Develop fully accessible major urban parks, open spaces and public plazas in Surrey’s City Centre.
- B1.13 Provide for open spaces by including new neighbourhood parks and mini-plazas within walking distance (approximately 400 m) of high density development and multi-family residences.
- B1.14 Support an enhanced public realm by creating a series of walkways and streets linked throughout the Surrey’s City Centre and to Green Timbers Urban Forest. Support active transportation opportunities and allow residents and workers to connect to nature in their neighbourhoods (e.g. BC Parkway project).
- B1.15 Augment public parks and plazas by supporting the provision of publicly-accessible outdoor spaces, such as plazas and gardens, within private sector development and in appropriate locations along public streets and walkways.



Gateway Office Tower public amenity space.

B1 POLICIES: Transit

- B1.16 Work with TransLink to focus and increase convenient, frequent and high-quality transit infrastructure and services within, to, and from, Surrey's City Centre.
- B1.17 Ensure transit infrastructure and stations contribute to a high quality urban environment by integrating development and transit station design and location.
- B1.18 Continue to support the role of Surrey's City Centre as a focus for transit services throughout the city and as a major transit hub for "South of the Fraser" communities, including connections to other parts of the Metro Vancouver region.

B1 POLICIES: District Energy

- B1.19 Continue to expand the City's District Energy system to service all new development in City Centre in an effort to improve housing quality, provide energy resiliency and reduce greenhouse gas emissions.
- B1.20 Work with private property owners to connect existing City Centre development to the City's District Energy system to reduce greenhouse gas emissions and provide existing developments with energy resiliency.
- B1.21 Require new development in Surrey's City Centre to be compatible with and able to connect to the Surrey District Energy system, as set out in the *City of Surrey District Energy System By-law* (as amended).



Gateway Office Tower, Surrey City Centre

B1 POLICIES: Streets

- B1.22 Plan and develop a road network (see *Surrey City Centre Plan*) that supports high density development, fosters the convenient and safe movement of pedestrians and cyclists and allows for the efficient movement of vehicles.
- B1.23 Create an interconnected, short-block, finer-grained network of public streets and green lanes in Surrey's City Centre to support direct, pleasing and safe pedestrian and cycle movements and to distribute traffic efficiently.
- B1.24 Consider opportunities to expand on-street parking, wherever feasible, to provide a buffer between pedestrians and moving vehicles and to enhance street-fronting retail business viability.
- B1.25 Design King George Boulevard through Surrey's City Centre as a 'Great Street'. Create an attractive pedestrian environment while also maintaining King George Boulevard as an important traffic and transit corridor.
- B1.26 Support City Centre's role in Surrey's economic vitality by ensuring there is convenient and efficient goods movement into, within, and through City Centre while also supporting the long-term reduction of regional truck traffic along the City Centre portion of King George Boulevard.
- B1.27 Ensure new and existing streets in Surrey's City Centre accommodate as many street trees as possible. Redevelopment sites shall prioritize the provision of street trees through retention or new plantings and by taking steps to ensure planting conditions are suitable for long-term, healthy tree growth. Species chosen should be adaptable to increasingly warmer summer climates.



University Boulevard, Surrey's City Centre.

B1 POLICIES: Urban Design

- B1.28 Implement high architectural and urban design standards for development within Surrey’s City Centre and create a public realm that is safe, beautiful, active and vibrant.
- B1.29 Enhance the identity and definition of Surrey’s City Centre by creating strong and memorable gateways.
- B1.30 Develop a dramatic and engaging Surrey City Centre skyline appropriate to a major regional downtown. Support the construction of high-rise and landmark architecture buildings in appropriate locations.
- B1.31 Create comfortable, attractive, human-scaled streets at key locations, by using wide sidewalks, enhancing landscaping, adding quality street furniture and installing attractive, pedestrian-oriented lighting and public art.
- B1.32 Ensure individual developments integrate and coordinate building orientation, circulation systems and landscaping elements with adjacent sites.
- B1.33 Reduce the number and size of large surface parking lots in Surrey’s City Centre, particularly those located between a building and a street. Support zero-lot line development in City Centre to create pedestrian-friendly, urban-based development designs.
- B1.34 Consider opportunities to reduce parking requirements where developments are well-served by frequent transit service.

B1 POLICIES: Housing Diversity

- B1.35 Address the integrated affordable housing and social service needs of vulnerable residents in Surrey’s City Centre, in partnership with Provincial government agencies, BC Housing, the Fraser Health Authority and the non-profit sector.
- B1.36 Support a balanced demographic community in Surrey’s City Centre by maintaining a diversity of housing types and unit sizes, including housing for seniors and families with children.
- B1.37 Retain and increase the supply of affordable rental housing in City Centre, including larger units that accommodate families.
- B1.38 Support the retention of detached housing in Urban-designated neighbourhoods within the *City Centre Plan* area while discouraging the construction of low-density detached housing in areas of the *City Centre Plan* that are intended for higher density development.



Surrey City Centre Library, under construction.

B2

Distinctive Town Centres

Develop each Town Centre as the distinctive, social, cultural and commercial centre of its community

Town Centres serve the commercial, recreational, cultural and social needs of local residents and visitors and have a sufficient residential density to support urban services. Concentrated residences and services such as shops, offices, institutions and cultural and entertainment uses allow for efficient pedestrian and cycling transportation networks, as well as a frequent public transit network. The density of development investment in centres also allows diverse, vibrant and active public spaces to flourish with a high level of urban design.

In order to meet Objective B2, the following policies apply:

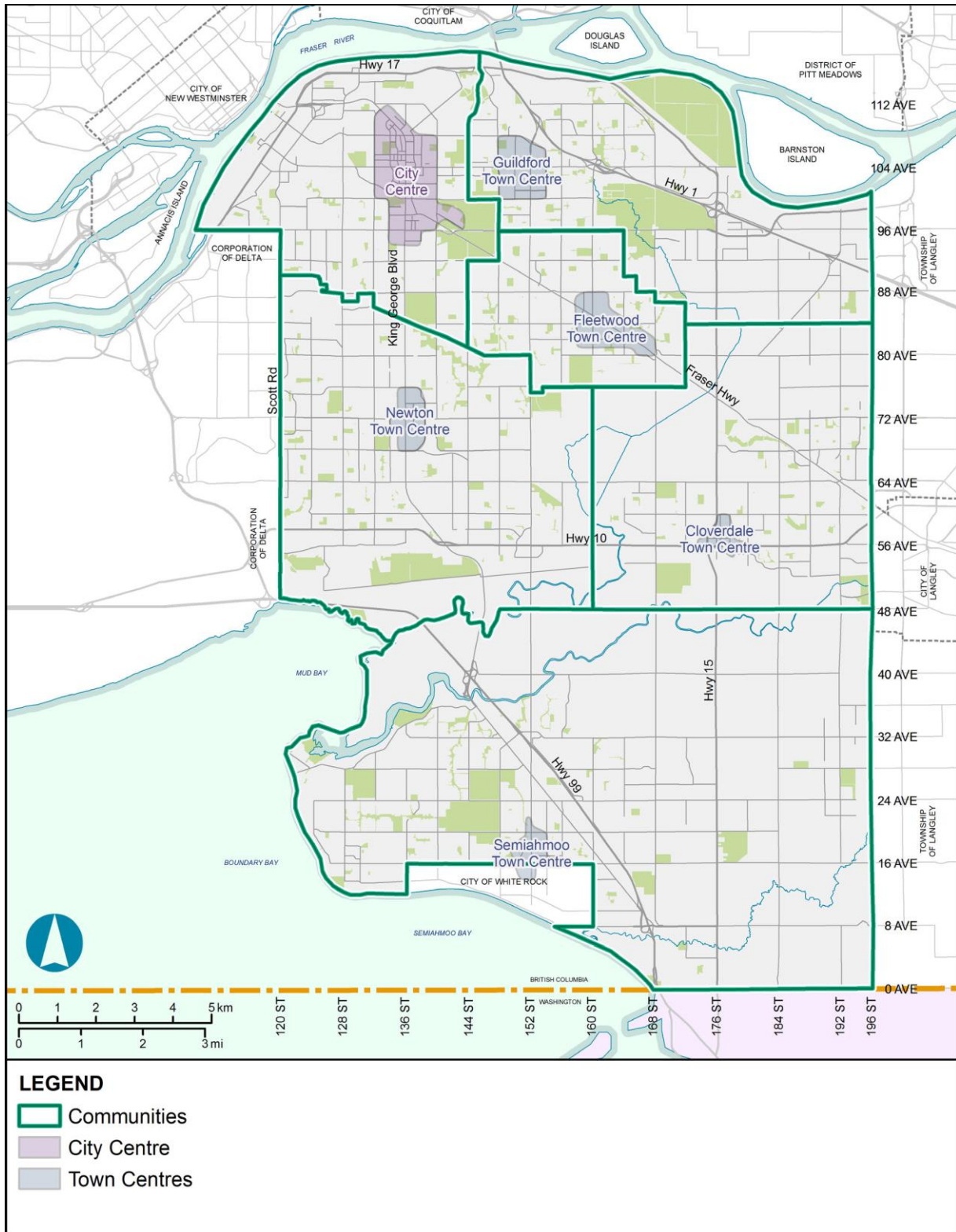
B2 POLICIES: General

- B2.1 Support each Town Centre (Guildford, Fleetwood, Newton, Semiahmoo and Cloverdale) (as shown on Figure 20) as the primary centre for its community, the location of higher intensity urban development, and the location of community-serving civic, cultural, social and recreational facilities. Support the City Centre as the Town Centre for the Whalley community as well as the primary centre for Surrey as a whole.
- B2.2 Complete and/or update secondary plans, as needed, to provide guidance on land use, density, urban design, transportation networks, infrastructure requirements, and a distinctive character for each Town Centre.
- B2.3 Densities for commercial, multi-family residential and mixed-use developments in Town Centres (see Land Use and Density Section) may be calculated based on gross site area where the City has requested land dedication for pathways, roadways, green spaces, amenities or utility installations. Where this density calculation conflicts with existing Town Centre plans, the policies within the Town Centre plan shall take precedence.



Cloverdale Town Centre

Figure 20: Town Centres



City Centre serves as a hub for rapid transit and frequent bus service and is the focus of significant high density residential and commercial development intended to accommodate 75,000 people by 2041. Extensive civic development has also taken place including a new Stadium, Senior's Centre, RCMP E Division Office, Hospital Outpatient Care Facility and Emergency Room expansion. Additionally, city-wide cultural events are held in City Centre such as Surrey's Winterfest and the ever-popular Fusion Festival.



CITY CENTRE

Guildford Town Centre is characterized by overall low-density commercial development, a few stand-alone high density commercial developments and older, walk-up style apartment buildings. Higher density residential and commercial development is expected to increase given its proximity to the Trans Canada Highway (Highway #1).



GUILDFORD

The Newton Town Centre hosts a variety of land uses including civic and recreational facilities, transit, retail, commercial, light industrial and medium-to-high density residential. This Town Centre will accommodate higher commercial and retail densities in conjunction with the extension of rapid transit infrastructure along King George Boulevard.



NEWTON

Surrey's Fraser Highway is designated as a Frequent Transit Corridor. This designation, coupled with the rapid growth of multiple family residential in the area, has increased the need to expand commercial uses within the Fleetwood Town Centre and to accommodate higher residential densities in those areas that would directly benefit from the extension of rapid transportation infrastructure.



FLEETWOOD

Cloverdale Town Centre is the heart of a community with a very rich historical past. With one of Surrey's more recognizable main streets, Cloverdale Town Centre has the advantage of using its distinctiveness to attract investment and remain a viable and important area for Surrey and Cloverdale residents alike.



CLOVERDALE

The Semiahmoo Town Centre is vibrant and pedestrian-friendly with a mix of shopping, services, entertainment and amenities. Expanding all of these elements and increasing residential densities is the intended vision for Semiahmoo while also sustaining the unique character of South Surrey's Town Centre.



SEMAIHM00

B2 POLICIES: Land Uses

- B2.4 Support economic and cultural vitality in Surrey’s Town Centres by encouraging higher-density residential and retail development.
- B2.5 Locate community-serving commercial uses in Surrey’s Town Centres to maximize accessibility and minimize impacts on residential areas.
- B2.6 Encourage co-locating or clustering complementary businesses and service uses within Surrey’s Town Centres to support compact employment areas so as to reduce the need for multiple trips to access these services.
- B2.7 Plan Surrey’s Town Centres to accommodate a wide range of households throughout their life cycle, including multi-generational households, seniors, and families with children.
- B2.8 Locate centres for community life such as recreation centres, gathering places, public institutions and cultural spaces in or adjacent to Surrey’s Town Centres.
- B2.9 Locate special city-wide facilities such as museums, tourist destinations, specialized open spaces or public facilities within Surrey’s Town Centres, where it is appropriate to do so, and where it also provides a distinct role and identity for the community.
- B2.10 Ensure that land uses and densities of emerging nodes including Grandview Heights, Clayton and South Port Kells evolve in a complementary manner to Surrey’s existing Town Centres.

B2 POLICIES: Transportation

- B2.11 Ensure that Surrey’s Town Centres are well-connected to adjacent neighbourhoods by creating a fine grain network of local streets. Support walking, cycling, transit use and vehicle access to a Town Centre from nearby neighbourhoods.
- B2.12 Consider opportunities to reduce parking requirements within Surrey’s Town Centres that are well-served by rapid transit and frequent bus service.
- B2.13 Discourage auto-oriented development such as drive-thrus and large-format retail, within Surrey’s Town Centres.
- B2.14 Encourage underground parking and provide landscaped open spaces and pedestrian environments in place of surface parking.
- B2.15 Connect Surrey’s Town Centres with one another and with Surrey’s City Centre through safe, direct and frequent transit routes, arterial roads, bike routes and major greenways.



Add identifiable street signs and street furniture to further delineate Town Centres, e.g. Cloverdale street signage

B2 POLICIES: Place-Making

B2.16 Create a distinct character for each of Surrey's Town Centres, consistent with its community context, by building on existing natural and cultural assets. Enhance each of Surrey's Town Centre's distinctiveness by:

- Promoting, preserving and enhancing unique characteristics such as architectural styles, built and natural heritage, cultural heritage, social gathering spaces, places of worship, landmark buildings, landscapes or signs
- Creating distinctive gateways and identification signs at main entrances
- Coordinating a characteristic and defining design style by coordinating the use of signs, banners, public art, paving materials and patterns, street trees, street lights and site furnishings
- Emphasizing a high standard of urban, landscaping and architectural design
- Identifying and implementing branding and beautification efforts.

B2 POLICIES: Parks and Greenspace

B2.17 Develop a central, universally-accessible urban park in each of Surrey's Town Centres. Where possible, locate the park adjacent to community facilities (e.g. libraries and recreation centres) and design and program it for community events and gatherings.

B2.18 Use linear corridors, walkways, greenways, streets and other functional open spaces to connect the public to private outdoor spaces such as plazas and squares.

B2.19 Link Surrey's Town Centre parks and open spaces to the city-wide network of greenways.



Walkways and greenways used to promote pedestrian cycle movements.

BL 19519
BL 19801

B3

Transit Corridors

Support Transit Oriented Development along major corridors linking urban centres and employment areas

Transit Oriented Development (TOD) focuses on creating compact, complete urban neighbourhoods with higher density mixed uses that align with and support frequent transit service. TOD helps to reduce greenhouse gas emissions by supporting active and public transportation choices and relieves congestion on busy streets by reducing reliance on the automobile. In order to develop a transit-oriented city, it is important to locate higher density development in appropriate locations along existing and proposed Frequent Transit Corridors.

In order to meet Objective B3, the following policies apply:

B3 POLICIES: General

B3.1 Support higher-density residential, commercial and mixed-use development in appropriate locations along existing and planned Frequent Transit Corridors and Skytrain Corridor Planning Areas (see Figure 22) outside of Surrey's Town Centres.



King George Boulevard Transit Corridor; accommodates pedestrians, cyclists, rapid transit, bus transit, vehicles and commercial trucks.

Figure 21: Frequent Transit Development Types

BL 19519
BL 19801

For the purposes of the policies represented in this OCP, the following classifications apply:

Frequent Transit Development Areas

These areas are located outside of Town Centres (as shown on Figure 9) where higher densities may be expected to be permitted and in accordance with the Land Use and Density Section of this Official Community Plan.

Frequent Transit Corridors

These are urban areas located along TransLink’s Frequent Transit Network (as shown on Figure 22).

Rapid Transit Station Areas

These areas are urban nodes generally located within 800 metres of existing or planned rapid transit stations. These station areas may be located within the City Centre, a Town Centre, a Frequent Transit Development Area or along other parts of a Frequent Transit Corridor.



Image credit: Ron Walkey, from the City of Surrey Sensitive Urban Infill Design Charrette Report

BL 18787
BL 19519
BL 19801

B3 POLICIES: Land Use and Density

- B3.2 Encourage mixed-use development within Commercial and Multiple Residential land use designations along Frequent Transit Corridors (see Figure 22), particularly within 400 metres of existing or planned Rapid Transit stops and within Skytrain Corridor Planning Areas.
- B3.3 Support redevelopment opportunities for medium-density, street-oriented, mixed-use infill within identified Frequent Transit Development Areas (see Figure 9), that is sensitive to the character of existing residential areas (see Figure 19).
- B3.4 Encourage the retention and development of affordable housing along transit corridors, particularly along Frequent Transit Corridors (see Figure 22), and within existing or planned Rapid Transit Station Areas (urban nodes located within 400 m of an existing or planned rapid transit station).
- B3.5 Ensure that densities along Frequent Transit Corridors and within FDTA and Skytrain Corridor Planning Areas are sufficient to support rapid transit infrastructure investments.
- B3.6 Define appropriate land uses and densities for areas along Frequent Transit Corridors, including in Skytrain Corridor Planning Areas, (as shown in Figure 22) through a community engagement and consultation process. These processes may result in defining additional FTDA's within Surrey.

Example of Multiple Family development within Surrey

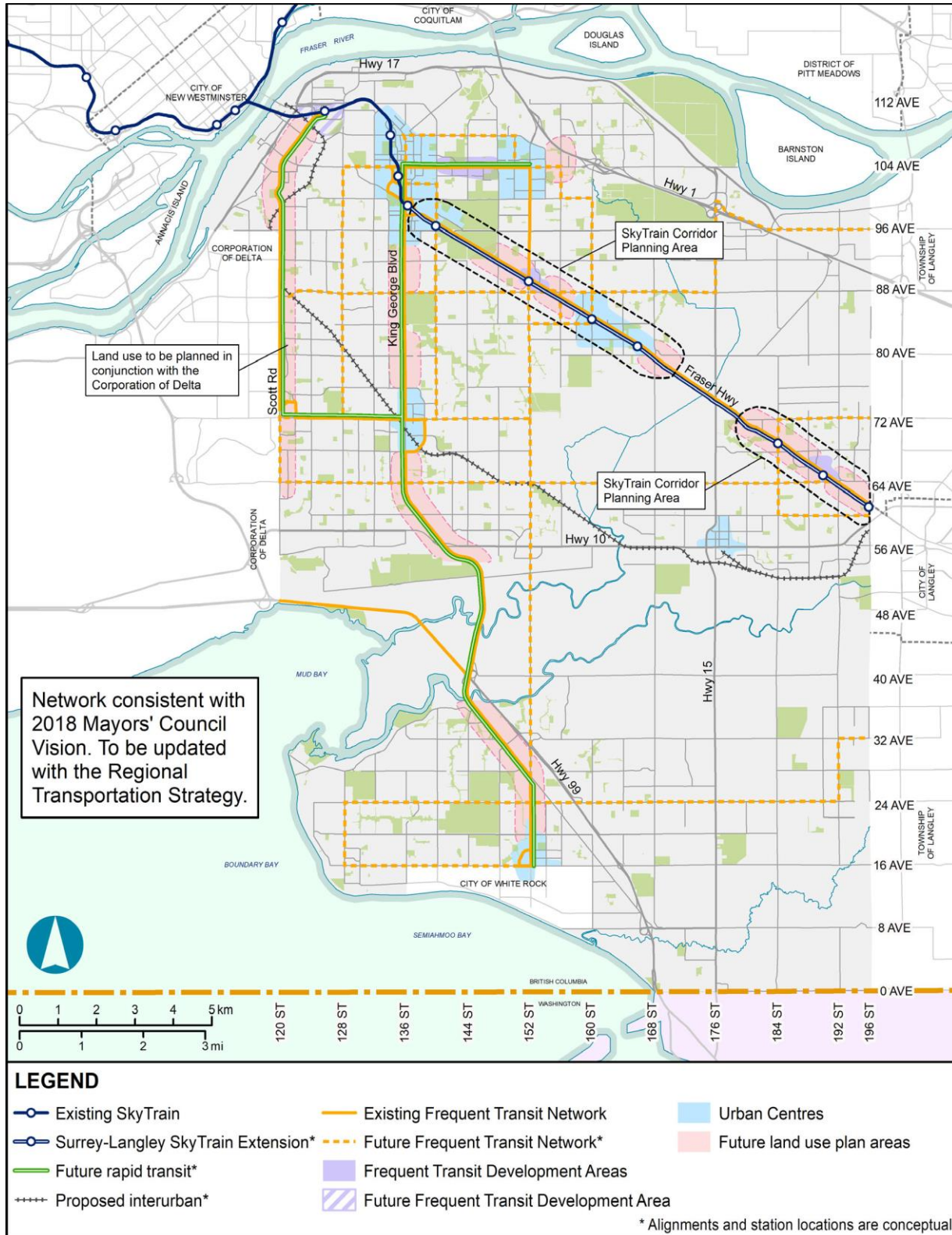


B3 POLICIES: Urban Design

- B3.7 Orient new buildings to directly face and front onto streets along all transit corridors, providing convenient access to residences and businesses from transit routes.
- B3.8 Ensure safe, convenient and universally-accessible pedestrian and cycle access from adjacent neighbourhoods to bus stops and transit stations.
- B3.9 Encourage development that supports increased transit, pedestrian and cycle use along existing or planned Frequent Transit Corridors, including in Skytrain Corridor Planning Areas, by:
 - Creating attractive public places at major arterial intersections and/or major development sites that are accessible to the public and serve as neighbourhood gathering places
 - Providing generous sidewalk widths that are free of obstacles and are finished with a high level of pedestrian amenities such as street lighting, street trees, landscaped boulevards, transit shelters, benches and seating areas
 - Considering transit-priority measures such as reserved lanes, signal pre-emption and bus bulges to increase passenger comfort and reduce delays
 - Placing parking underground or at the rear of buildings
 - Minimizing the number and width of driveway entrances on arterial roads or along existing or planned Frequent Transit Corridors (see Figure 22).
- B3.10 Ensure development in Skytrain Corridor Planning Areas (Figure 22) is designed to create vibrant streetscapes that contribute to complete and sustainable communities and neighbourhoods.

Figure 22: Frequent Transit Corridors

BL 19519
BL 19801



B4 Healthy Neighbourhoods

Build complete, walkable and green neighbourhoods

Complete, walkable and green neighbourhoods provide opportunities for working, living and recreating while accommodating the needs of all age groups and abilities. These neighbourhoods are safe to move around in, provide a mix of housing types for a range of incomes and households, provide convenient transportation alternatives and offer accessible natural areas.

The City of Surrey is committed to planning and supporting neighbourhoods that provide a high quality of life, promote healthy living, provide opportunities for social connections and that sustain a healthy natural environment.

In order to meet Objective B4, the following policies apply:

B4 POLICIES: General

- B4.1 Develop complete, accessible and walkable green neighbourhoods through the planning and development of new neighbourhoods and through sensitive redevelopment within existing neighbourhoods (see Figure 19).
- B4.2 Plan and design urban neighbourhoods with sufficient densities to support a high-quality transit system that is accessible to most residents.
- B4.3 Clearly define neighbourhood centres, including appropriately-scaled commercial and community facilities. Where appropriate, several adjacent neighbourhoods may share a neighbourhood centre.
- B4.4 Foster safe and socially-cohesive neighbourhoods by supporting neighbourhood design and community-building initiatives that bring people together in public spaces.



Crescent Beach Coach House, Surrey.

B4 POLICIES: Housing

- B4.5 Plan and design new neighbourhoods to accommodate a wide range of diverse households throughout their lifecycle by encouraging innovative and flexible forms of housing to support a diversity of household sizes and composition and rental opportunities.
- B4.6 Direct higher residential densities to locations within walking distance of neighbourhood centres, along main roads, near transit routes and adjacent to major parks or civic amenities.
- B4.7 Design housing units to front directly onto public streets and/or public spaces, in order to facilitate a safe, welcoming, public streetscape and public realm.
- B4.8 Maintain an attractive, pedestrian-friendly streetscape by requiring detached housing, with less than 12 m frontage along a public street, to be accessed from back lanes.
- B4.9 Plan for housing units to front onto riparian and green areas, where possible, to increase visibility into those areas and to increase the amenity features for those residences.
- B4.10 Incorporate live/work units into appropriate locations in higher density urban neighbourhoods. Opportunities for live/work units shall be considered in the development of secondary plans.



Multiple Family development is required to support Surrey's growing population.

B4 POLICIES: Neighbourhood Services

- B4.11 Create mixed-use neighbourhood centres that support the needs of local residents by including local-oriented shopping, services, schools and amenities within easy walking and cycling distance in order to reduce dependency on private vehicles.
- B4.12 Include small-scale offices and community spaces in neighbourhood centres that are well serviced by transit. Support retrofitting existing commercial spaces to accommodate childcare centres.
- B4.13 Design and manage public facilities such as schools, community halls and recreation centres to provide maximum community use and benefit.
- B4.14 Ensure neighbourhoods are well served by civic and community facilities such as indoor and outdoor recreation centres, childcare centres, neighbourhood parks, and amenities specifically geared to youth.
- B4.15 Work in conjunction with School District No. 36 to locate schools where they are safely and conveniently accessible by walking, cycling and public transit. Locate community parks in conjunction with school sites near the heart of each neighbourhood.
- B4.16 Work in partnership with School District No. 36 so that schools serve as after-school and weekend multi-purpose centres and gathering places for local neighbourhood activities.
- B4.17 Plan neighbourhoods in consultation with School District No. 36 to ensure that urban development and population growth is considered in the planning of new, appropriately-sized and well-located public schools.
- B4.18 Locate neighbourhood parks and play areas so that they are within 400 m of most homes.
- B4.19 Develop mini-parks and mini-plazas as outdoor gathering spaces in urban neighbourhoods. Design and locate these public places to front directly onto public streets and be clearly visible from adjacent residences.
- B4.20 Include attractive civic spaces within new and developed neighbourhood centres (e.g. plazas, common green space or a main street shopping area) in conjunction with mixed-use buildings and public amenities.
- B4.21 Retain and enhance attractive and unique neighbourhood characteristics by:
 - Using landscaping or gateway features to define or enhance existing or potential neighbourhood boundaries
 - Appropriately and sympathetically interfacing areas between new development and existing neighbourhoods
 - Developing guidelines to enhance the unique features of activity centres.



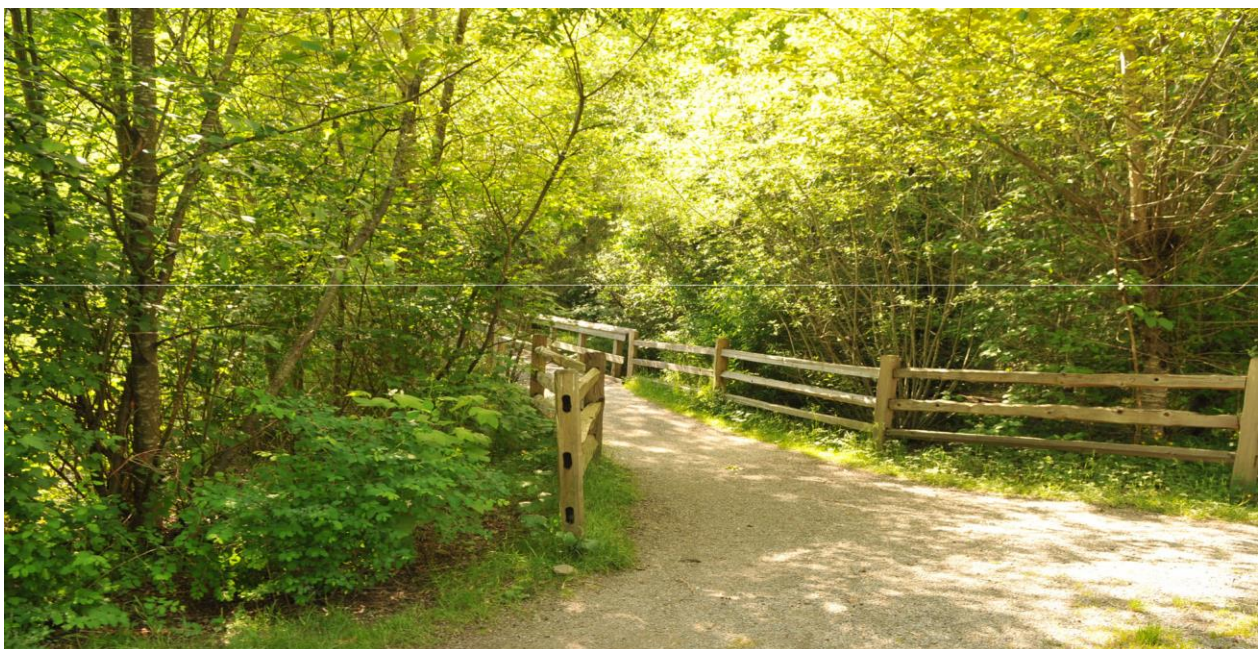
Fraser Heights Recreation Centre.

B4 POLICIES: Nature and Green

- B4.22 Provide residents with visual and physical access to natural areas by establishing trail systems that connect Surrey’s neighbourhoods with natural areas.
- B4.23 Maintain natural ecosystem connections within neighbourhoods by minimizing road crossings through natural habitat corridors and riparian areas.
- B4.24 Protect and retain significant trees, forest stands and other natural features within new and existing neighbourhoods.
- B4.25 Incorporate natural open drainage systems and green infrastructure features into neighbourhood plans.
- B4.26 Encourage developers of neighbourhoods to pursue certification through LEED-ND (Neighbourhood Development) or similar “green development” programs.

B4 POLICIES: Street and Walkways

- B4.27 Design local streets to allow multiple modes of travel and enhanced pedestrian and cycling opportunities. Wherever feasible, sidewalks and treed boulevards shall be located on both sides of all streets.
- B4.28 Support the easy, fluid, unobstructed movement of pedestrians throughout urban areas by planning local street networks using a finer-grained, modified grid pattern (as shown on Figure 23) and by strongly discouraging walled and gated neighbourhoods.
- B4.29 Where necessary, provide pedestrian connections mid-block or through private and/or strata developments to facilitate neighbourhood access to transit stops, shops, local services and amenities. Connect on-site pathways on private property with public walkways and streets.
- B4.30 Consider implementing traffic calming methods and devices, where warranted, with an emphasis on elementary schools.
- B4.31 Limit direct access to arterial roads by supporting the use of rear lanes to provide private property vehicle access.



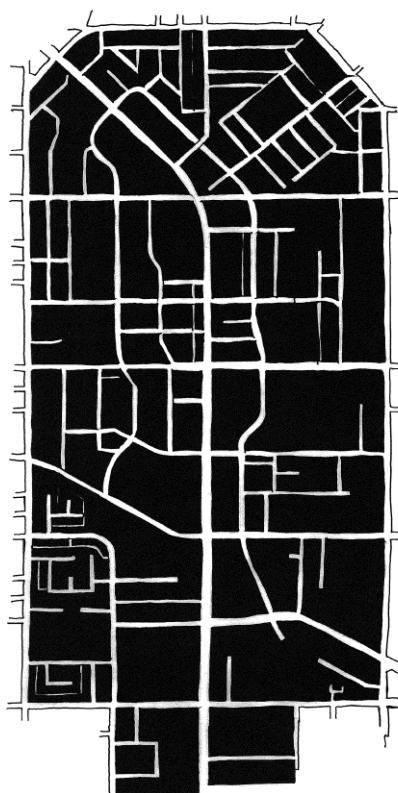
Green spaces and natural area are available throughout all neighbourhoods in Surrey.

Figure 23: Finer Grained Road Network

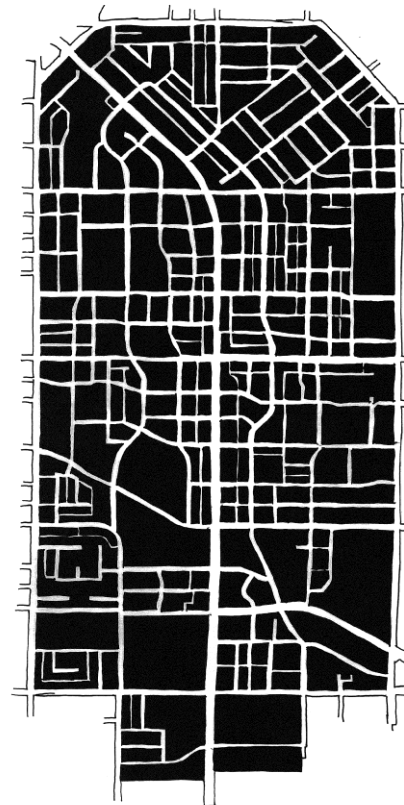
BL 19364

As Surrey continues to evolve and grow into a large metropolitan centre, it slowly transforms from a landscape of large tracks of agricultural and suburban land into a dense urban environment. A significant part of this transformation involves creating a finer grained road network, particularly within Surrey’s City Centre or downtown core. Supporting this finer grained network is vitally important to creating a livable city where walking and cycling are viable and convenient forms of transportation. Large blocks of un-interrupted land serve as barriers to comfortable and direct pedestrian and cycle circulation, and encourage inefficient vehicle movements by supporting the use of a few large arterial roads for a majority of vehicle circulation. A finer grained network allows for more even distribution of traffic throughout smaller local roads and allows for pedestrians and cyclists to move about with significantly more options and conveniences.

The proposed street network for Surrey’s City Centre (shown below) illustrates the increase in choice available to all transportation modes when a finer grained network is utilized, allowing for a more diverse and healthy community fabric of services, commercial ventures and amenity opportunities. This type of road network will also be supported in other areas of the city, where possible.



**Existing
City Centre Road
Network**



**Proposed
City Centre Finer Grain
Network**

B5

Memorable Features

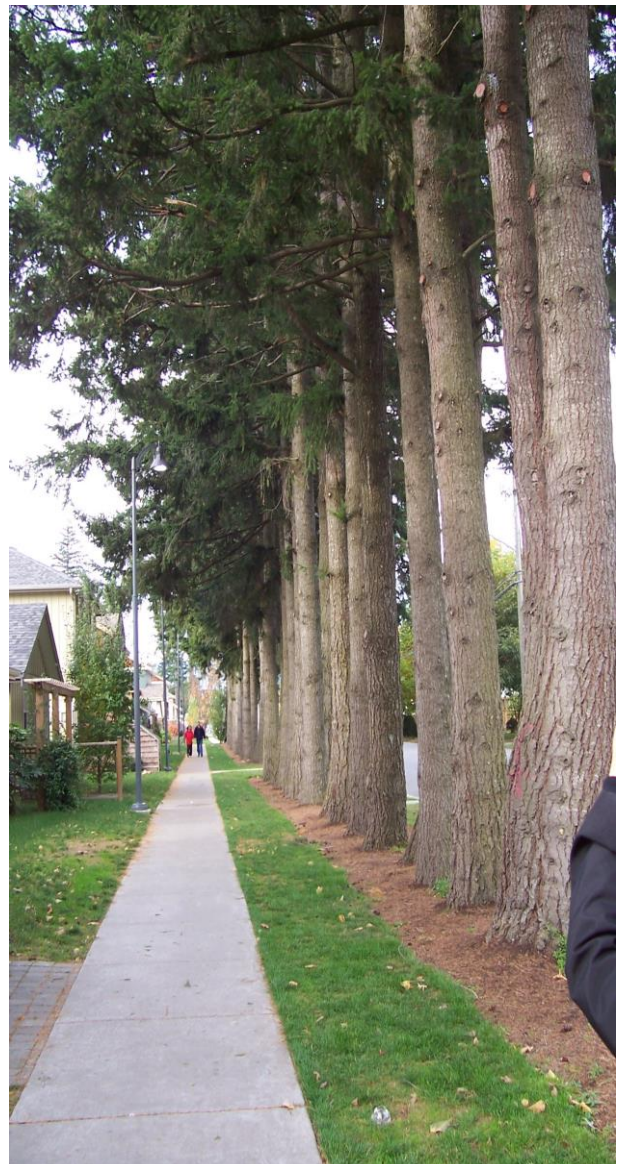
Retain and enhance memorable natural and built features and places

“Sense of Place” is a characteristic of a specific geographic area that has a recognizable, unique or particular identity. Surrey’s Sense of Place is a product of its location within the lower Fraser Valley where the natural landscape is defined by the wide, mostly flat, agricultural lowlands of the Little Campbell, Nicomekl and Serpentine River valleys, framed by upland areas with wooded escarpments. Memorable views are available from many locations within Surrey and along linear, treed, road corridors. Key views into Surrey from across the Fraser River and from the major bridges include the Fraser River shoreline with a wooded North Slope escarpment and an emerging City Centre skyline. These features are particularly important to retain as Surrey continues to grow. The City is committed to identifying, retaining and protecting significant landscapes, historic centres and places and the built and natural features that provide Surrey its Sense of Place.

In order to meet Objective B5, the following policies apply:

B5 POLICIES: General

- B5.1 Incorporate the protection and enhancement of the overall quality of Surrey’s “Sense of Place” and natural heritage into the preparation of land use plans, public works projects and the review of development proposals.
- B5.2 Encourage local, community-based initiatives and support public outreach and education programs that protect, enhance and increase awareness of Surrey’s neighbourhoods and their natural and built features.



Significant tree stands create memorable features in many areas of Surrey.

B5 POLICIES: Gateways and Corridors

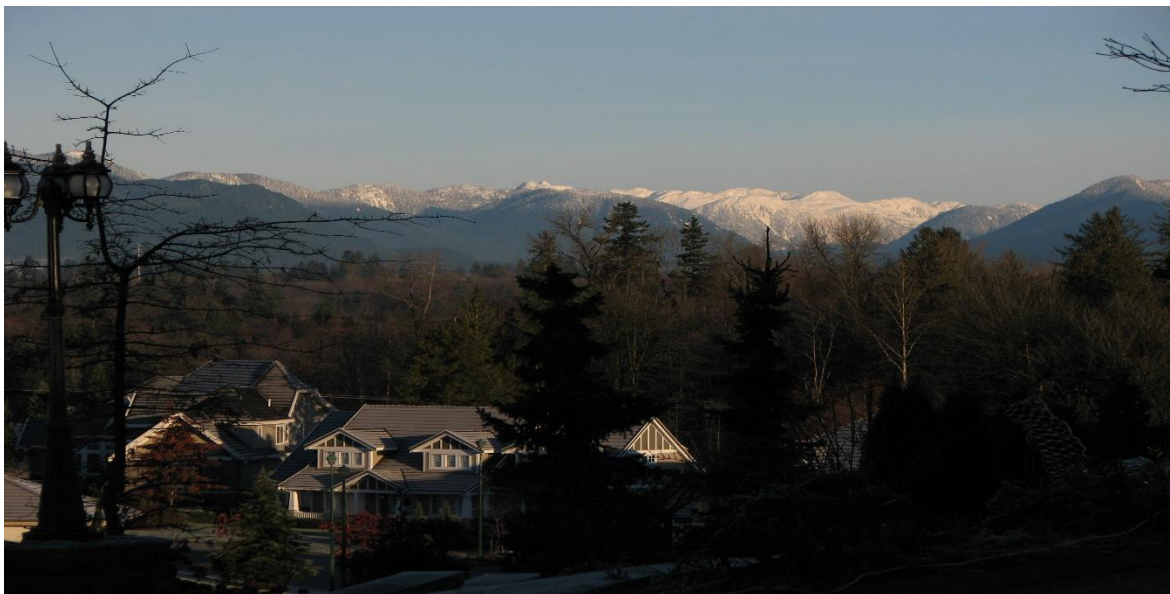
BL 19364

- B5.3 Identify distinctive and high-quality entrances and gateways into Surrey through landscape design, public art installations and consistent City signage on both public and private property.
- B5.4 Retain and enhance attractive natural and cultivated landscapes and built structures along major highways and roads throughout Surrey by protecting native vegetation and significant trees, avoiding blank walls and outdoor storage in development and by ensuring high quality signage.
- B5.5 Create a cohesive, high quality interface along all municipal boundaries in collaboration with neighbouring municipalities.

B5 POLICIES: Views and Viewpoints

- B5.6 Identify and protect significant views, where possible, from public lands and gathering places as an amenity for Surrey residents. Consider the following as significant views to promote:
- Mt Baker and the North Cascade Mountains
 - The North Shore Mountains and the Golden Ears
 - The Fraser River, including its islands, log booms and marine features
 - Semiahmoo Bay, the Georgia Straight and the Gulf Islands
 - Crescent Beach, Boundary Bay and Mud Bay
 - Alex Fraser, Pattullo, Port Mann and SkyTrain Bridges
 - The agricultural valleys of the Little Campbell, Nicomekl and Serpentine Rivers
 - The wooded escarpments along the Fraser River (North Slopes), the Semiahmoo Peninsula (Ocean Park Bluffs) and the agricultural lowlands
 - The City Centre skyline
 - Boundary Bay-Roberts Bank-Sturgeon Bank (Fraser River Estuary) Important Bird Area.

Surrey boasts spectacular views from many vantage points and neighbourhoods



B5 POLICIES: Rivers and Escarpments

- B5.7 Enhance the natural character of riverbanks and shorelines in Surrey, in consultation with and subject to, the jurisdiction of relevant government agencies.
- B5.8 Identify and enhance opportunities for public access points and trails along the ocean shores, the Fraser River and, where appropriate, in consultation with the agricultural community, and with the approval of the Agricultural Land Commission, along the Nicomekl, Serpentine and Little Campbell Rivers and their tributaries.
- B5.9 Work with businesses using the Fraser River to enhance the industrial character and shoreline image of the Fraser River. Use riparian enhancements and the planting of native vegetation, including trees and shrubs, as a screening mechanism, where feasible.
- B5.10 Protect and enhance the generally forested character of the escarpment slopes along the Fraser River and the agricultural valleys through tree protection measures and replanting programs.

B5 POLICIES: Rural Landscapes

- B5.11 Retain the rural character of roadways through agricultural and suburban areas of the city by maintaining road cross-sections specific to the area.
- B5.12 Encourage the retention and maintenance of common landscape features and built structures that provide Surrey's distinct character and identity. Incorporate these features into parks and public places, where feasible.
- B5.13 Where recreational activity does not interface with agricultural operations, encourage the use of country roads and dykes for recreational cycling or walking.
- B5.14 Protect and enhance the character of the agriculture/urban interface by retaining natural landscape features and planting appropriate landscape buffers between urban development and agricultural operations.



Surrey Bend, Fraser River

B5 POLICIES: Natural Heritage

BL 19364

B5.15 Protect the character of heritage and scenic roads by using special landscape guidelines and/or heritage markers. The most significant roads include:

- Crescent Road
- Harvie Road
- Old McLelland Road
- 56 Avenue (new McLelland Road) along Panorama Ridge
- Fraser Highway through Green Timbers Urban Forest
- Semiahmoo Trail
- Old Yale Road
- Hyland Road.

B5.16 Protect identified heritage and significant trees throughout the City during road works projects and adjacent to land development, including those areas along King George Boulevard and Semiahmoo Trail.

Spectacular views of Mount Baker can be seen from many areas and locations throughout Surrey.



B6

Urban Design

Encourage beautiful and sustainable urban design

Urban Design involves the arrangement of publically-accessible spaces and the structures within and around these spaces including streets, plazas, parks and public infrastructure, privately-owned building facades and gardens. Urban design is concerned with the creation of a physical environment that engages in a safe, inviting and pleasing manner.

Surrey is committed to improving the urban design of development and redevelopment projects throughout the city, enriching the sensory quality and sustainability of the urban environment and strengthening the Sense of Place of Surrey's diverse neighbourhoods.

In order to meet Objective B6, the following policies apply:

B6 POLICIES: General

BL 18423

- B6.1 Ensure high-quality and sustainable urban design through the implementation of Development Permit Areas and Guidelines, as set out in DP1 of the Implementation Section of this Official Community Plan.
- B6.2 Integrate principles of urban design into the planning of neighbourhoods and centres and as part of development review processes, including:
- Reflecting and enhancing local neighbourhood character and identity
 - Ensuring compatibility of scale, massing and architecture with adjacent sites
 - Maximizing safety and comfort
 - Integrating, connecting and maximizing access for all users
 - Promoting vibrant, active and pedestrian-friendly public and private environments
 - Providing diversity, choice and variety within a coherent, consistent physical environment
 - Designing for flexibility of uses and resiliency in changing contexts
 - Responding appropriately to environmental features to create a "Sense of Place".
- B6.3 Reflect community attributes through civic beautification initiatives and through consistent design guidelines applied to new development.
- B6.4 Ensure new development responds to the existing architectural character and scale of its surroundings, creating compatibility between adjacent sites and within neighbourhoods.
- B6.5 Enhance community pride by using public art as an integral component of community, neighbourhood and project design, in keeping with the City's Public Art Policy.

B6 POLICIES: Public-Private Interface

- B6.6 Design buildings to enhance the activity, safety and interest of adjacent public streets, plazas and spaces by:
- Locating buildings so that they directly face public streets
 - Placing street-facing retail uses and/or residential units (e.g. townhouses) on the ground floor of buildings
 - Providing doors, windows and ‘active’ building faces along public streets and plazas
 - Providing a seamless connection from streets and public areas to buildings by designing and constructing the ground floor of buildings to meet adjacent grades and sidewalk levels
 - Providing a clear delineation between public space and private residential areas by using street trees, landscaping and low walls or fences that do not create a visual barrier
 - Avoiding locating parking lots or driveways between buildings and public streets
 - Avoiding blank facades and providing ‘eyes on the street’ by placing active uses within parts of the building that front onto streets and public spaces.

B6 POLICIES: Public Realm

- B6.7 Infuse public and publicly-accessible spaces with animation and interesting features and activities including programmed events, art installations, performances, ceremonies, festivals, street markets and unstructured social interactions.
- B6.8 Create opportunities for unstructured play for children.
- B6.9 Create opportunities for interactive and enjoyable public plazas by using furniture, banners, signs, interactive public art, heritage interpretation, paving patterns, vibrant colours and other ‘placemaking’ design elements.



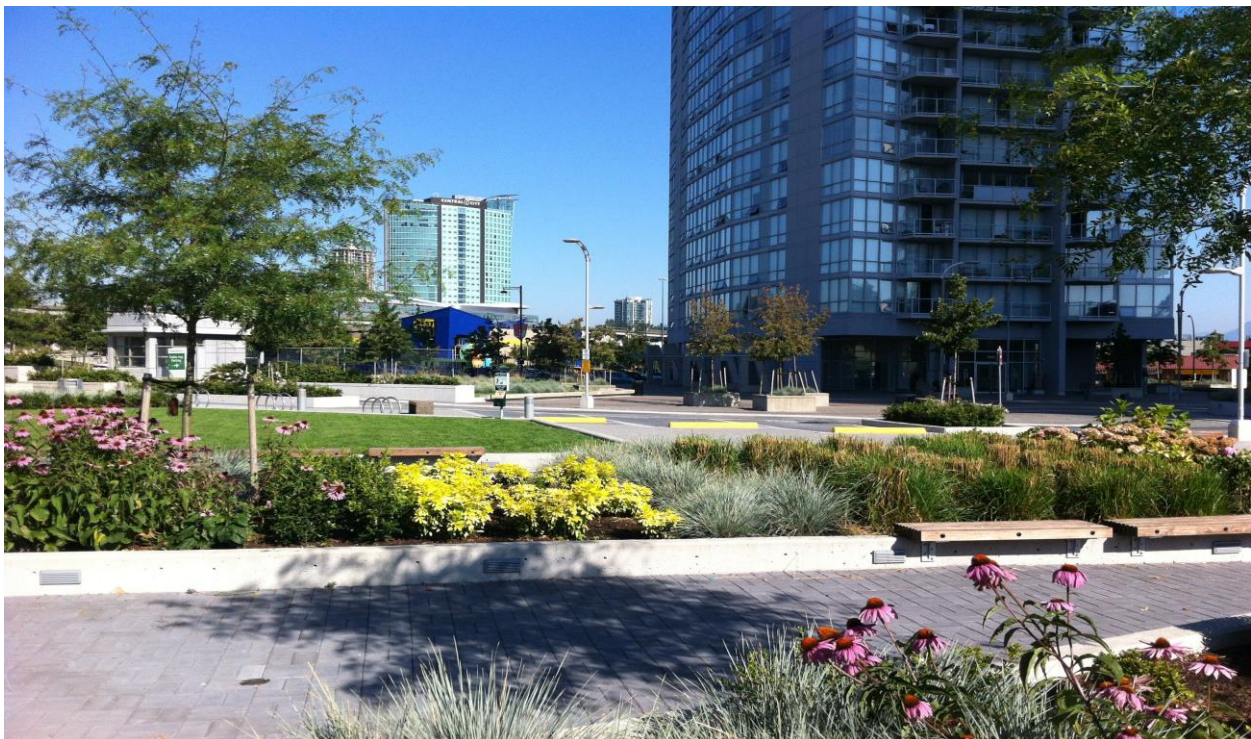
Holland Park, Surrey City Centre

B6 POLICIES: Street Networks and Circulation

- B6.10 Achieve a finer-grained network of streets, lanes and walkways when developing neighbourhood plans and when redeveloping larger properties to create interesting urban spaces and high quality pedestrian experiences (as shown on Figure 23).
- B6.11 Provide wide sidewalks, bike lanes and treed boulevards to create a safe and more attractive streetscape.
- B6.12 Where feasible, encourage on-street parking to support retail viability.
- B6.13 Ensure street widths and designs facilitate safe and convenient pedestrian travel.

B6 POLICIES: Innovative Design

- B6.14 Use Development Permits, where feasible, to ensure the incorporation of green elements into the design of public spaces, including:
 - Sufficient boulevard widths and areas able to support large-canopy street trees
 - Existing trees retained and incorporated into new and redevelopment projects
 - Attractive public and private gardens and landscaped areas that provide areas of shade and that use drought-tolerant plants
 - Well-managed community gardens on residential, commercial and institutional sites
 - Green roofs, walls and screen plantings that are integrated into buildings and structures
 - Sustainable rainwater management features
 - Use of native plants for landscaping, including coniferous species.



Park Place, Surrey City Centre

B6 POLICIES:

Comfort, Accessibility and Safety

- B6.15 Maximize public access to sunlight by managing and shaping the mass, setback, height and bulk of new development.
- B6.16 Consider human scale and comfort by providing continuous weather protection, where feasible.
- B6.17 Ensure universal accessibility within the public realm by fully integrating accessibility and adaptable design into buildings, site development and public spaces.
- B6.18 Where there is underground parking, ensure it is safe and accessible.
- B6.19 Consider planting shade trees, particularly in areas with extensive hard surfaces, to provide shade, increase comfort and reduce heat island effect.
- B6.20 Incorporate Crime Prevention Through Environmental Design (CPTED) principles and guidelines into the design and review of development projects.
- B6.21 Promote healthy and comfortable urban areas and environments, work places and health care centres by providing adequate access to trees and natural landscapes.
- B6.22 Ensure a clear and easily understood urban environment by using appropriate wayfinding and signage.
- B6.23 Create a safe and comfortable pedestrian environment in urban areas by ensuring crosswalks, crossing signals, reduced crossing distances and boulevard separation from traffic are considered during development.



PARKit, a City initiative provides vibrant and comfortable pedestrian-focused activities.



THEME C: INFRASTRUCTURE and FACILITIES



THEME C: Introduction



Infrastructure provides the systems and networks that connect a city and supports the functions of contemporary urban life. In this Official Community Plan, the definition of infrastructure includes engineering and utility networks such as sanitary sewers, drainage systems, water distribution lines, energy networks and transportation systems. It also includes civic facilities such as recreation and community centres and libraries as well as a network of parks and greenways that knit Surrey and its neighbourhoods together.



As a geographically large city made up of several distinct urban areas that are separated by agricultural land, Surrey faces challenges in maintaining its extensive infrastructure networks in an efficient and cost-effective manner. This reality places increased importance on the careful planning and management of each network of public infrastructure.



The objectives within Theme C support maximizing the sustainability of Surrey’s civic infrastructure by emphasizing low-impact development and construction practices, extending the safe and useful lifespan of public infrastructure assets, minimizing waste and inefficiency, and designing and managing streets, parks and greenways to encourage active transportation (walking and cycling) and to enhance environmental health.

THEME C: Objectives

1. Develop efficient and adaptable infrastructure systems.
2. Provide a comprehensive transportation network that offers reliable, convenient and sustainable transportation choices.
3. Maintain an integrated network of parks and greenways to promote healthy living and connect the City and its neighbourhoods.
4. Provide an equitable and integrated network of civic and recreational facilities to support community and neighbourhood life.

THEME C: INFRASTRUCTURE AND FACILITIES

Develop Complete and Sustainable
Parks, Public Infrastructure and
Transportation Systems

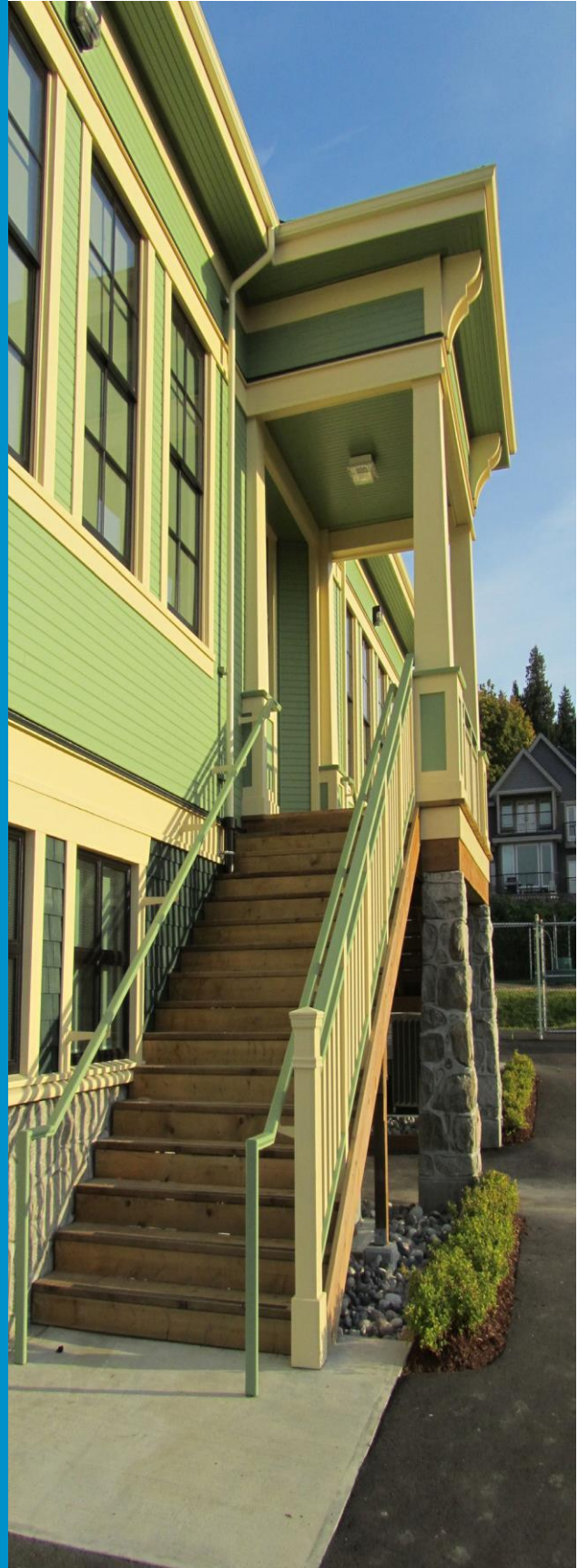
Sections

C1 Adaptable, Greener
Infrastructure

C2 Transportation

C3 Parks and Greenways

C4 Civic and Recreation Facilities



C1

Adaptable, Greener Infrastructure

Develop efficient and adaptable infrastructure systems

Surrey residents enjoy a high level of municipal services (e.g. water, drainage, liquid and solid waste and energy supply) which contribute significantly to a high quality of life. The provision and maintenance of these service demands are detailed in Surrey's *10-year Engineering Servicing Plan*.

Surrey emphasizes a sustainable approach to managing its infrastructure systems including:

- Maximizing the efficiency and performance of existing infrastructure before building new systems
- Planning for efficiencies that reduce new development infrastructure needs
- Reducing infrastructure energy consumption
- Using 'green' and natural stormwater infrastructure components to minimize local ecosystem impacts
- Supporting District Energy systems to help reduce long term energy costs
- Planning for infrastructure to support future development.

In order to meet Objective C1, the City will:

C1 POLICIES: General

- C1.1 Maintain a 10-year *Engineering Servicing Plan* (as amended) to identify the infrastructure works and services necessary to meet the needs of the City's existing population and to support future development within approved areas.
- C1.2 Encourage the development of more compact and efficient land uses and servicing systems, emphasizing infill and intensification in order to use existing infrastructure systems efficiently and to minimize the costs of new utility infrastructure.
- C1.3 Coordinate infrastructure upgrades and system extensions with land use and density requirements to ensure cost effective urban development.



Crescent Beach pump station.

C1 POLICIES: Water

- C1.4 Provide, operate and maintain the City’s water distribution system (see Figure 24) in support of the goals and objectives of Metro Vancouver’s *Drinking Water Management Plan* (as amended).
- C1.5 Encourage the use of development practices, landscaping designs and building systems that reduce water consumption, particularly in high-demand periods.
- C1.6 Require developers to pay for water extensions or system upgrades needed to support their new development.
- C1.7 Support land owners and senior levels of government to protect groundwater resources and aquifers from contamination.

C1 POLICIES: Stormwater

- C1.8 Provide, operate and maintain the City’s stormwater drainage system (see Figure 25) in support of the goals and objectives of Metro Vancouver’s *Integrated Liquid Waste Resource Management Plan* (as amended).
- C1.9 Complete Integrated Stormwater Management Plans (ISMP) for each watershed catchment in the City as a crucial step to the management of the natural environment in conjunction with land use and infrastructure planning.
- C1.10 Protect natural watercourses from encroaching development and enhance and improve where necessary.
- C1.11 Plan and implement appropriate on- and off-site stormwater management practices and facilities that support development and meet the water quality and biodiversity needs of the watershed.
- C1.12 Support alternative stormwater management techniques such as drainage infiltration, absorbent landscaping, pervious pavements and natural area preservation that more closely reflect natural drainage systems and pre-development hydrological conditions.
- C1.13 Continue dialogue with senior levels of government regarding floodplain management, dyking standards and sea level rise strategies for vulnerable areas.
- C1.14 Continue to implement and manage the *Strategic Plan for Lowlands Flood Control* (as amended) for the Serpentine River and Nicomekl River floodplains in an effort to support agricultural production in these areas.
- C1.15 Require developers to pay for stormwater extensions or system upgrades needed to support their new development.



Surrey Lake

Figure 24: Water Distribution Infrastructure

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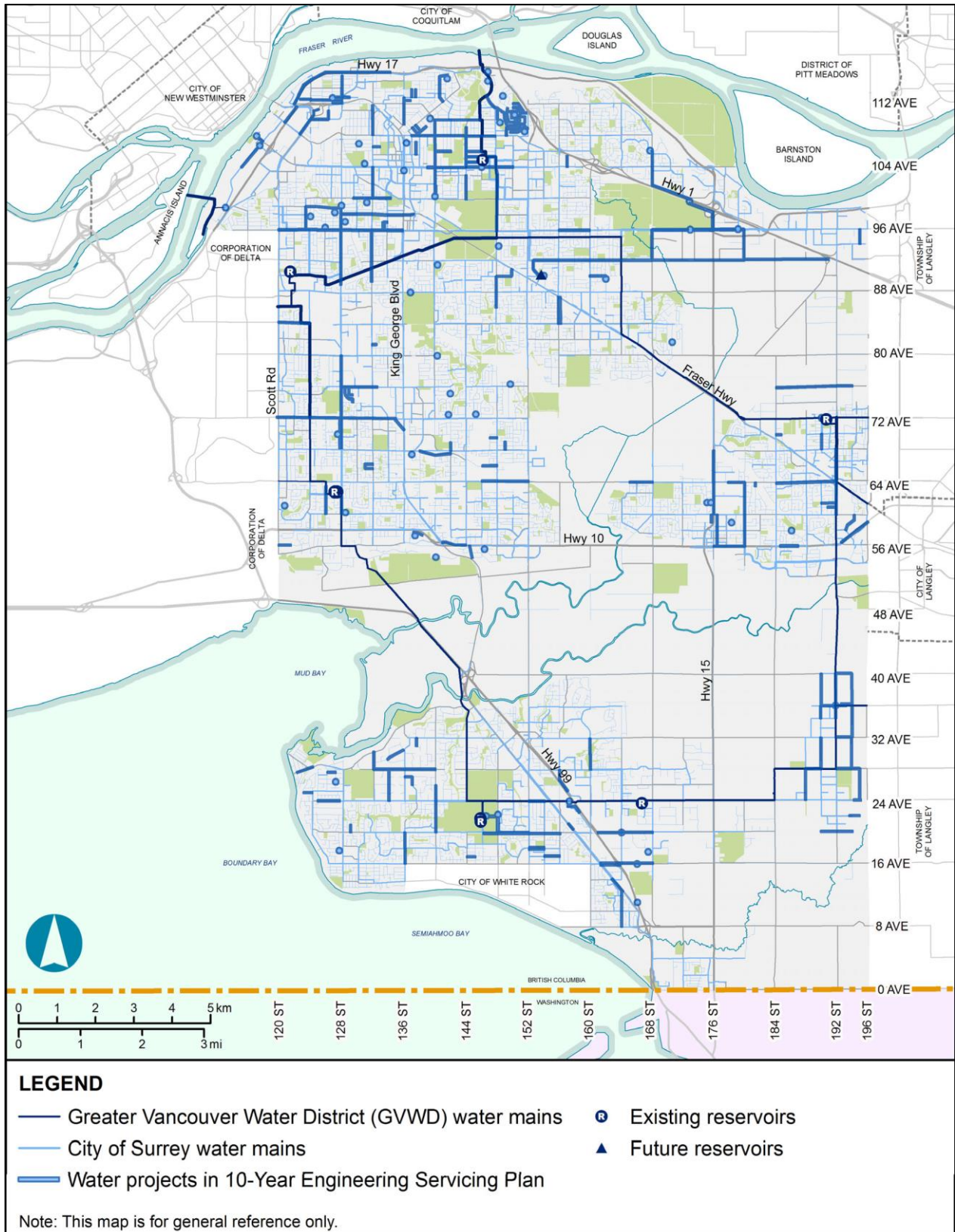
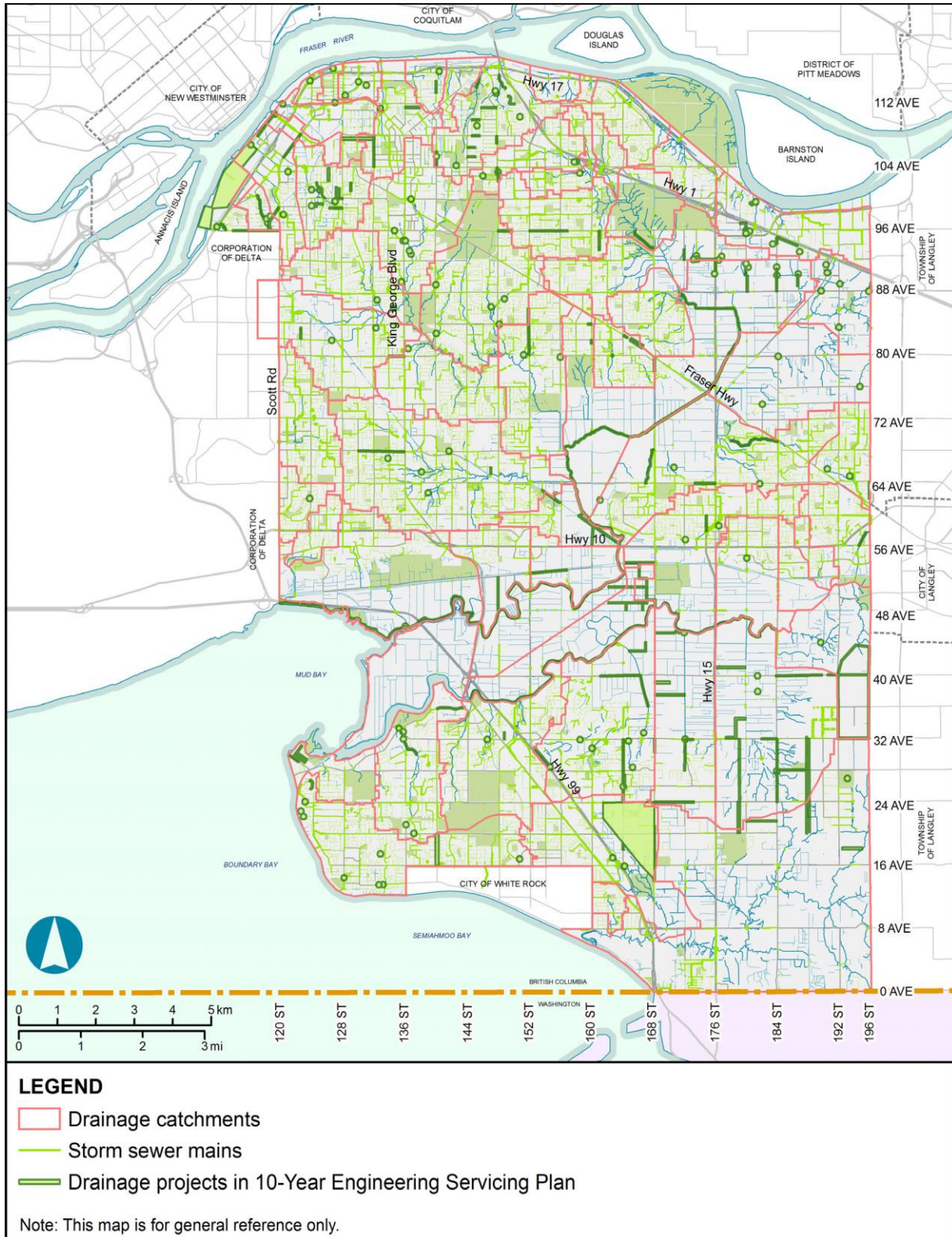


Figure 25: Stormwater Infrastructure

BL 18650



C1 POLICIES: Sanitary Sewer

- C1.16 Provide, operate and maintain the City’s sanitary sewer system (see Figure 26) in support of the goals and objectives of Metro Vancouver’s *Integrated Liquid Waste and Resource Management Plan* (as amended).
- C1.17 Require new development within Neighbourhood Concept Plan areas to demonstrate cost-effectiveness prior to approving any upgrades or extensions to the sanitary sewer system.
- C1.18 Ensure that development provides the sanitary sewer extensions or system upgrades necessary to support new development.
- C1.19 Work with Metro Vancouver and neighbouring communities to control and ultimately eliminate Sanitary Sewer Overflows (SSO) from the regional sanitary sewer system.

C1 POLICIES: Solid Waste

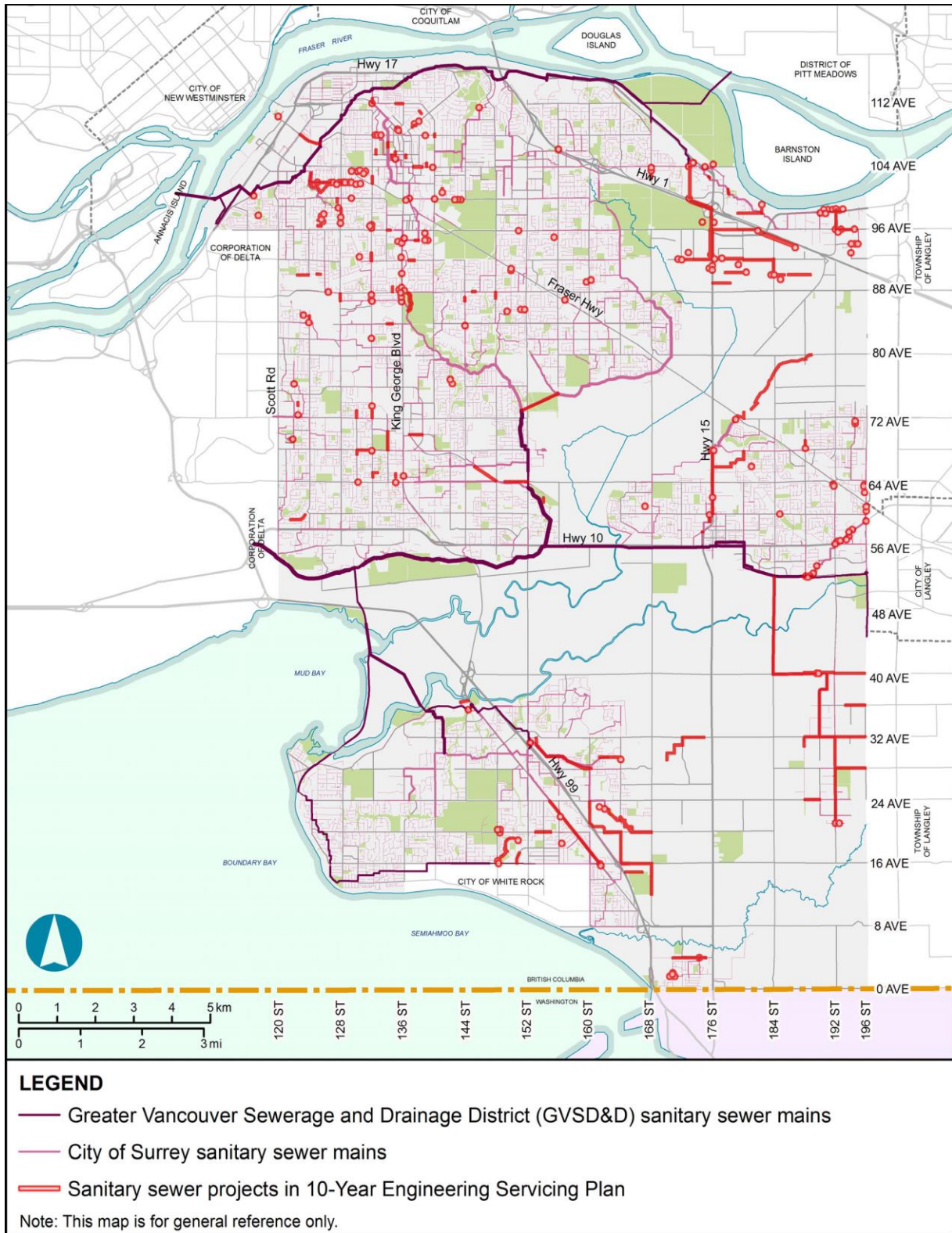
- C1.20 Provide solid waste services in Surrey according to Metro Vancouver’s *Integrated Solid Waste and Resource Management Plan* (as amended).
- C1.21 Participate with partners, including Metro Vancouver, non-profit organizations and the private sector to promote cost-effective and sustainable waste reduction and diversion initiatives and practices.
- C1.22 Reduce solid waste produced by Surrey residents, commercial and industrial operations and construction activities through recycling, home composting, curbside pick-up restrictions and public promotion and education.
- C1.23 Reduce the energy needed for waste disposal by encouraging private development to accommodate composting and recycling facilities in multi-family and commercial developments.
- C1.24 Support the co-location of industrial uses so that the waste from one can be used as a resource by another.
- C1.25 Encourage venues and special events to adopt Zero Waste goals as part of a larger “green events” policy. Use incentives and technical assistance to help implement their initiatives.
- C1.26 Work with senior levels of government to move toward prohibiting non-essential, non-recyclable materials, products and packaging in manufacturing and product distribution.

Surrey supports solid waste reductions by using a green bin and recycling program.



Figure 26: Sanitary Sewer Infrastructure

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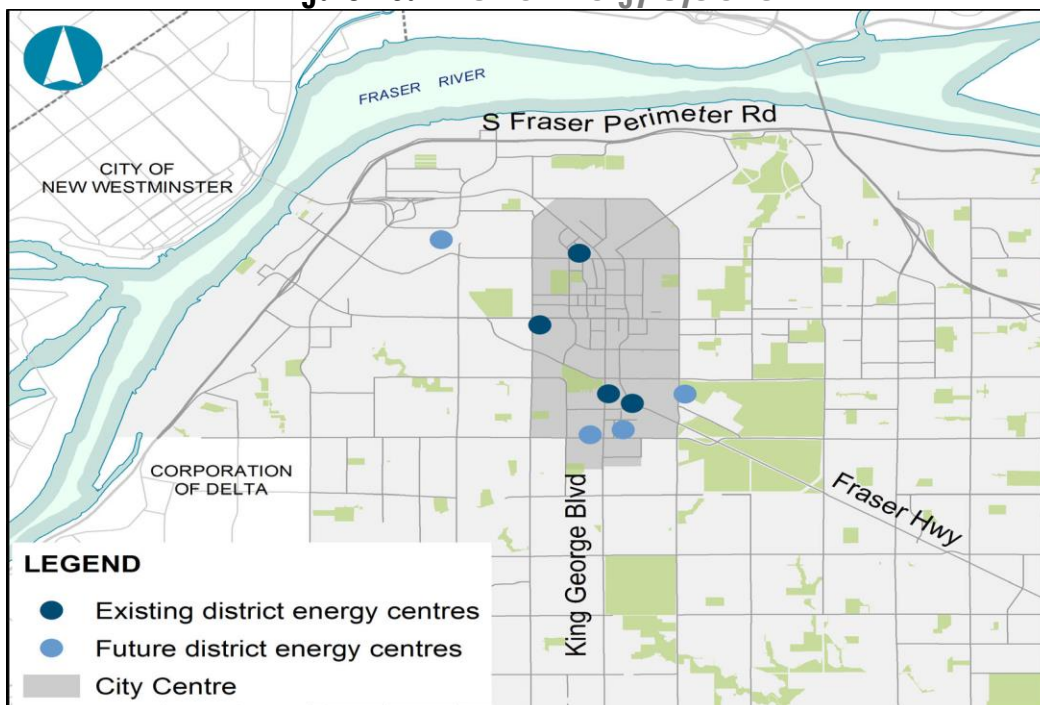


C1 POLICIES: Energy

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- C1.27 Consider energy efficiency in community planning and building design. Support land uses, development plans, transportation and utility infrastructure, building forms and energy alternatives that increase energy efficiency and conservation in a sustainable manner.
- C1.28 Work closely with BC Hydro and other utility companies to coordinate planning and development of electricity and utility infrastructure to ensure project efficiencies, to minimize costs and to reduce public nuisance.
- C1.29 Advance the implementation of District Energy Systems (see Figure 26a) to provide thermal energy to new and existing buildings to improve community energy resilience, facilitate the use of renewable energy sources and reduce greenhouse gas (GHG) emissions.
- C1.30 Focus the initial development of District Energy systems to Surrey’s City Centre. Consider the feasibility of using District Energy systems along high-density corridors and in Surrey’s Town Centres.
- C1.31 Encourage innovative ways to produce, supply, store and conserve energy at the community, neighbourhood and building levels. Explore and identify opportunities to use appropriate energy sources to satisfy energy demands.
- C1.32 Promote the use of renewable energy sources through energy demonstration projects.
- C1.33 Retrofit city facilities over time to increase energy efficiency by incorporating low-impact renewable energy sources into building and construction practices.
- C1.34 Encourage upgrading of existing buildings during alterations, additions or redevelopment to accommodate infrastructure for solar hot water and/or electric vehicle charging stations.

Figure 26a: District Energy Systems



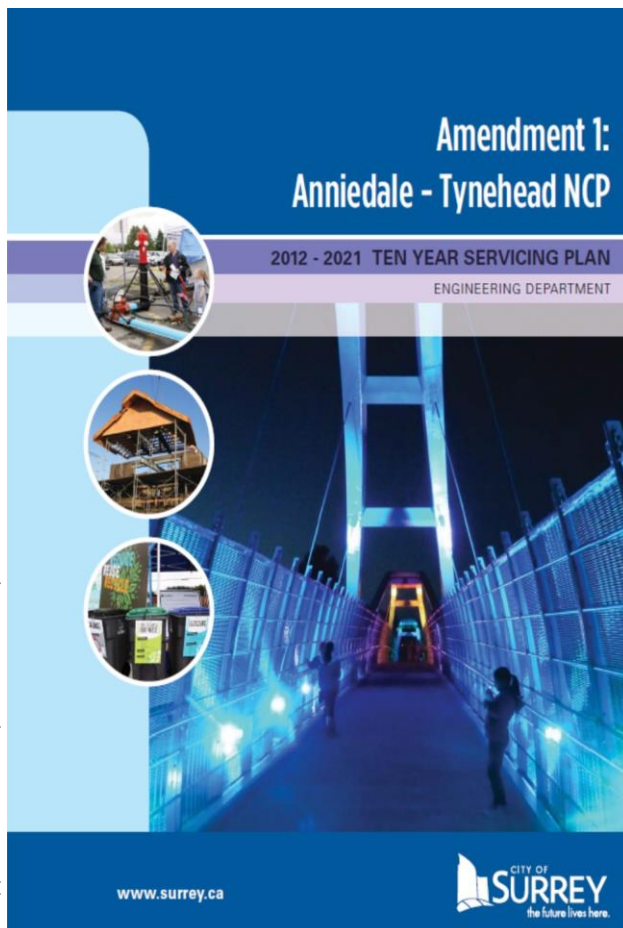
C1 POLICIES: Other Utilities

- C1.35 Ensure the appropriate interface between land uses and utility corridors, including electrical transmission corridors and oil and gas Rights-of-Way.
- C1.36 Ensure the coordination of land use planning with the provision of essential utility infrastructure including telecommunications and energy networks.
- C1.37 Develop and implement policy on the appropriate location, design criteria and neighbourhood integration of communications infrastructure in consultation with neighbourhoods and the communications industry.

C1 POLICIES: Implementation

- C1.38 Ensure Development Cost Charges (DCCs) are sufficient to fund all growth-related requirements included in the 10-Year *Engineering Servicing Plan* (as amended). Require developers to finance the utility system improvements that are necessary to accommodate their new development.
- C1.39 Use alternative financing tools such as DCC Front-Ending Agreements and Development Works Agreements to assist the development community in providing DCC-eligible infrastructure necessary to service urban development.
- C1.40 Recognize that neighbourhoods will continue to have different standards and levels of service for street lights, sidewalks, drainage and parks and that retrofitting will be considered on a limited basis for high priority areas or in areas experiencing significant redevelopment through infill or intensification. New development will be required to meet current standards or standards specified within Neighbourhood Concept Plan areas.
- C1.41 Provide opportunities for existing properties to improve their local infrastructure through the use of Local Area Servicing Agreements.
- C1.42 Ensure long-term infrastructure sustainability by establishing financial plans and reserves to ensure adequate long-term funding exists for the maintenance and replacement of the City's infrastructure systems.

Neighbourhood Concept Plans detail costs and requirements needed to support new development in Surrey.



C2

BL 19801

Transportation

Provide a comprehensive transportation network that offers reliable, convenient and sustainable transportation choices

Transportation plays a role in almost all aspects of people’s lives and is essential for people to access employment, education, shopping, recreation and social and cultural destinations in a safe, affordable, environmentally-friendly and efficient way.

The City’s vision for transportation is articulated in the *Transportation Strategic Plan*, which sets the foundation for delivering a quality, sustainable and integrated transportation system and which recognizes that: increasing mode choice and reducing reliance on the car are key components of a sustainable, balanced system; and that transportation is integrally connected to land use.

Extending rapid transit to link Surrey’s Town Centres with its City Centre is a key component of the City’s transportation vision. This vision also includes working with the regional transportation authority and senior governments to fully integrate Surrey’s existing SkyTrain and transit infrastructure along three key corridors in order to shape the development of transit-oriented centres and neighbourhoods.

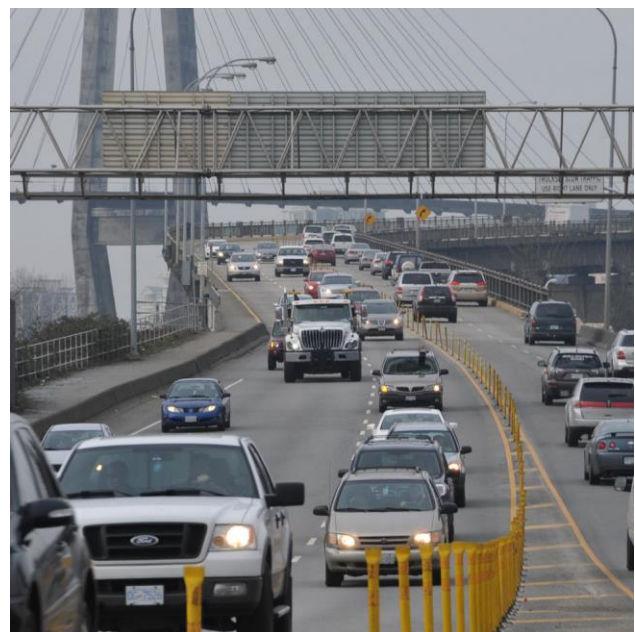
In order to meet Objective C2, the City will:

C2 POLICIES: General

C2.1 Implement the actions contained in the *Transportation Strategic Plan* (as amended), in order to achieve its objectives of:

- Efficiently managing, maintaining and improving a transportation system for all modes
- Promoting alternative and sustainable travel choice and providing better accessibility to jobs, education, health and recreation for all
- Improving community safety, health and quality of life
- Reducing congestion and supporting the sustainable economic development and vitality of Surrey
- Reducing the impacts of transportation on the built and natural environment
- Promoting the integration between transportation and land uses to reduce the need for travel and to support trips by sustainable modes.

C2.2 Plan and develop Surrey’s transportation system with a regional perspective, considering Metro Vancouver’s *Regional Growth Strategy (RGS)*. Coordinate with the strategic and operational plans of TransLink and the Ministry of Transportation and Infrastructure.



Puttallo Bridge; looking toward New Westminster

C2 POLICIES: General (cont.)

- C2.3 Achieve a balanced transportation network by considering the needs of road users in the following order:
 1. Pedestrians
 2. Transit users
 3. Cyclists
 4. Commercial traffic and trucks
 5. High occupancy vehicles
 6. Single occupancy vehicles
- C2.4 Provide direct and convenient bike, transit and vehicle access to and within employment areas.
- C2.5 Encourage development patterns, densities and designs that support transit services and that promote efficient walking and cycling.
- C2.6 Ensure that Surrey’s transportation network is planned and designed to support existing and future land uses and densities by providing transportation alternatives, including rapid transit, to improve infrastructure efficiencies.
- C2.7 Consider modifying infrastructure requirements to respond specifically to special considerations such as crossing riparian areas or significant tree retention.

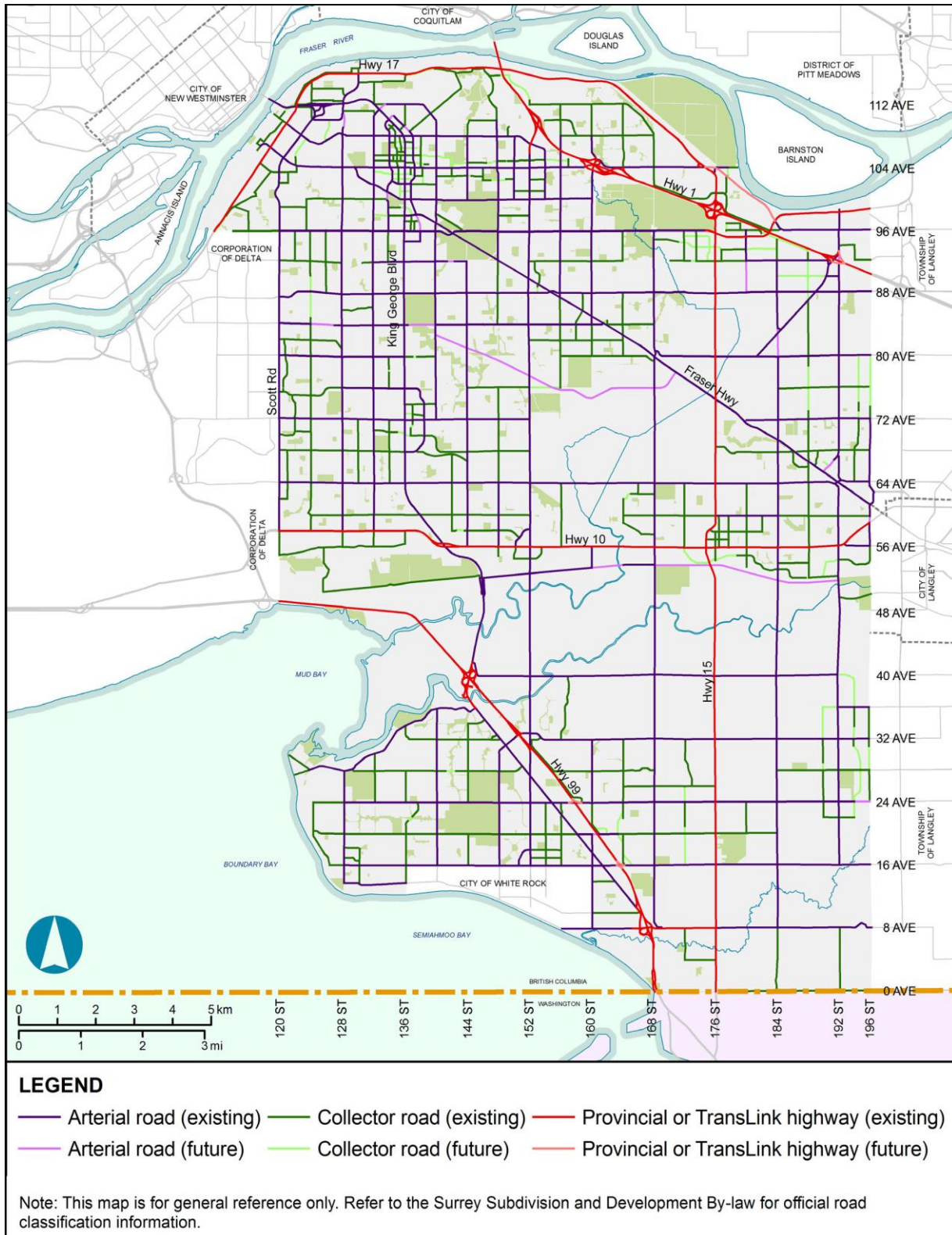
C2 POLICIES: Roads

- C2.8 Maintain road classifications including arterial, collector and local roads (see Figure 27) to provide a sufficient street network to facilitate mobility within and across Surrey.
- C2.9 Maintain appropriate street standards and road cross-sections to provide consistency in design.
- C2.10 Retain existing, and identify future, Rights-of-Way and/or other parcels of land that may be needed for future transportation corridors.
- C2.11 Ensure adequate Rights-of-Way are established to accommodate existing and future transportation needs for pedestrians, cyclists, transit services, vehicle travel, goods movement, boulevards (including street trees) and drainage and utility service corridors, as illustrated in the Major Road Allowance Map of Surrey’s Subdivision and Development By-law, as amended.
- C2.12 Manage the safe and efficient performance of existing transportation infrastructure systems before widening roads. Focus on prioritizing the completion of missing strategic links.
- C2.13 Coordinate the planning of the City’s arterial street system with major regional roads and provincial highways to facilitate regional links while minimizing traffic impacts on Town Centres and neighbourhoods.
- C2.14 Accommodate consistent flows of traffic along high-volume streets by restricting the number of driveways that directly access arterial roads.

Design with pedestrians in mind including providing areas of refuge and/or resting spots.



Figure 27: Major Road Classifications



C2 POLICIES: Roads (cont.)

- C2.15 Reduce the impacts of transportation infrastructure on the natural environment including watercourses, vegetation, trees, agriculture and conservation lands by:
- Reducing the amount of stormwater runoff that enters watercourses from roads by capturing pollutants and sediments before they enter the natural environment
 - Pursuing the greening and planting of trees along streets and pathways
 - Providing wildlife passages at key locations as identified in the *Biodiversity Conservation Strategy* (as amended)
 - Modifying road layouts and cross sections, where feasible, to protect significant trees and natural areas.
- C2.16 Implement traffic calming measures such as signage, alternative paving, narrower roads, traffic circles, curb bulges, speed humps and speed tables, where warranted, to further enhance neighbourhood safety.



WestStone Development, Surrey City Centre; public infrastructure installation.

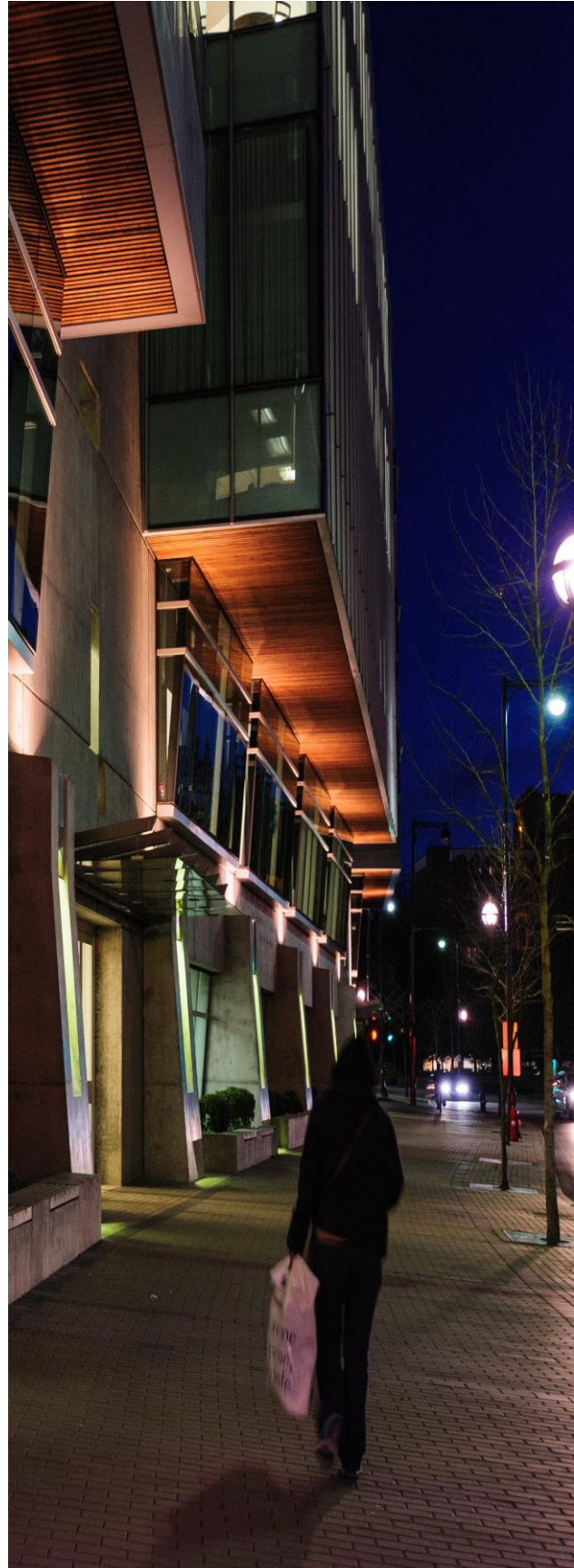
C2 POLICIES: Walking

BL 18423

- C2.17 Develop a safer, more convenient and fully accessible pedestrian network by implementing Surrey's *Walking Plan* (as amended), including:
- Providing sidewalks and walkways for access within and between neighbourhoods
 - Shortening crossing distances across roadways, where feasible
 - Linking major destination points, such as Town Centres, workplaces, transit stops, transit stations and exchanges, schools, parks, community facilities and neighbourhood commercial areas
 - Considering and addressing issues of urban design, universal access, sustainability, public health and public safety in development proposals
 - Ensuring City inter-departmental collaboration to promote and support active transportation.
- C2.18 Create short-block, well-connected street networks in new neighbourhoods and redevelopment areas using a grid or modified grid pattern that is convenient and interesting for pedestrians.
- C2.19 Where creating smaller blocks is not feasible, use public walkways and Rights-of-Way to effectively link residential areas to neighbourhood amenities such as transit stops, shops, parks, schools and greenways.

C2 POLICIES: Walking (cont.)

- C2.20 Ensure that development sites plan for and provide multiple and direct pedestrian connections linking to adjacent public streets, particularly connecting to transit stops.
- C2.21 Include sidewalks on all new streets and improve existing streets by adding sidewalks, on a priority basis, as budgets permit and as opportunities arise, with priority given to locations with high pedestrian activity.
- C2.22 Promote a positive pedestrian experience by designing, constructing and maintaining sidewalks and walkways with sufficient width, curb separation, appropriate surfacing, adjacent street trees and adequate lighting. Consider adding street furniture and weather protection amenities, where appropriate, to enhance pedestrian comfort.
- C2.23 Identify, promote and implement safer walking and biking routes to neighbourhood schools through the use of traffic signals, crosswalks and traffic calming features.



Attractive and friendly urban environments support pedestrians and walking as a viable form of transportation.

C2 POLICIES:

Transit and Passenger Rail

C2.24 Collaborate with TransLink and senior governments to plan and implement the expansion and extension of rapid transit in Surrey including:

- Linking City Centre with Guildford Town Centre and Highway No. 1 (Trans Canada) along 104 Avenue
- Linking City Centre through Fleetwood Town Centre and Cloverdale to Langley City along the Fraser Highway
- Linking City Centre from Newton Town Centre along King George Boulevard
- Bus Rapid Transit (BRT) along King George Boulevard between Highway No. 10 (56 Avenue) and Semiahmoo Town Centre, as shown in Figure 28.

C2.25 Continue to work with TransLink to improve transit service in Surrey, including:

- Implementing the *South of Fraser Area Transit Plan* (as amended), including the extension of rapid transit and frequent bus services along identified “Frequent Transit Network” corridors (see Figure 28)
- Improving existing bus service by increasing the frequency and hours of service on existing routes
- Implementing transit priority enhancements such as reserved transit lanes, “queue-jumping” lanes, transit priority signals, and express transit services
- Improving the safety, convenience, attractiveness and accessibility of transit stops and exchange areas, including the integration of public art installations
- Periodically reviewing transit routes, schedules, vehicle types, connections and hours of operation, to ensure that the best possible level of service is provided.

C2.26 Protect the former BC Hydro ‘inter-urban’ rail corridor Right-of-Way for future potential as a passenger rail corridor extending from Surrey into the Fraser Valley.

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C2.27 Advocate support for a Surrey-based stop on the Canada/US passenger rail line route.

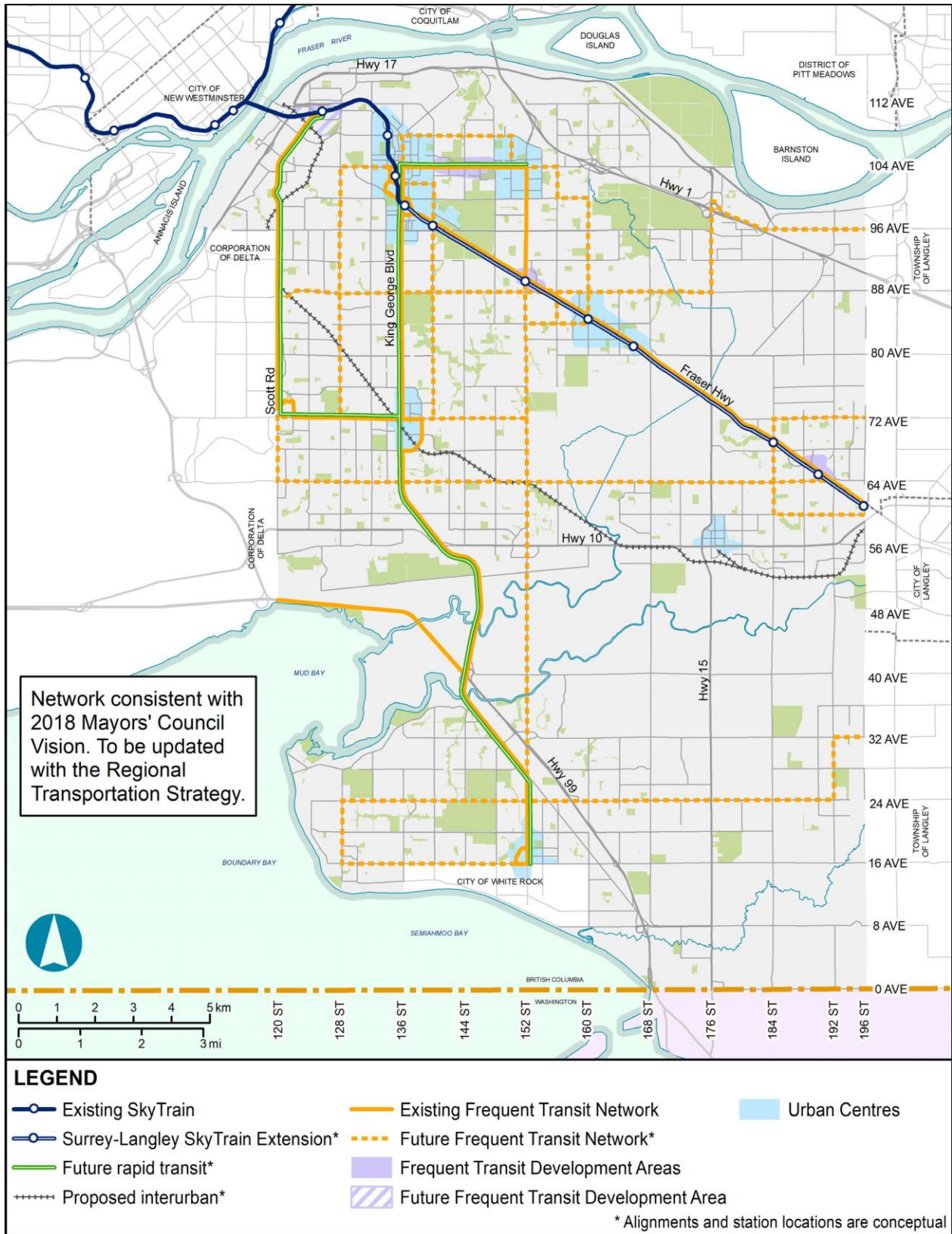
C2.28 Explore opportunities for the long-term relocation of the rail corridor along the Semiahmoo Bay and Boundary Bay waterfront to a more direct inland corridor.



Public Transit use in Surrey; Newton Transit Exchange

Figure 28: Frequent Transit Networks

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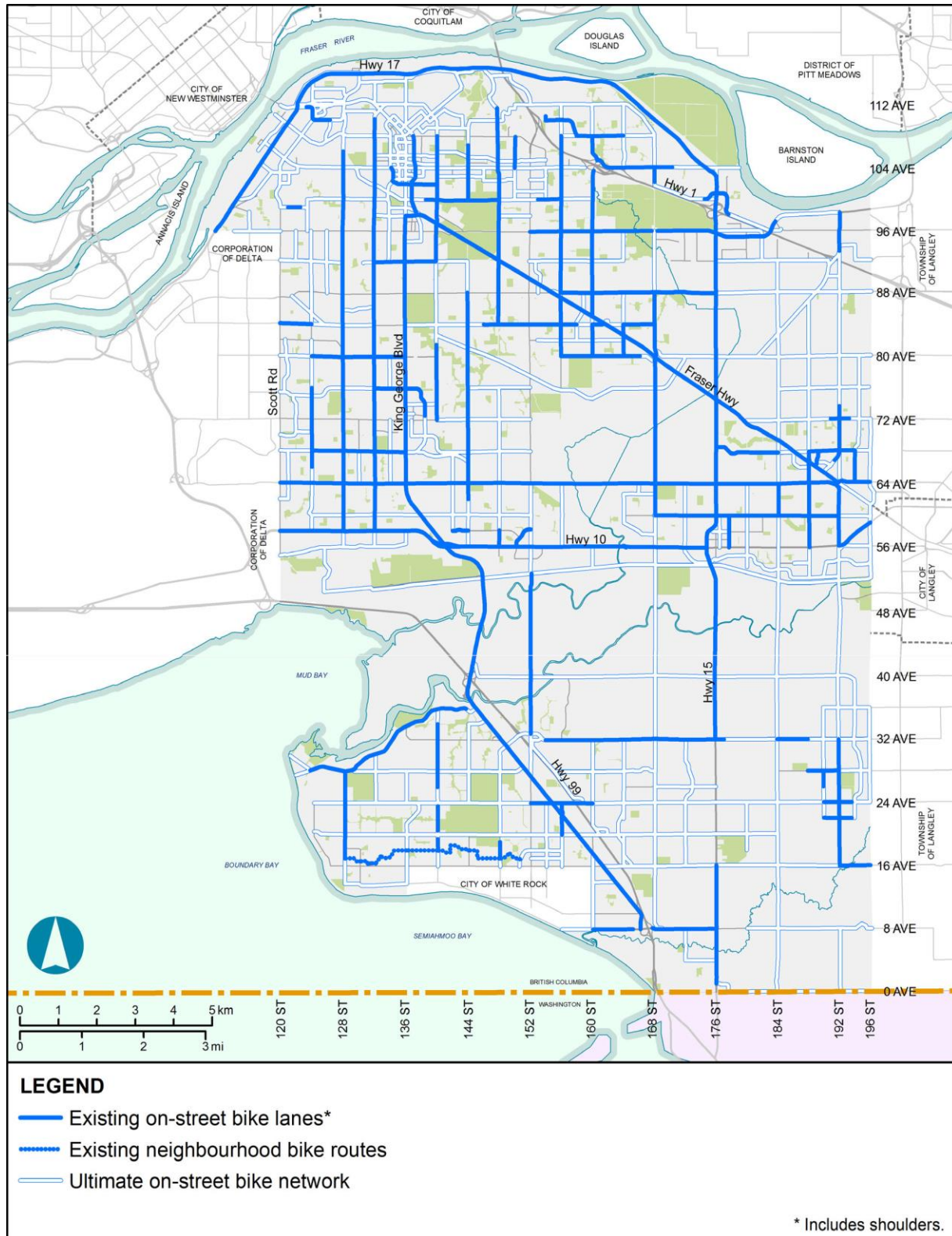
C2 POLICIES: Cycling

- C2.29 Plan, implement and maintain a complete, city-wide system of bike lanes, designated bike routes and multi-use pathways in accordance with the *Surrey Cycling Plan* and the *Surrey Greenways Plan* (as amended).
- C2.30 Provide continuous, convenient and direct cycle access to major destinations within Surrey to encourage cycling as a viable alternative to driving, and to provide a healthy, alternative mode of transportation for Surrey residents (see Figure 29).
- C2.31 Integrate and connect on-street cycle routes (e.g. bike tracks, bike lanes, shared streets) with off-street recreational cycling such as multi-use pathways, greenways and park trails.
- C2.32 Incorporate cycling routes, convenient bicycle parking, storage and end-of-trip facilities into public and private developments.
- C2.33 Locate bicycle-supportive activities, facilities and features at or near rapid transit stations, bus exchanges or at appropriate locations along transit corridors.



Appropriate infrastructure encourages cycling a viable means of transportation.

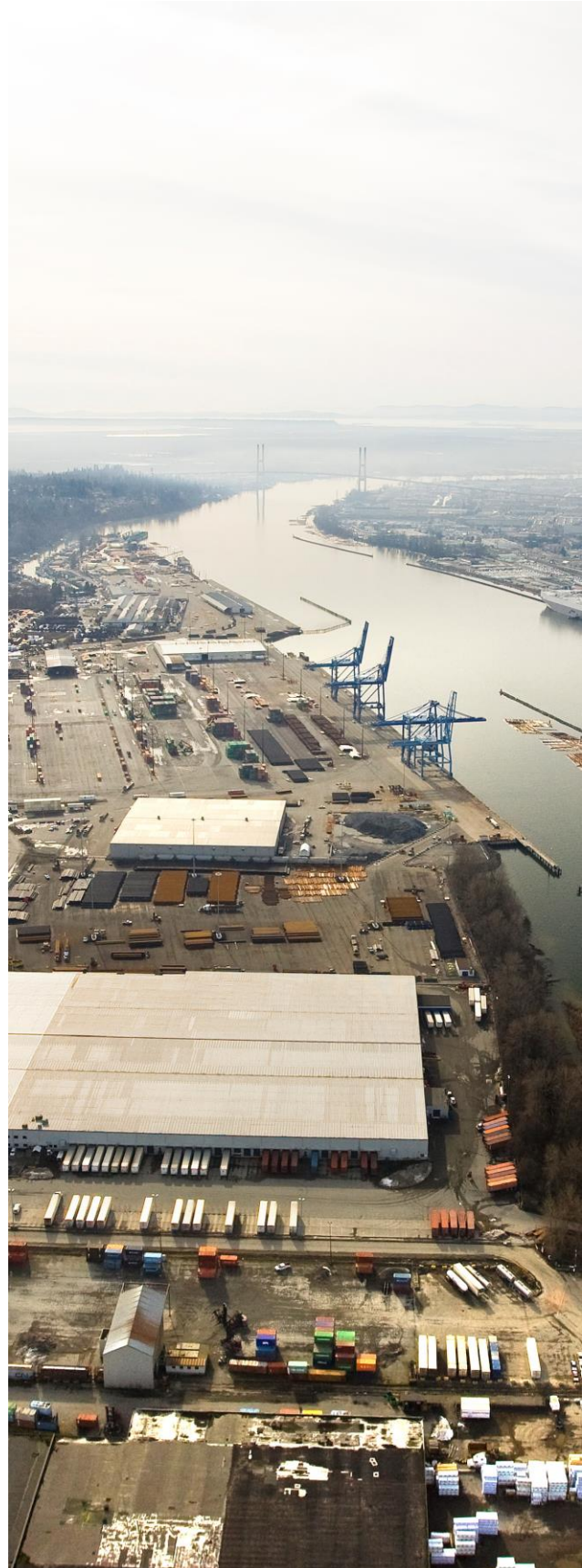
Figure 29: Cycle Routes



* Includes shoulders.

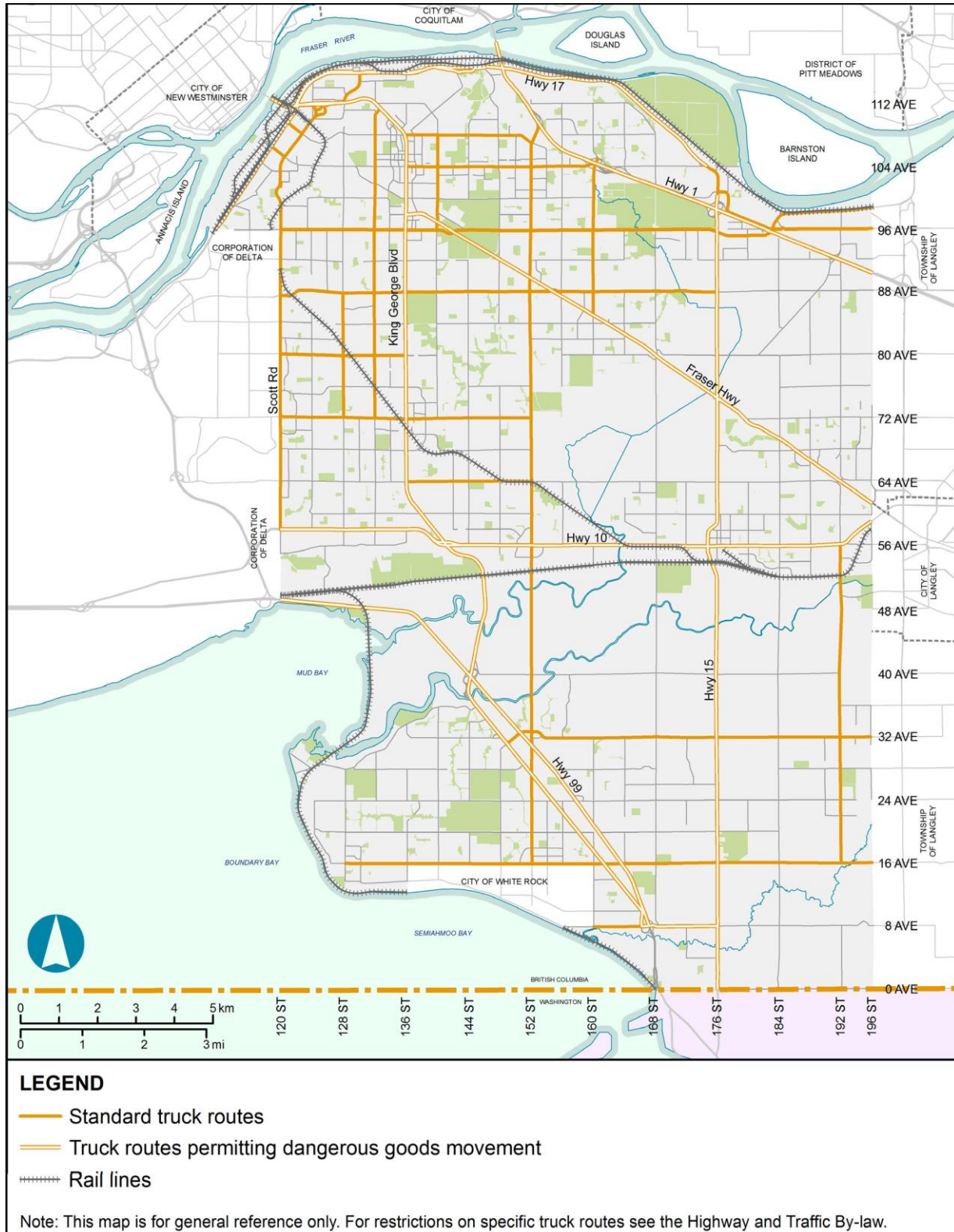
C2 POLICIES: Goods Movement

- C2.34 Collaborate with senior levels of government and TransLink to improve efficiencies along strategic goods movement corridors, including those accessing Port facilities, International Border crossings, rail lines and major river crossings, in order to support economic development.
- C2.35 Complete a grid road pattern consisting of arterial and major collector roads that link all areas of Surrey (see Figure 27). Support the movement of goods throughout the city by relying on established Designated Truck Routes (see Figure 30).
- C2.36 Reduce potential conflicts by considering the location of goods movement and designated truck routes, including designated dangerous goods corridors, in the planning and approval of new residential development.
- C2.37 Reduce the impact of truck traffic noise on neighbourhoods by maintaining and enforcing existing and designated truck routes (see Figure 30) and by using setback reductions, landscaping installations and building orientation and acoustical upgrades.
- C2.38 Ensure the loading and on-site access of goods delivery vehicles is considered in conjunction with overall urban development site design.
- C2.39 Encourage Federal and Provincial initiatives to achieve grade separation at railway crossings to improve safety and reduce delays and train whistle noise impacts.



Rail lines, ports and highways are integral elements in Surrey's goods movement infrastructure.

Figure 30: Goods Movement



C2 POLICIES: Transportation Demand Management

- C2.40 Support and promote Transportation Demand Management (TDM) measures and initiatives that result in changes to transportation modes, trip reductions, Single Occupancy Vehicle use reductions and a shift in travel behavior such as:
- Carpooling
 - Car-sharing
 - Parking management
 - Alternative transportation information
 - High-occupancy vehicle lanes.
- C2.41 Promote initiatives to reduce congestion, air pollution and greenhouse gas emissions by supporting the reduction of the number and length of trips made by single-occupancy vehicles.
- C2.42 Promote alternatives to car travel by improving access to safe and convenient walking and cycling opportunities, including convenient access to transit stops.
- C2.43 Collaborate with TransLink and other Metro Vancouver local governments on regional Transportation Demand Management (TDM) initiatives.

C2 POLICIES: Parking

- C2.44 Support the strategic location of off-street parking facilities to promote pedestrian use and Park-n-Ride opportunities, particularly within Town Centres, the City Centre and strategic transit hubs.
- C2.45 Review off-street parking regulations to ensure appropriate parking is provided to support sustainable land uses and to promote sustainable transportation (e.g. provide electric vehicle charging stations).
- C2.46 Consider policies that permit cash-in-lieu payments for off-street parking provisions in order to support capital projects, in appropriate locations, that provide new transit infrastructure and centralized parking facilities.



Prioritizing co-op parking increases the convenience of not owning a vehicle.

C2 POLICIES: Implementation

- C2.47 Require the preparation of a Transportation Impact Study for new significant developments that examines issues including, but not limited to, the following:
- Impacts of development-generated traffic, for all modes, on the capacity of adjacent or nearby transportation corridors
 - Need for modifications to transportation infrastructure to support and/or accommodate new development
 - Potential for incorporating Transportation Demand Management measures into new developments.
- C2.48 Investigate the means needed to reduce reliance on property taxes to fund transportation improvements by establishing secure, sustainable and predictable funding streams and by securing additional funding through federal, provincial and private agency grant programs.



Existing transit exchange within the Surrey's City

C3

Parks and Greenways

Maintain an integrated network of parks and greenways to promote healthy living and connect the City and its neighbourhoods

Surrey's identity is intrinsically connected to its parks and green spaces. This legacy is imprinted on the physical form of the city and in the minds of residents. To build on this identity and meet evolving objectives for active transportation, Surrey envisions connecting green spaces and community destinations with off-road pedestrian and bicycle pathways.

Surrey maintains over 2,500 ha (6,000 ac) of public parkland and has several significant regional parks maintained by Metro Vancouver. These parks provide excellent opportunities for residents and visitors to enjoy healthy lifestyles as well as organized and informal activities.

The City of Surrey is committed to developing a comprehensive network of greenways to connect its parks with its green spaces and to augment Surrey's on-street bike routes, sidewalks and park trails.

In order to meet Objective C3, the City will:

C3 POLICIES: General

- C3.1 Support equitable distribution and access to high quality public parks and greenways throughout Surrey (see Figures 31 and 34).
- C3.2 Maintain a hierarchy of parks that permit a variety of uses and activities according to the scale of park and the area it serves.
- C3.3 Implement the *Parks, Recreation and Culture Strategic Plan* (as amended) by supporting a parkland provision ratio of 4.2 ha of park per 1,000 residents, in the following categories (see Figure 32):
- City Parks (1.2 ha per 1,000 residents)
 - Community Parks (1.0 ha per 1,000 residents)
 - Neighbourhood Parks (1.2 ha per 1,000 residents)
 - Nature Preserve and Habitat Corridors (0.8 ha per 1,000 residents).
- C3.4 Maintain a long-range park capital improvement program that balances acquisitions, development and operations, provides a process and criteria for capital improvement project selection and emphasizes creative and flexible financing strategies.



Darts Hill Garden Park, Surrey

Figure 31: Parks

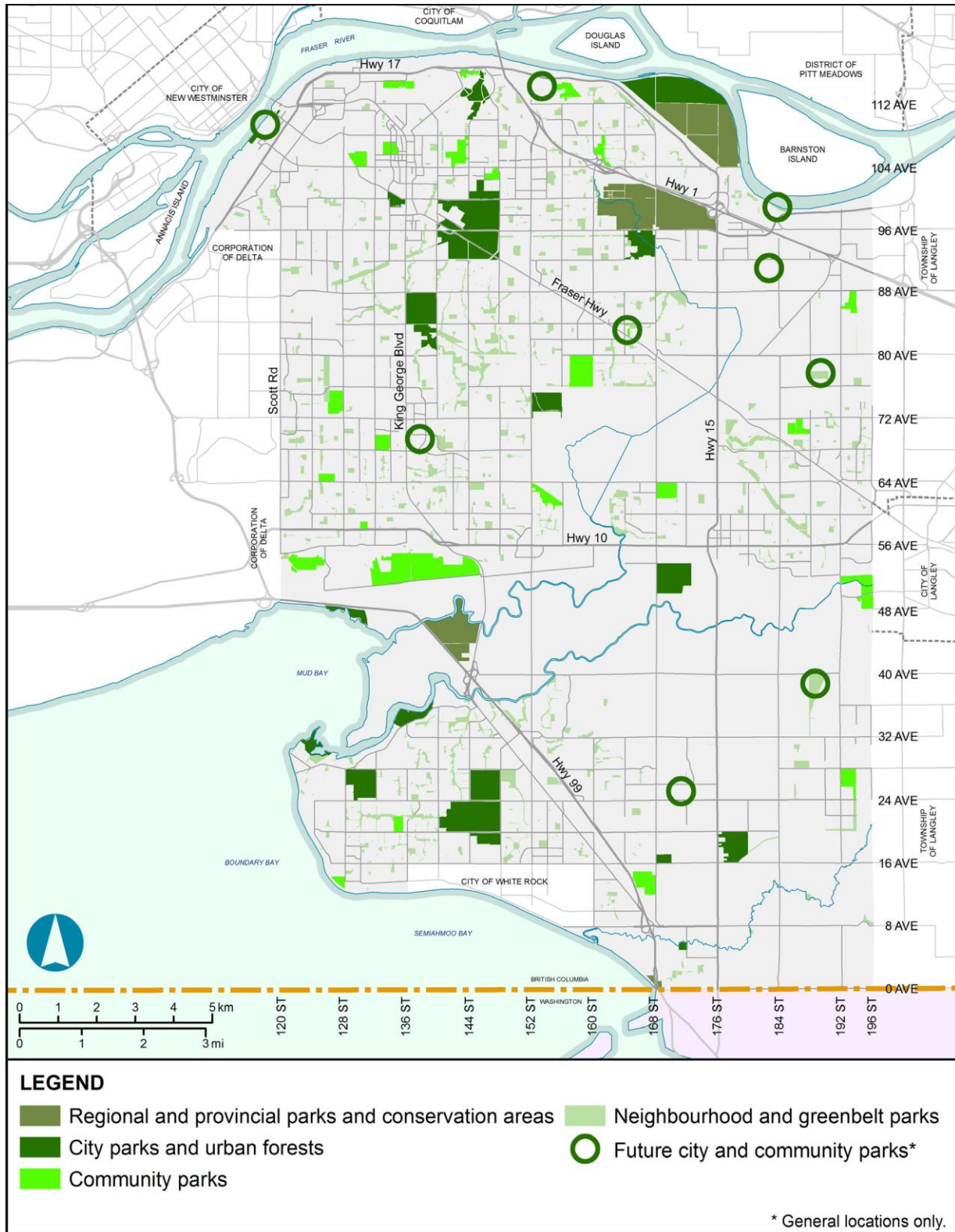


Figure 32: Park Types

The *Parks, Recreation and Culture Strategic Plan* identifies different categories of parks within Surrey and what their intended roles are for serving the recreational, cultural and environmental protection needs of Surrey’s residents. Classifying these parks helps determine how additional spaces can be acquired over time.

City Parks:

embody the identity and image of the city and are readily recognizable by residents as places for city-wide celebration and activity; or are of unique natural significance. Festivals and cultural events occur at these parks while still leaving opportunities for day-to-day informal use. Bear Creek, Crescent and Redwood Parks are examples of City Parks.

Community Parks:

contain amenities that attract residents from outside of their immediate neighbourhood. Are often the focus for special events and large-scale active recreation and can be comprised of sports fields, natural areas and passive amenities.

Neighbourhood Parks:

are intended to serve the needs, and be within walking distance, of an immediate neighbourhood. May be a small, local park, a linear park connection with pathways leading to a larger park or other neighbourhood amenity, or they may be a mini-plaza or plaza in a high density area. Neighbourhood Parks adjacent to elementary schools often also include playgrounds as a community use.

Nature Preserves/Habitat Corridors:

vary in size and are used to protect the intrinsic value of natural areas for biodiversity conservation and habitat connectivity. Examples include riparian corridors, steep slopes, flood plains, terrestrial corridors, significant tree stands and large habitat hubs. Some public access is available through trails and viewpoints but the primary function is for preservation, not recreation.



Image credit: City of Surrey, *Sensitive Urban Infill Design Charter Report*

C3 POLICIES: Parks Acquisition and Planning

- C3.5 Maintain a parks acquisition program that defines acquisition tools and that targets potential sites for the following priorities:
- growing neighbourhoods with a limited supply of park land and other public-accessible open spaces
 - natural areas and habitat corridors for the protection, preservation and restoration of habitat and ecosystem features
 - linear waterfront access rights to shorelines and beaches
 - larger land holdings to provide usable space for community benefit.
- C3.6 Acquire land in advance of future development, where possible, and where it is determined that parks and other open spaces and linkages may be required to serve the community's needs for recreation, conservation or environmental protection.
- C3.7 Plan appropriate sizes, locations and types of parks and greenways in conjunction with land uses, densities and transportation networks in all secondary plan processes (e.g. Neighbourhood Concept Plans, Local Area Plans, Infill Areas and Town Centre Plans) to support the parkland needs of future residents.
- C3.8 Monitor land use proposals in Town Centres and Secondary Plan areas to ensure that:
- land is provided for park purposes sufficient to meet anticipated demand
 - opportunities for improvement of existing park land are identified
 - usable, on-site public and semi-private open space is provided, on development sites.
- C3.9 Create large destination parks, such as Green Timbers Urban Forest Park, and waterfront parks along the Fraser River, that function as city-wide and regional attractions.
- C3.10 Utilize the City's available information on biodiversity conservation to target the acquisition of parkland for conservation and to enhance Surrey's Green Infrastructure Network.
- C3.11 Develop large, centrally-located parks in City Centre and each Town Centre, as community gathering places.
- C3.12 Develop and maintain sports field complexes throughout the city to ensure adequate and equitable access for sports groups. Ensure the provision of sports facilities are appropriate to the local community context.
- C3.13 Provide recreational access to the Fraser River and Boundary Bay waterfronts through parkland acquisition and trail development.



Redwood Park, Surrey.

C3 POLICIES: Parks Acquisition and Planning (cont.)

- C3.14 Provide mini-parks and plazas within walking distance of residential areas in dense urban communities (e.g. City Centre and Town Centres) where population growth makes it difficult to meet per capita park provision standards.
- C3.15 Work toward providing a park within walking distance (400-600 m) from all multiple family residences.
- C3.16 Promote the importance of older, larger community Heritage Parks, (e.g. Elgin, Port Kells and Redwood Heritage Parks) as an ongoing physical reminder of the historic character of Surrey.
- C3.17 Consider creating wide linear parks and greenways as agricultural buffers in new neighbourhoods where they are adjacent to farmland.
- C3.18 Incorporate the planning of active neighbourhood parks with elementary school sites and the planning of community parks with secondary school sites. Parkland and adjacent school sites should share open space and facilities to promote safety, accessibility and space and land use efficiencies.
- C3.19 Work toward entering into Joint Use Agreements with School District No. 36 to guide coordinated park programming and maintenance.
- C3.20 Collaborate with Metro Vancouver and neighbouring municipalities to ensure the delivery of complimentary and coordinated parks and greenway systems.

C3 POLICIES: Parks Development

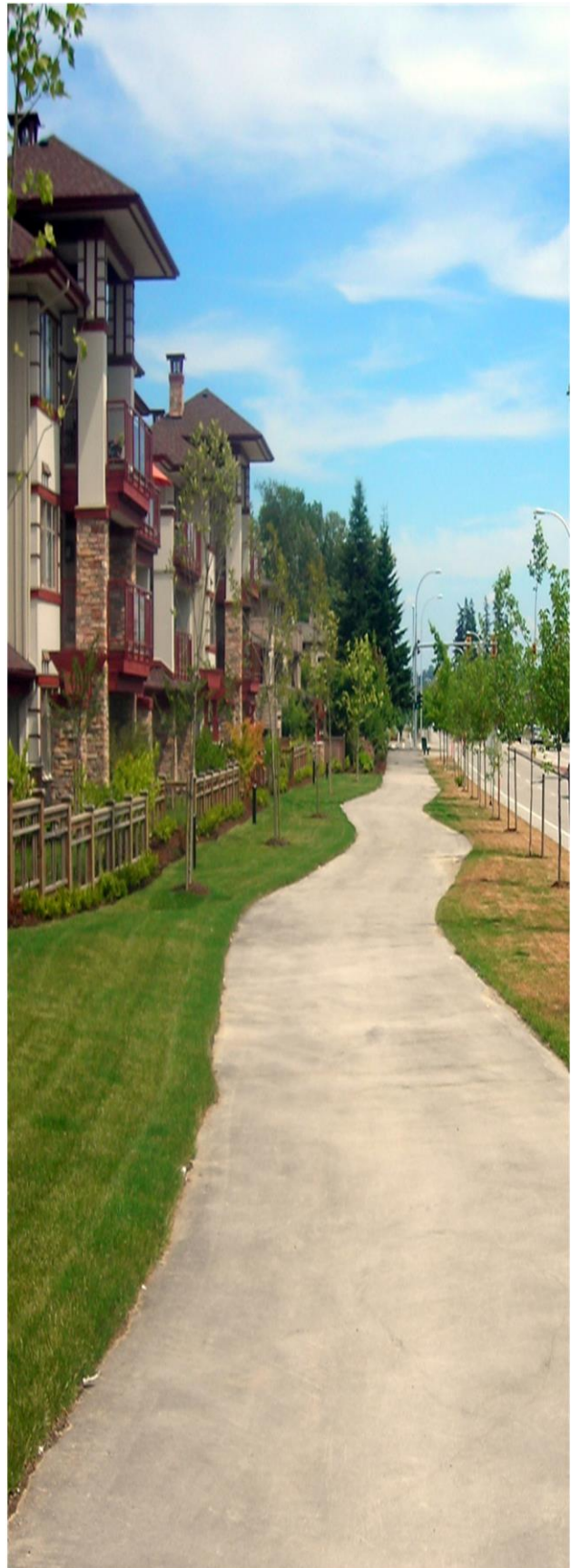
- C3.21 Design and develop new parks in consultation with neighbourhood residents so that facilities and uses are appropriate to neighbourhood context and with ecological objectives.
- C3.22 Implement the recommendations of Surrey's *Dog Off-Leash Area Strategy* (as amended) to balance the needs of dog owners with other park users.
- C3.23 Integrate stormwater management facilities into parkland in a functional and attractive manner while simultaneously retaining and/or enhancing environmental functions for habitat preservation.
- C3.24 Where parks or greenways are located adjacent to agricultural areas, ensure the protection of agriculture and the efficient and viable operations of farms by using fencing, barrier plantings and setback restrictions to reduce potential conflicts.



Dogwood Park, Dog Off-Leash Area, Surrey

C3 POLICIES: Greenways

- C3.25 Plan and implement a hierarchy of regional, city-wide and neighbourhood-level greenways, in keeping with Surrey's *Greenways Plan* (as amended) (see Figure 33).
- C3.26 Develop an integrated network of recreational greenways and multi-use paths that connect Town Centres, major parks, schools, transit facilities, community destinations and neighbourhoods (see Figure 34). Link to regional destinations and greenways, in accordance with Surrey's *Parks, Recreation and Culture Strategic Plan*, *Cycling Plan*, *Walking Plan* and the *Greenways Plan* (as amended).
- C3.27 Work with other government organizations to promote access and to complete a regional network of continuous pathways along the Fraser River.
- C3.28 Identify and promote the expanded use of utility corridors including hydro and gas Rights-of-Way as greenways.
- C3.29 Design greenways with a more urban character within Surrey's City Centre and Town Centres, compared with greenways located within lower density residential and rural areas.
- C3.30 Enhance the user's experience of greenways and multi-use pathways by using amenities such as benches, viewpoints, public art installations, heritage markers, shelters, information kiosks and plantings at appropriate and strategic locations.
- C3.31 Develop and maintain appropriate design standards for greenway widths, grades, surfacing, signage, fencing, landscaping and lighting.



Surrey greenway adjacent to arterial road

Figure 33: Greenways

Surrey’s greenways network is made up of loops and connectors. A greenway loop is proposed around each of Surrey’s six distinct communities whereas a greenway connector is proposed between each community. The greenway network creates an interconnection between major parks, natural areas and recreation facilities. Greenways are a significant component of the Active Transportation Network and offer community benefits for Surrey’s residents in areas of personal well being, social and cultural aspects, economic activity and environmental stewardship. These benefits are explored in more detail below:

Personal Well Being

Greenways help encourage physical fitness and healthy lifestyles for all age groups, income levels and cultural backgrounds. They create new active living opportunities, provide safe transportation alternatives and allow users to engage with and perceive the physical, social and natural details of their community, contributing to spiritual and mental wellness.

Social and Cultural Aspects

Greenways provide a myriad of social and cultural benefits including providing venues for social interaction and the ability to foster community involvement by providing partnership opportunities. Greenways also help to protect and connect users with cultural and historic assets, provide areas for natural learning and encourage youth to find entertainment in healthy environments.

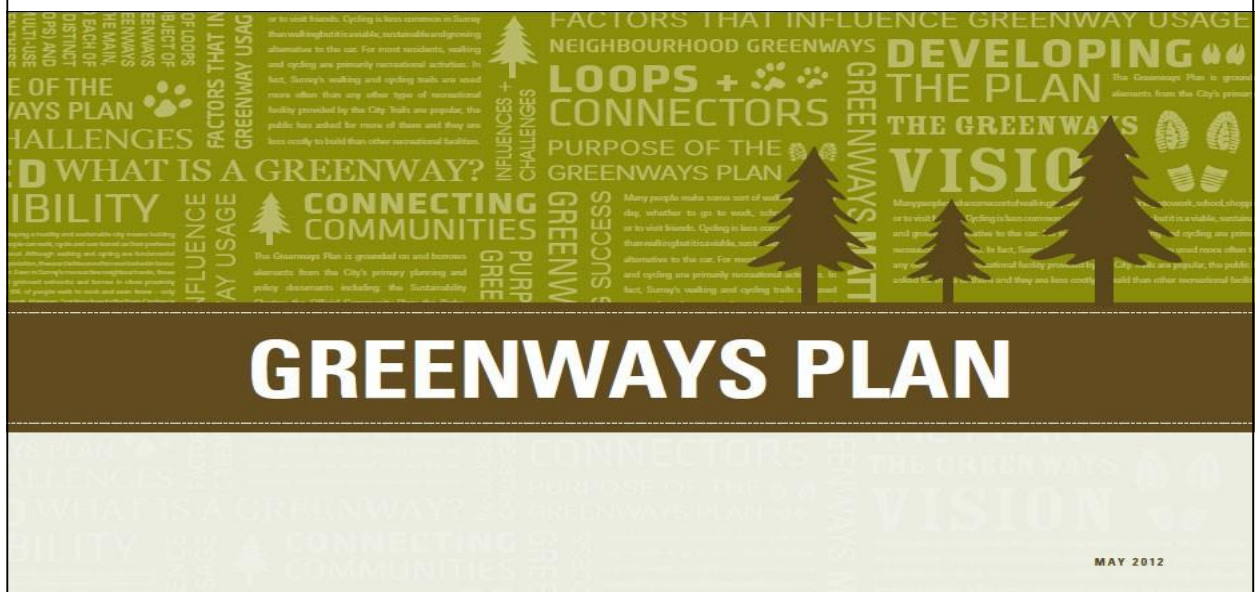
Economic Benefits

Greenways are the most desired and least expensive recreation facility the City provides. A stimulant to tourism and money spent within Surrey for recreation-based activities, greenways add to the local economy and simultaneously improve the image and attractiveness of the city as a place to establish a business and as a place to live.

Environmental Stewardship

Greenways can provide an important connection to sustainability and environmental protection by allowing for the preservation of green space, improving air quality and creating corridors that improve habitat connections and maneuverability.

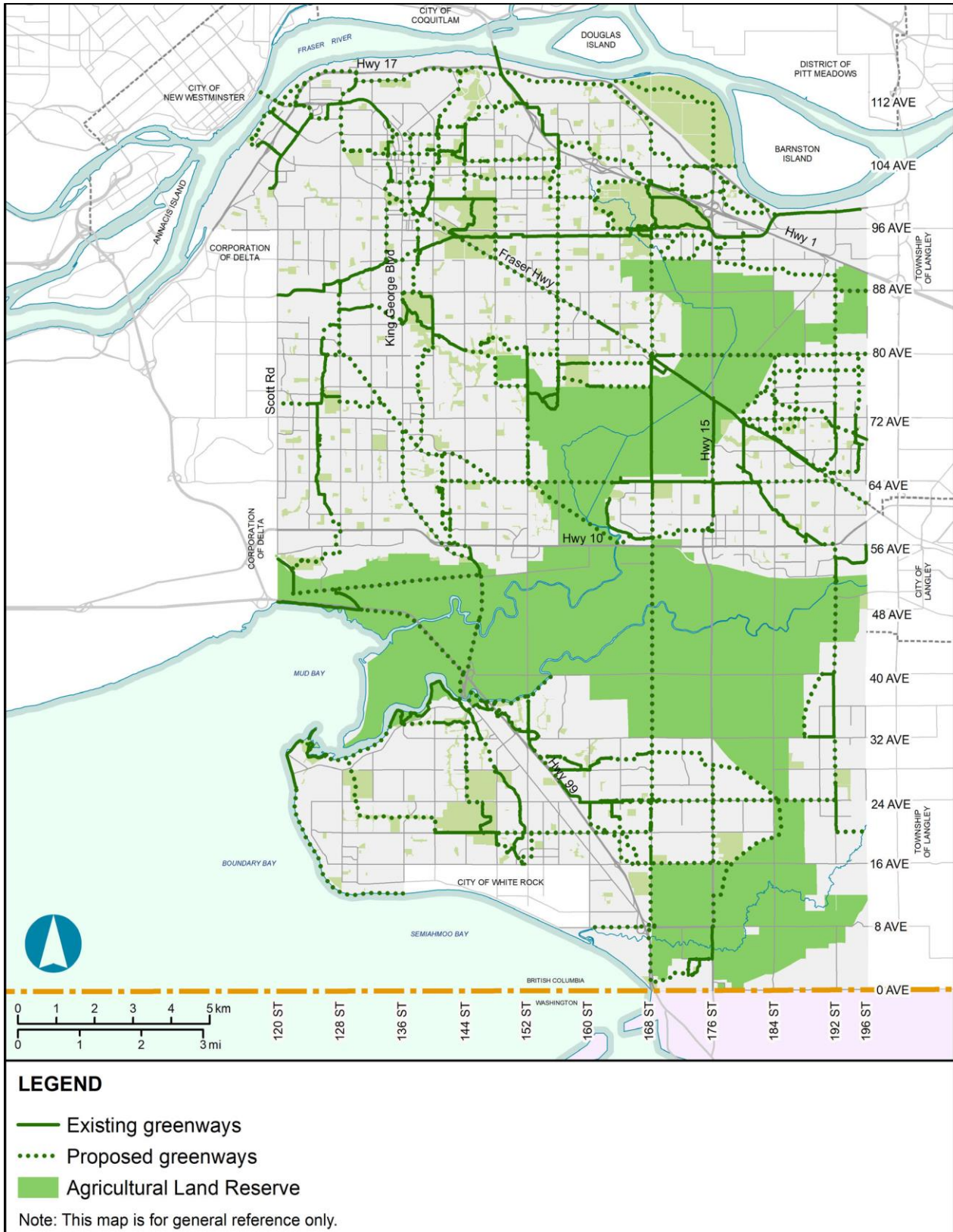
The City’s greenways were created from over 50 years of constructing trails where opportunities presented themselves. Gaps in the system remain that the City is working on completing (see Figure 34). Connecting gaps will continue to take place over time and in conjunction with adjacent development opportunities. Once complete, Surrey’s Greenway Network will span the entire city, to and from, all four corners.



City of Surrey Greenways Plan (2012)

Figure 34: Greenways Network

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C3 POLICIES: Greenways (cont.)

- C3.32 Provide a safe user environment by incorporating Crime Prevention Through Environmental Design (CPTED) principles into the planning, construction, assessment and maintenance of greenways, including the consideration of surrounding land uses, lighting and signage.
- C3.33 Pursue universal accessibility throughout the greenway network by considering the needs of all users regarding pathway grades, surface materials, traffic signals and signage.
- C3.34 Review greenways proposed to pass through the Agricultural Land Reserve (ALR) with the Surrey Agriculture and Food Security Advisory Committee (AFSAC) and with the Agricultural Land Commission (ALC) prior to any application being submitted to the Agricultural Land Commission.
- C3.35 Ensure environmental protection where greenways are located along or through sensitive ecosystems or where they cross watercourses and/or Riparian Areas.
- C3.36 Implement interim measures such as using existing sidewalks, road shoulders, park trails and local roads as temporary greenways connections until full implementation of the network is completed.



Seasonal trail along edge of Mud Bay, Surrey

C3 POLICIES: Implementation

- C3.37 Use all available tools to acquire or gain access to land for park and recreational use, including donations, bequests, long-term leases, joint use agreements, Development Cost Charges (DCC), subdivision Cash-in-Lieu payments, density bonusing and amenity contributions.

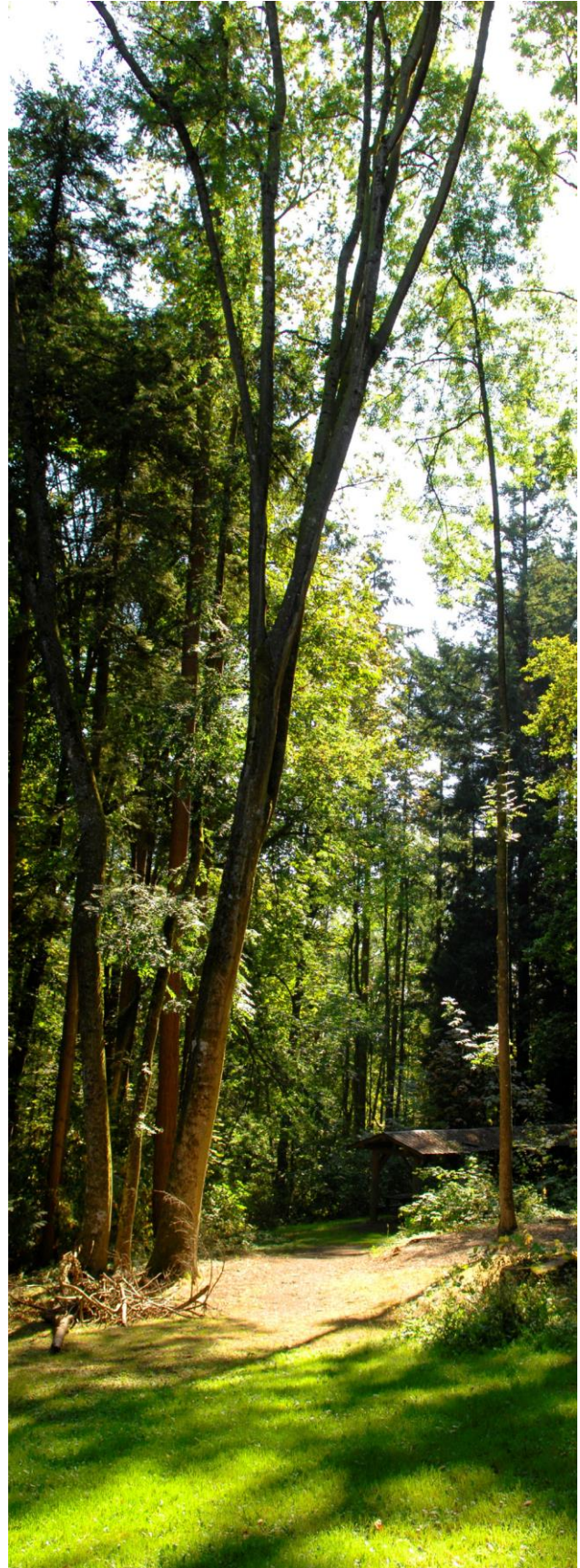
- C3.38 Pursue the conveyance of riparian areas to the City of Surrey through the subdivision and development process in order to ensure the consistent management of natural environmental values in Riparian corridors.

- C3.39 Establish and maintain funding mechanisms such as parkland DCCs, amenity contributions and parkland impact fees to support the acquisition and development of parkland that serves the needs of residents in new developments.

- C3.40 Utilize property or funds collected through the required five percent (5%) subdivision parkland dedication to secure land suitable for public recreation such as active and passive parkland, playgrounds and pathways.

- C3.41 Secure and implement the greenways network through both public and private investment, including City capital works programs (e.g. parks, streets, drainage and utilities) and through the land development process.

- C3.42 Pursue public ownership of greenway alignments; where this is not feasible, secure public access on greenways by using easements or Statutory Rights-of-Way across private property.



Redwood Park Trail, Surrey.

C4

Civic and Recreation Facilities

Provide an equitable and integrated network of civic and recreational facilities to support community and neighbourhood life

Surrey has developed a growing inventory of civic facilities that span a range of purposes and that support and enrich the social and cultural life of the community. These include: the Surrey Museum, Surrey Art Gallery, heritage sites, recreation centres that provide space for community meetings, child care services, educational programs and cultural expression, community-based arts and culture spaces, libraries that serve as community learning hubs, and fire halls that allow rapid response to emergencies in all areas of the city.

The Surrey City Centre Civic Precinct includes City Hall, the Civic Plaza and the Central City Library and provides a new focal space for community life. Each Town Centre also includes community and recreation centres that are appropriate to the unique needs of each community.

In order to meet Objective C4, the City will:

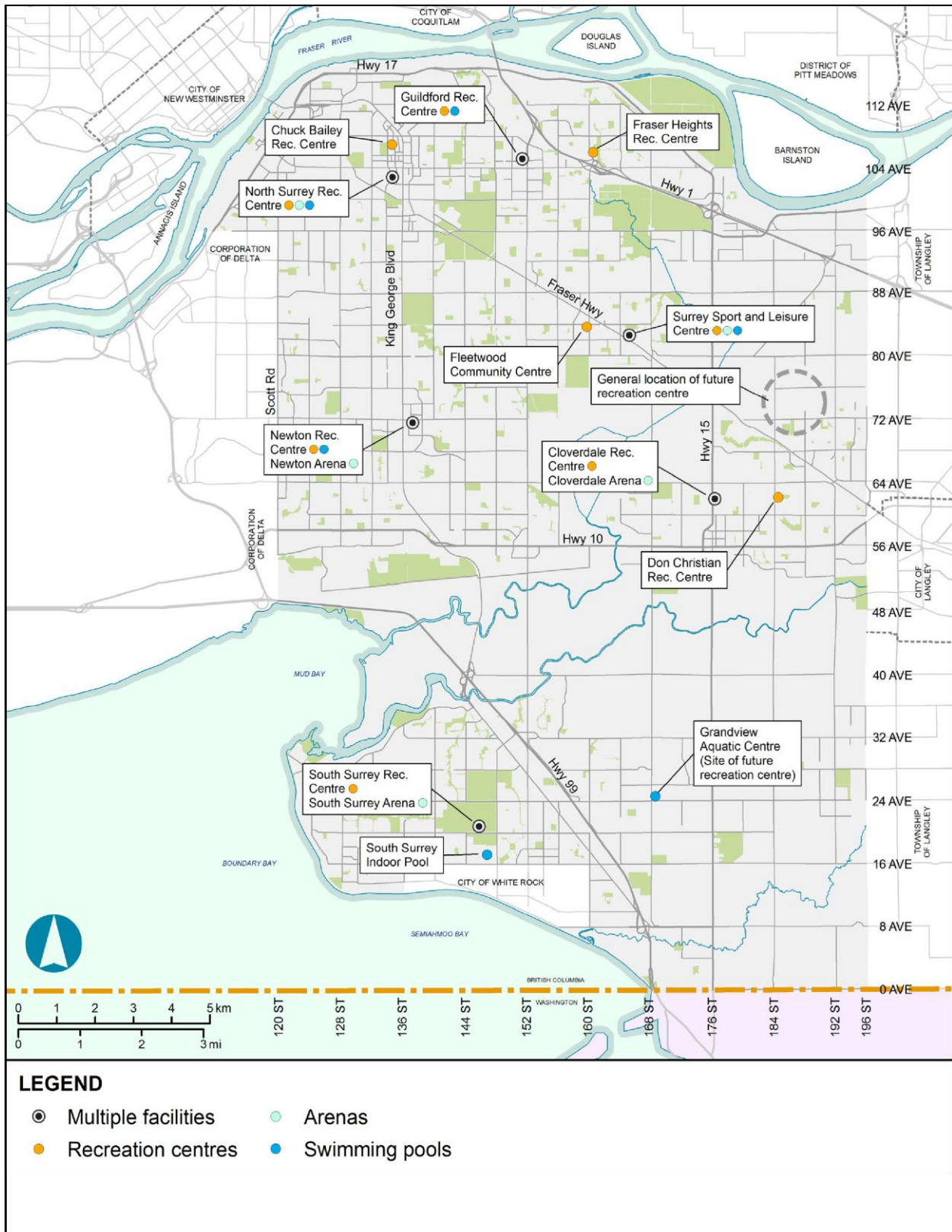
C4 POLICIES: General

- C4.1 Develop a comprehensive plan that identifies how civic facilities will be adequately provided throughout Surrey in order to meet the needs of the City. These facilities should include recreation centres, community centres, libraries, cultural facilities, police and fire facilities, municipal works yards and government buildings (see Figure 35, 46, 47 and 49).
- C4.2 Maintain and expand existing community recreation and civic facilities as community hubs within each Town Centre and in emerging centres such as Clayton, Grandview, South Port Kells and Fraser Heights.
- C4.3 Emphasize sustainable design including energy efficiency and renewable energy systems in all City facilities in order to reduce lifestyle facility costs and reduce greenhouse gas emissions.
- C4.4 Emphasize a high standard of architecture, landscape and urban design in the development and renovation of community facilities, including the integration of public art installations.



Surrey's Civic Plaza accommodates community events including Part of the Planet shown here.

Figure 35: Recreation Facilities



C4 POLICIES: Recreation Centres and Facilities

- C4.5 Provide indoor recreation centres in each community with an emphasis on central locations in existing Town Centres and emerging neighbourhood centres.
- C4.6 Plan public community recreation centres that are sensitively integrated into their community and neighbourhood context.
- C4.7 Co-locate indoor and outdoor facilities, where feasible, so that recreation facilities and sites are safe, efficiently used and maintained and provide a focus and meeting place for all demographic groups.
- C4.8 Locate new recreation centres so they are easily and safely accessible from surrounding neighbourhoods by walking, cycling and transit.
- C4.9 Develop facility plans and design programs for community recreation centres in consultation with user groups and local communities.

C4 POLICIES: Other Civic Facilities

- C4.10 Plan for new and expand existing Fire Halls in response to urban growth and development to provide adequate emergency response times as set out in the *Surrey Fire and Rescue Service Master Plan* (as amended).
- C4.11 Coordinate with the Surrey RCMP to locate community policing facilities within each of Surrey’s Town Centres and in emerging population centres, to ensure operational efficiency and community access to police services.
- C4.12 Coordinate with Surrey Libraries to locate new library branches and to expand existing libraries in areas of growth in the city, with a focus on Town Centres and emerging population centres such as Clayton and Grandview, in order to ensure wide public access to Surrey’s library branches.
- C4.13 Continue to expand heritage facilities, (e.g. the Surrey Museum) and performing and visual arts facilities (e.g. Surrey Art Gallery). Pursue the development of cultural facilities in Surrey’s City Centre and in South Surrey to compliment existing arts and heritage facilities in Newton and Cloverdale.



New Guildford Aquatic Centre; artistic rendering.

C4 POLICIES: Partnerships

- C4.14 Pursue partnerships and collaborations to coordinate the development of new civic facilities with other public facilities, such as libraries and schools, to create community hubs and to achieve efficiencies in facilities and programming.
- C4.15 Consider opportunities to integrate space for community groups, non-profit organizations, childcare operations and other community programs into civic facilities in consultations with community groups and stakeholders.
- C4.16 Consider public-private recreation partnerships, where appropriate, where non-profit operators such as the YM-YWCA operate publically-accessible private recreation facilities on public lands or within existing shared facilities.

C4 POLICIES: Implementation

- C4.17 Develop and maintain a capital funding plan for future development and expansion of civic facilities.
- C4.18 Vigorously pursue senior government grants and matching funding programs to support the development of community facilities throughout Surrey.
- C4.19 Consider using incentives such as bonus density provisions for private-sector development, to deliver space for community facilities such as cultural space, childcare space and community meeting space in appropriate locations and specifically within Surrey's Town Centers.



Cloverdale Recreation Centre



THEME D: ECOSYSTEMS



THEME D: Introduction



While Surrey has been settled by people of European descent for almost 150 years and virtually all of its original forests and wetlands have been modified by human activity, the City still contains diverse and healthy natural ecosystems that support a rich biodiversity. These ecosystems include extensive tidal flats around Boundary Bay, salmon-bearing streams and their riparian corridors, mature second-growth forests, wetlands, old fields and meadows and successional woodlands. Many of the most significant ecosystems are protected in Regional or City parks or in wildlife management areas; however, other important natural areas exist on private property that have urban development or agricultural potential.

The City of Surrey is committed to identifying and protecting its significant ecosystems and the elements supporting its biodiversity, and managing these public lands to maintain a healthy natural environment. The City also influences the management of private lands that contribute to ecological health through regulation and by encouraging best practices.

THEME D: Objectives

1. Identify, protect, enhance and manage Surrey's biodiversity and network of significant natural ecosystems.
2. Reduce exposure to natural hazards through the appropriate location and design of development.
3. Encourage and implement greener development and building practices to improve water, air, soil and habitat quality.
4. Design a community that is energy-efficient, reduces carbon emissions and adapts to a changing climate.

THEME D: ECOSYSTEMS

Protect and Enhance Healthy Ecosystems and Practice Sustainable Development

Sections

D1 Green Infrastructure and Ecosystem Management

D2 Hazard Lands

D3 Greener Site Development

D4 Energy, Emissions and Climate Resiliency



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D1 Green Infrastructure and Ecosystem Management

Identify, protect, enhance and manage Surrey’s biodiversity and network of significant natural ecosystems

The foundation of ecosystem management is the identification and protection of sensitive and significant ecosystems. The protection of Surrey’s natural ecology, one of the City’s most valuable assets, will ensure Surrey remains a highly livable, attractive and memorable place to live, work or play.

The *Ecosystem Management Strategy* (EMS) identifies a Green Infrastructure Network (GIN) made up of natural elements that exist at a site, neighbourhood, community, or regional scale. The GIN is a natural interconnected network that conserves natural ecosystem values and functions and that sustains clean air and water. The GIN provides a wide array of benefits to people and wildlife and helps the City of Surrey establish priorities for environmental management.

In order to meet Objective D1, the City will:

D1 POLICIES: General

- D1.1 Utilize the *Ecosystem Management Strategy* and the *Biodiversity Conservation Strategy* (as amended) to guide the management and protection of Surrey’s diverse ecosystems.



City of Surrey Biodiversity Conservation Strategy (2013).

D1 POLICIES: Green Infrastructure Network

- D1.2 Establish plans, strategies and policies to protect, enhance and manage the *Ecosystem Management Strategy* (as amended) Green Infrastructure Network (GIN) as shown in Figure 36 by:
- Strategically acquiring land for ecosystem conservation purposes
 - Identifying natural areas for protection in the preparation of Secondary Plans
 - Implementing Development Permit Guidelines for the protection of the natural environment as detailed in DP3 of the Implementation Section of this OCP.
- D1.3 Identify and continue to work toward identifying and protecting sensitive fisheries zones including aquatic habitats, wetlands and riparian areas as defined in conjunction with other agencies and as shown in Figure 37.
- D1.4 Preserve riparian areas and watercourses in their natural state and link them with upland natural areas to develop a connected network of natural areas throughout Surrey.
- D1.5 Facilitate wildlife movement and habitat protection by conserving, enhancing and promoting wildlife corridors through parks and by connecting hubs, open spaces and riparian areas.
- D1.6 Work toward protecting existing natural urban forests and natural vegetative coverage to maximize Surrey’s tree canopy and reach the target goal of 40 percent (40%) canopy coverage for the entire city
- D1.7 Develop and implement strategies for protecting and enhancing biodiversity throughout Surrey, such as:
- Employing conservation tools, for example, covenants, eco-gifting programs and land trusts and incentives, to encourage environmental protection on private lands
 - Retaining and protecting significant trees and undisturbed natural vegetation areas through the development process and the implementation of *Surrey’s Tree Protection By-law* (as amended) and other regulatory by-laws to achieve the City’s conservation objectives
 - Encouraging the clustering of development to achieve conservation objectives
 - Using natural buffers to manage transitions between development and Conservation/Recreation designated lands.
- D1.8 Encourage and promote the planting of native vegetation and trees on public and private property to increase overall tree canopy coverage and to enhance wildlife populations and habitat quality.
- D1.9 Encourage ecological restoration of riparian and/or significant natural areas to improve stream health, to support biodiversity and to improve ecological health of the Green Infrastructure Network.



Natural infrastructure systems in Surrey.

Figure 36: EMS Green Infrastructure Network (GIN)

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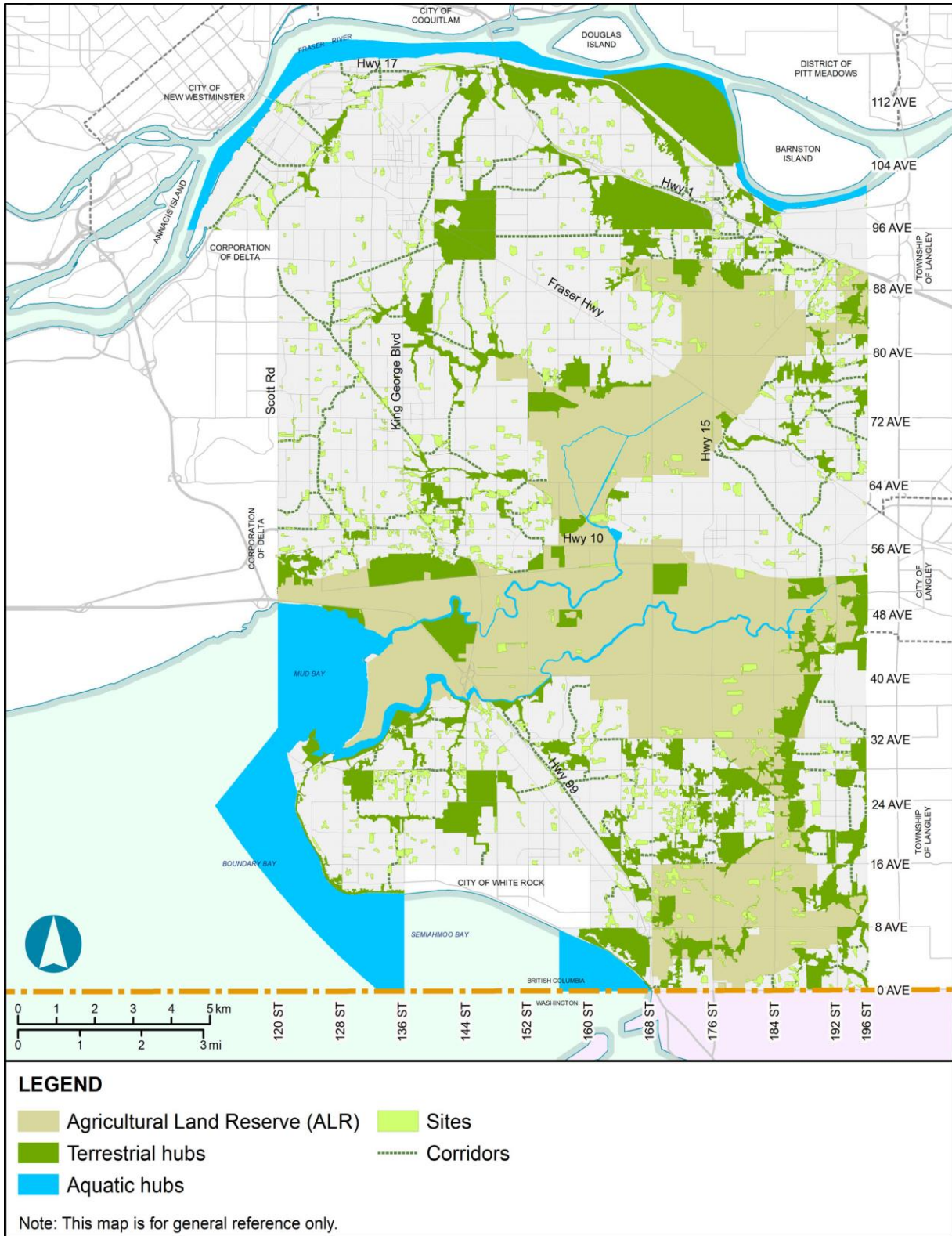
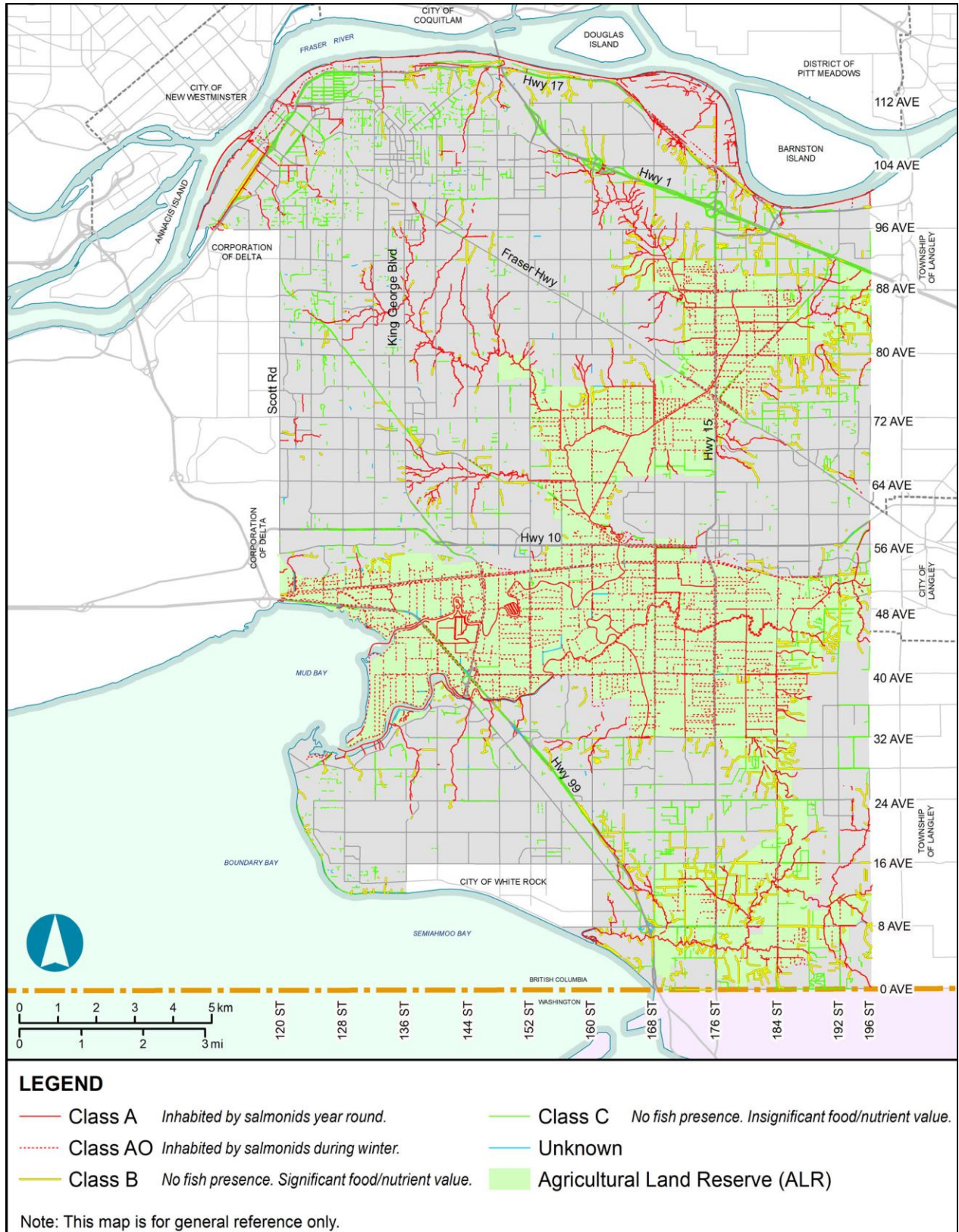


Figure 37: Fish Watercourse Classifications

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D1 POLICIES: Partnerships and Education

- D1.10 Support and partner with senior governments, Metro Vancouver and other local governments and agencies to protect sensitive ecosystems in Surrey.
- D1.11 Work cooperatively with the farming community to identify opportunities to protect and enhance wildlife habitat in agricultural lands, while recognizing the primary role of food production.
- D1.12 Work with the development community, and community stakeholders groups such as watershed stewardship groups, environmental groups and the City’s Environmental Sustainability Advisory Committee (ESAC), to identify opportunities to enhance biodiversity at all levels.
- D1.13 Continue to develop programs and information supplements that raise public awareness and understanding about ecosystem planning and management.
- D1.14 Develop educational and instructional programs that help property owners understand how to retain natural vegetative cover and how to use native and drought resistant plants.
- D1.15 Use Surrey’s Environmental Sustainability Advisory Committee (ESAC) to provide input and advice on environmental issues in relation to City policies and initiatives.

Example of signage used to educate users of sensitive ecosystem areas; located at entrance to Serpentine Wildlife Area in South Surrey.



D1 POLICIES: Implementation

- D1.16 Incorporate the protection and restoration of ecosystems and biodiversity into all Secondary Plans and Local Area Plans.
- D1.17 Consider biodiversity objectives in the design and review of all capital projects and the review of all development applications.
- D1.18 Incorporate wildlife habitat considerations into capital project planning and construction including using narrower roads, wildlife bridges or large culvert underpasses, where feasible.
- D1.19 Use Development Permit Guidelines to reduce night light pollution in urban areas.
- D1.20 Investigate the use of financial tools as a way to protect significant natural hubs and sites within Surrey's Green Infrastructure Network.
- D1.21 Explore mechanisms to facilitate a fair and equitable distribution of the costs of managing ecosystems and biodiversity in Surrey.
- D.22 Incorporate bird-friendly designs and material specifications into Development Permit Guidelines to help reduce window collisions and bird deaths in urban areas.



Mud Bay in Surrey.

D2 Hazard Lands

Reduce exposure to natural hazards through the appropriate location and design of development

Natural hazards include such forces as flooding, landslides, land erosion, seismic movements and wildfires that threaten public safety and property. These hazards pose a greater concern in certain areas than others due to slope stability, soil conditions, floodplain elevations and proximity to sources of risk. Locating and designing development to reduce the risk of exposure to natural hazards is a key component of sustainability, ensuring public safety and reducing property loss.

In order to meet Objective D2, the City will:

D2 POLICIES: General

- D2.1 Avoid development in areas subject to natural hazards including flood-prone hazard lands and steep or unstable slopes (see Figure 38 and Figure 39) to reduce exposure to risk due to natural hazards.
- D2.2 Where development or building in areas subject to natural hazards is unavoidable, permit such development only where effective protective measures are taken as certified by a Qualified Professional (see DP2 of the Implementation Section of this OCP).
- D2.3 Work with the development and construction industry, educational institutions, professional organizations, utility companies and other levels of government to make information on sustainable development costs, benefits and implementation practices readily available, easy to understand and applicable to the development environment in Surrey.



Flooding challenges/hazards in Surrey can occur frequently in lowland areas.

D2 POLICIES: Steep Slope Hazards

- D2.4 Require geotechnical assessments for development and capital projects on slopes steeper than 20 percent (20%). The location and boundaries of known Steep Slope Hazard Lands are shown conceptually (see Figure 38). These approximate locations may be revised with additional study or as development proceeds.
- D2.5 Designate Development Permit areas and implement Development Permit Guidelines for development within Steep Slope Hazard Lands, including mitigating measures or restrictions (see DP2 of the Implementation Section of this OCP).
- D2.6 Protect identified steep slopes as green space unless shown by a geotechnical or engineering study that the steep slope can accommodate a proposed development. Development will only be supported if it can be shown by a Qualified Professional that no detrimental impact is being placed on adjacent lands or on the proposed development itself. Where it is demonstrated by a Qualified Professional that development may occur on steep slopes, the developer shall provide the City with a Save Harmless Covenant. Development must proceed in strict compliance with the recommendations of the geotechnical report.
- D2.7 Require geotechnical investigations where potentially unstable soils are encountered during development. Reports by a Qualified Professional are to determine the extent of the expanse of soils and make recommendations to eliminate or mitigate any potential hazards.

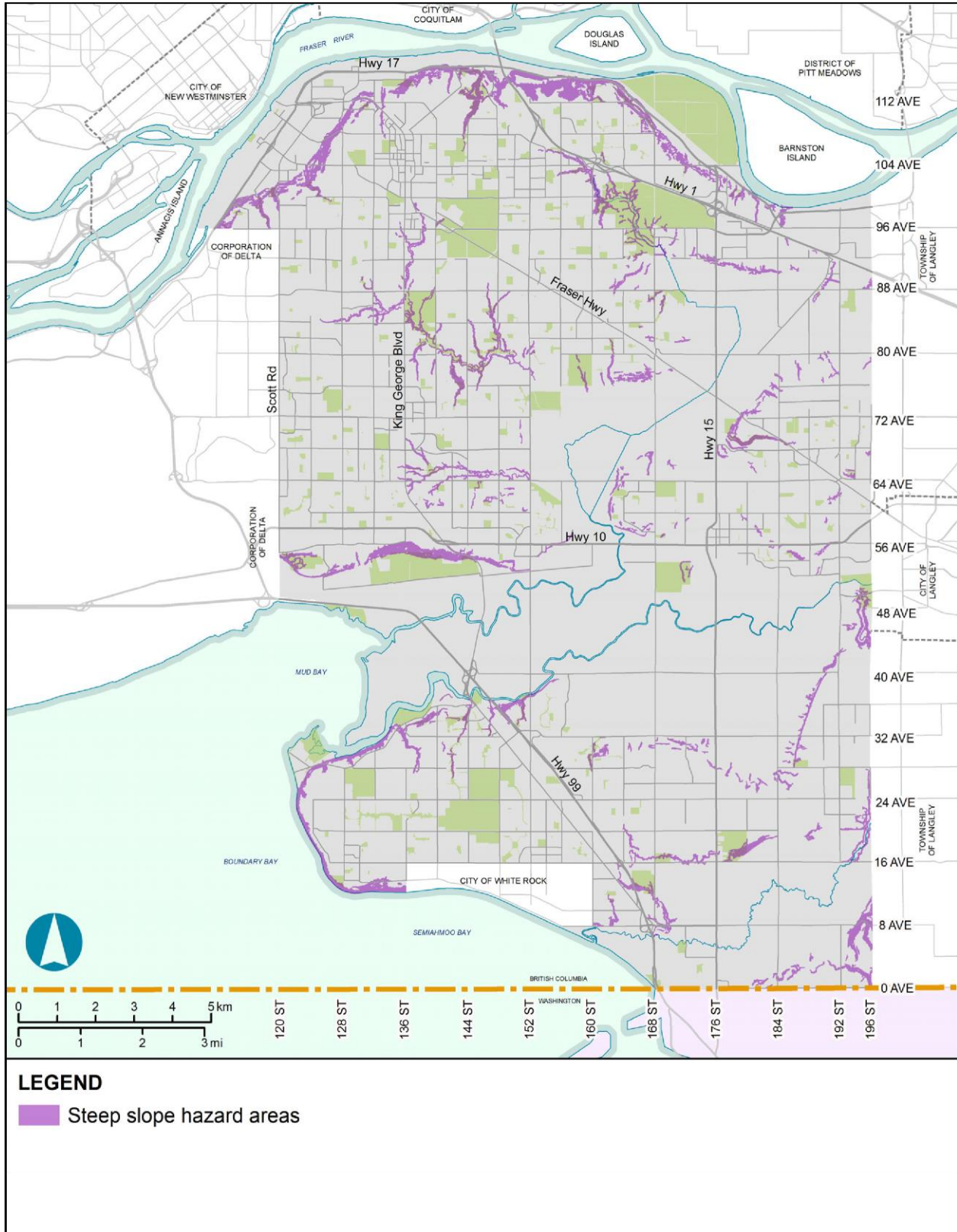
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Unstable banks and steep slope hazards in Surrey.

Figure 38: Steep Slope Hazard Areas

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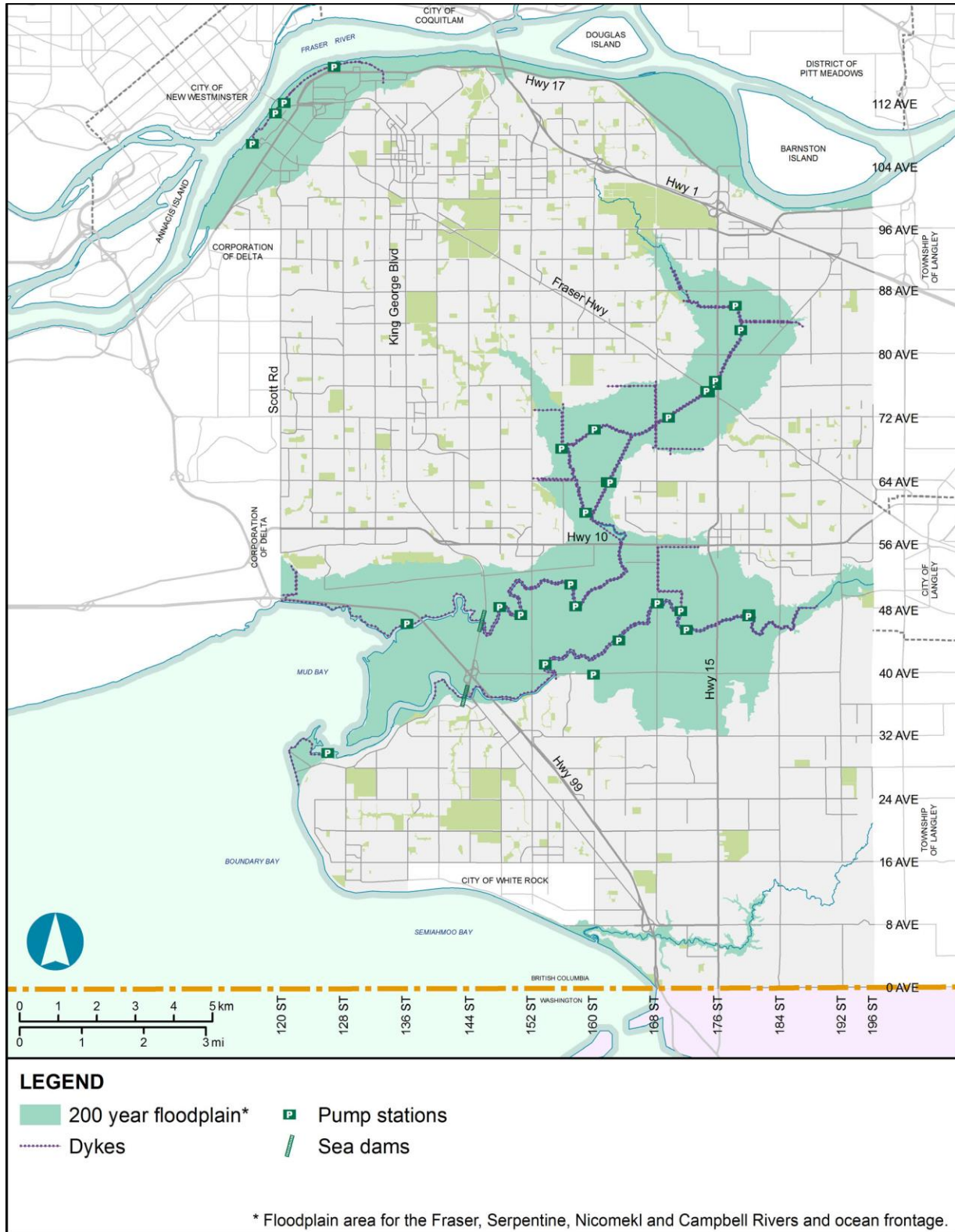
D2 POLICIES: Flood Hazards

- D2.8 Restrict new urban development in areas subject to flooding, including the 200 year floodplain of the Pacific Ocean and the Fraser, Serpentine, Nicomekl and Little Campbell Rivers, as revised from time to time (see Figure 39).
- D2.9 Permit development in areas subject to flood hazards only when it is demonstrated by a Qualified Professional that the proposal meets current flood protection requirements, at the time of development, with respect to proposed uses, building materials and required building elevations (see DP2 of the Implementation Section of this OCP). A Save Harmless Covenant indemnifying the City is required for all development within flood prone areas.
- D2.10 Consider and prepare for the projected impacts of climate change on flood hazard areas due to sea level rise and flood risk. Take into account the effects of long-term climate change such as increased flooding events, increased runoff due to development and a reduced percentage of overall mature tree cover.
- D2.11 Continue working toward meeting Agricultural and Rural Subsidiary Agreement (ARDSA) standards for flood protection in agricultural low land areas within the city.



Lowland flooding of agricultural land along the Nicomekl River in Surrey.

Figure 39: Flood Prone Hazard Areas



D3

Greener Site Development

Encourage and implement greener development and building practices to improve water, air, soil and habitat quality

“Greener development” is a term that describes when buildings, other structures and site works, such as plazas, walkways, parking areas, drainage systems and landscaping, are constructed to incorporate and enhance natural ecosystems and processes to a greater extent than conventional development. Examples of greener buildings include those that are heated or cooled using renewable energy sources that are constructed using recycled materials or that have living green roofs or walls which reduce storm runoff and provide habitat for insects and wildlife. Examples of greener site development include rain gardens, permeable pavement and parking areas, site lighting that uses renewable energy sources or plantings of native trees, and shrub communities that reduce the need for irrigation and pest control.

The City of Surrey is committed to implementing greener development practices, both in its own civic facilities and sites and in private-sector development.

In order to meet Objective D3, the City will:

D3 POLICIES: General

- D3.1 Support land development and construction that minimizes impacts on the natural environment and that enhances environmental sustainability.
- D3.2 Develop and promote educational materials and programs for developers, homeowners and professionals to encourage greener development and building practices.



Sediment from poor site development practices.

D3 POLICIES: Site Development

- D3.3 Maximize the retention of existing native vegetative cover on development sites to help provide environmental benefits by controlling development-caused erosion and runoff.

D3.4 Encourage the planting of native plants adapted to local soil and water conditions to enhance local wildlife habitat, micro-climates and air quality.

D3.5 Use and install wildlife-supporting plant communities that provide food, nutrients and habitat.

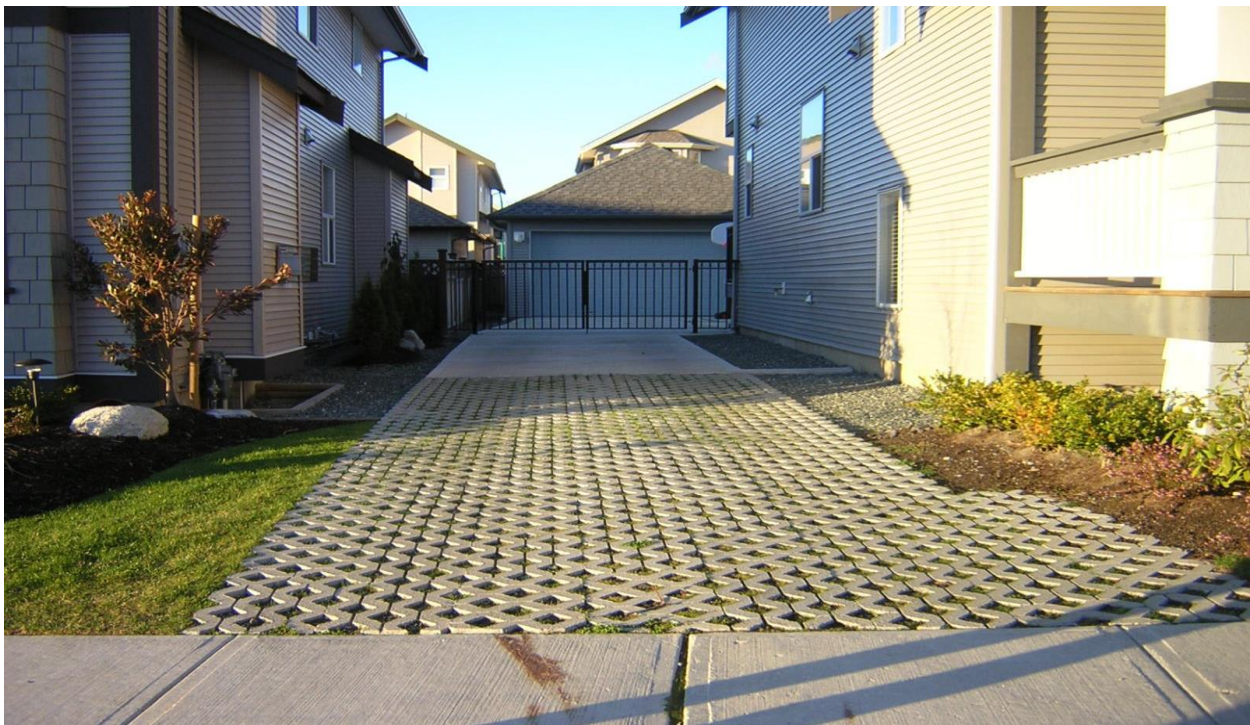
D3.6 Support sustainable development practices that reduce site erosion, maintain water quality, base flows and natural flow patterns for any receiving watercourses by limiting creek erosion, avoiding flooding and protecting aquatic habitats.

D3.7 Support site development that employs Best Management Practices that maximizes on-site infiltration of runoff and minimizes the extent of impermeable surfaces.
- D3.8 Require adequate control of sedimentation and erosion in runoff water during construction and ensure all facilities designed for erosion and sediment control meet the *Surrey Erosion and Sediment Control By-law* (as amended).

D3.9 Provide adequate growing material and soil depth on development sites and public boulevards to properly accommodate tree roots and adequate growth capacity to sustain site landscaping.

D3.10 Require the implementation of strategies for reducing construction waste and maximizing the reuse and recycling of construction material.

Green paving standards used in East Clayton neighbourhood
©Surrey.



D3 POLICIES: Buildings

- D3.11 Incorporate facilities for composting and recycling into development to reduce waste.
- D3.12 Encourage innovative housing and building design to achieve energy conservation, water conservation, waste reduction and greenhouse gas (GHG) reduction.
- D3.13 Integrate sustainable building features such as green roofs, green walls, rain gardens, on-site infiltration, clean energy systems, reduced light pollution, energy efficient buildings and wastewater recycling as well as community gardens into the design of buildings and sites.
- D3.14 Encourage the reduction, reuse and recycling of domestic water through appropriate site landscaping and building design technology.

D3 POLICIES: Implementation

- D3.15 Use the full range of tools available under the *Local Government Act* (as amended) to protect Surrey's ecosystems including, but not limited to, Development Approval Information Areas and Development Permit Areas and Guidelines, for the protection and management of the natural environment (see II(d) and II(e) of the Implementation Section of this OCP).
- D3.16 Evaluate and monitor development performance through the use of a sustainable development checklist to encourage greener development and building practices and to measure progress towards benchmarks for sustainability (see Implementation Section IV: Sustainability Indicators of this OCP).
- D3.17 Consider development incentives that support more energy efficient green development and building practices.



Landscaped islands in Surrey's City Centre also double as a garden.

D4 Energy, Emissions and Climate Resiliency

Design a community that is energy-efficient, reduces carbon emissions and adapts to a changing climate

Climate change is a significant global challenge with regional and local impacts, magnitude and timing that are not yet fully understood. Over the coming decades, it is expected that a changing climate will be characterized locally by drier summers with increasing drought and heat stress, and wetter winters with increasing flooding from the seasonal inundation of low-lying areas and from more intense storm events. It is also expected that higher global temperatures will result in:

- ⇒ sea level rise that will place pressure on Surrey’s dyking and flood control infrastructure;
- ⇒ the possibility of higher freshet levels on the Fraser River; and
- ⇒ the loss of productive agricultural lands and natural habitats.

A separate but related issue to a changing climate is the likelihood of higher energy prices in the future as conventional energy sources are depleted and more demand pressure is placed on alternative energy sources, including hydroelectricity.

The City of Surrey is committed to reducing the use of fossil-based fuels in its corporate operations and in the community as a whole in order to reduce greenhouse gas emissions and to protect residents and business from energy price increases. The City also recognizes the need to adapt to the inevitable effects of climate change in a flexible and precautionary manner in order to protect public safety, infrastructure and property interests.

In order to meet Objective D4, the City will:

The Sustainability Dashboard is an on-line platform that tracks and shares the progress of Surrey’s Sustainability Indicators over time.

SUSTAINABILITY
DASHBOARD BETA

D4 POLICIES: General

- D4.1 Implement the recommendations of Surrey’s *Community Energy and Emissions Plan* and *Corporate Emissions Action Plan* (as amended).
- D4.2a Reduce Surrey’s GHG emissions (see Figure 40) from non-agricultural and non-industrial activities to net-zero before 2050.
- D4.2b Show corporate leadership by demonstrating best practices in climate change mitigation by reducing City of Surrey corporate GHG emissions to absolute zero before 2050.
- D4.3 Support land uses, development options, transportation alternatives, built forms and infrastructure that reduce energy use and costs, integrate renewable energy sources and increase energy conservation through efficiency improvements.
- D4.4 Support the development of community-wide energy reduction targets by promoting the implementation of programs and policies that reduce energy use associated with transportation, utilities and buildings.
- D4.5 Promote the development and implementation of alternative financing strategies and mechanisms to address financial barriers associated with additional costs for efficiency and/or use of renewable energy.

D4 POLICIES: Buildings

BL 20026

- D4.6 Minimize GHG emissions from buildings by using incentives and by encouraging building design and construction to exceed the *BC Building Code* (as amended) energy efficiency standards.
- D4.7 Support building designs that allow for mixed use, combining work and living spaces to reduce the need to travel for employment purposes.
- D4.8 Consider programs that advance the construction of energy-efficient development and encourage the use of, or provide incentives for, energy efficient retrofits in existing commercial, institutional and residential buildings.
- D4.9 Support building and landscaping designs that increase energy efficiency by encouraging developers to take building orientation and the local climate into consideration (e.g. passive solar building design) as part of the overall development and site design. (See DP1 of the Implementation Section of this OCP.)
- D4.10 Explore implementing requirements for new developments to accommodate infrastructure for solar hot water and/or electric vehicle charging stations.



Surrey's portable Sustainability House provides interactive information about sustainability issues.

Figure 40: Climate Change and Greenhouse Gases (GHG)

Addressing the effects of Climate Change has been referred to as the greatest challenge of our time. There is strong evidence to suggest that Climate Change is the result of greenhouse gas (GHG) emissions from human activities, primarily the burning of fossil fuels and the methane released from agricultural practices, which have the effect of retaining the sun’s energy and warming the planet above natural temperatures. The identified impacts of this warming include the loss of polar ice caps, sea level rises that threaten to flood low-lying areas, a significant increase in the number of severe weather events and the devastation of British Columbia’s forests (e.g. pine beetle infestation). If uncontrolled, it is predicted that warming could lead to the mass extinctions of one third of the planet’s species.

Transportation accounts for approximately 62% of GHG emissions in Surrey while buildings systems account for approximately 35%. Land use policies can influence transportation impacts through density and form of development while design and construction practices can make a significant impact on the energy required for buildings (e.g. heating, cooling and lighting).

To meet the extraordinary challenge of effectively dealing with Climate Change, the Provincial government has legislated significant reductions in GHG emissions within British Columbia. Furthermore, the BC government requires local governments to establish their own GHG reduction targets and to implement policies and actions to meet these targets.

Addressing Climate Change involves the two inter-related components of mitigation and adaptation, defined as:

Mitigation : policy, regulatory or project-based measures that contribute to the stabilization or reduction of greenhouse gas concentrations in the atmosphere. Renewable energy programs, energy efficiency frameworks and substitution of fossil fuels are examples of climate change mitigation measures. Iso known as “avoiding the unmanageable”.

Adaptation: actions that respond to actual or projected climate impacts and which reduce the effects of climate change on natural or human systems (e.g. increasing drainage capacity to accommodate changing precipitation patterns). Also known as “managing the unavoidable”.

The principal long range tool for addressing climate change within the municipal sphere of influence is the creation of complete, compact communities that support the objectives of energy efficient buildings, sustainable energy systems and alternative transportation modes. Higher development densities, with a mix of land uses, are essential to achieving these objectives.



D4 POLICIES: Land Use

- D4.11 Ensure all new Neighbourhood Concept Plans (NCP) use information from appropriate agencies, including BC Hydro and Fortis BC, to determine neighbourhood-level projections and assessments of future energy use. Identify opportunities for energy efficiency improvements, energy conservation and use of renewable energy technologies.
- D4.12 Use development and design review applications, as well as Development Permit Guidelines, to implement energy policies supportive of community-based energy systems.
- D4.13 Develop policy that supports the evaluation of a development based on its projected levels of energy consumption and greenhouse gas emissions (GHG), including options for alternative energy supply such as District Energy and on-site generation of renewable energy.

D4 POLICIES: Transportation

- D4.14 Reduce GHG emissions within Surrey by focusing on the following:
- encouraging land use patterns that reduce vehicle use
 - advocating for improved vehicle efficiencies including enhanced tailpipe and vehicle emission standards
 - supporting the expanded use of alternative fuel vehicles
 - developing programs that work to eliminate unnecessary idling
 - actively promoting transportation alternatives through a range of Transportation Demand Management measures including parking management
 - continuing to work with TransLink to improve transit service.



Surrey's Electric Vehicle Fleet located at the New City

D4 POLICIES: Energy Supply

BL 18423

- D4.15 Promote the use of low-carbon, renewable energy sources to reduce reliance on fossil fuels and enhance local energy security through community energy solutions.
- D4.16 Explore innovative ways to produce, supply and store energy at the building, neighbourhood and community levels.
- D4.17 Support the use of District Energy as a method of promoting energy security while providing the flexibility to integrate a range of renewable energy sources over time.
- D4.18 Focus the initial development of Surrey's District Energy system within City Centre, developing future systems in Town Centres and along high-density corridors.
- D4.19 Continue to consult with community stakeholders including the development community, land owners, residents, commercial tenants and public institutions, in the development of any District Energy system in Surrey.
- D4.20 Within designated energy service areas, develop financial and policy tools to enhance the financial viability of District Energy implementation.

D4 POLICIES: Climate Adaptation

- D4.21 Implement Surrey's *Climate Adaptation Strategy* (as amended) to aid the City in anticipating and minimizing the impacts of short-and long-term climate change on infrastructure, development, human health, water supply, energy security, drainage and flooding, agriculture and natural systems.
- D4.22 Develop guidelines that specify how building design and material specifications can be used to adapt to the impacts of climate change.
- D4.23 Incorporate climate change adaptation into the City's risk management framework to ensure integration and implementation of Surrey's *Climate Adaptation Strategy* (as amended) across City departments.



Living wall installed on Surrey's Semiahmoo



THEME E: ECONOMY



THEME E: Introduction

The City of Surrey is committed to developing a complete community that balances a high-quality living environment with a diverse, vibrant and sustainable local economy. A strong local economy generates a wide range of stable jobs for Surrey residents, supports a robust and stable tax base to fund essential community services and provides wide access to consumer goods, services and business investment opportunities.

Surrey enjoys a number of advantages that will enhance its economic position within the region, including excellent transportation links within and beyond the Region, the emergence of City Centre as the primary metropolitan core for the South of Fraser area, a large skilled labour force, fertile and abundant agricultural lands, two growing post-secondary institutions and significant reserves of underdeveloped industrial land.

The objectives in Theme E aim to enhance sustainable economic growth in Surrey and take advantage of emerging business opportunities in order to support a broad-based, vibrant, high-quality economy.

THEME E: Objectives

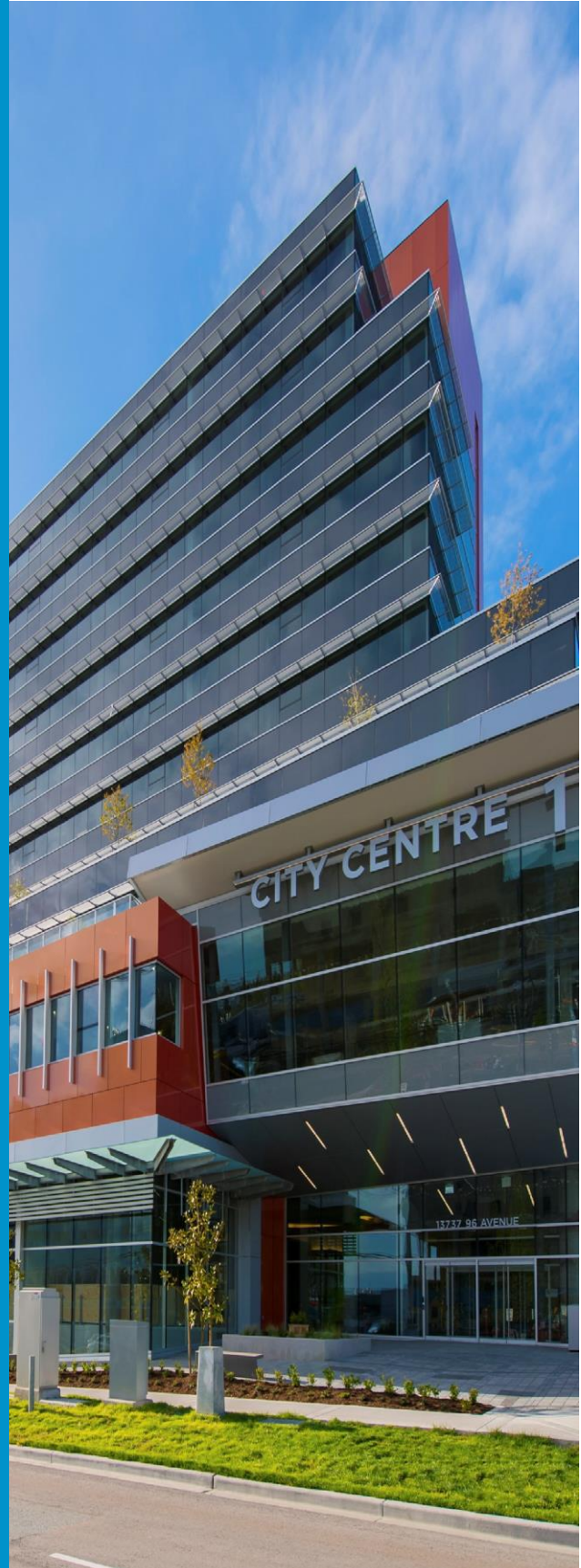
1. Ensure a sufficient supply and efficient use of employment lands.
2. Encourage high-quality business innovation and diversified employment and investment opportunities.
3. Support and enhance local agriculture and its necessary infrastructure.
4. Identify and protect strategic aggregate resources in appropriate locations.

THEME E: ECONOMY

Support a Diversified, Vibrant,
High-Quality Economy

Sections

- E1 Employment Lands
- E2 Employment, Investment and Innovation
- E3 Agriculture
- E4 Aggregates



E1 Employment Lands

Ensure sufficient supply and efficient use of employment lands

Surrey has extensive reserves of industrial-designated land. Protecting this land and the employment uses it supports is one of the most important actions the City can take to ensure a healthy economy into the future.

Due to a limited industrial land base within the region, Surrey faces an ever increasing demand for its employment lands. As Surrey continues to accommodate a significant portion of the employment uses in the Lower Mainland, it is imperative that the City uses its employment reserves strategically and efficiently. In addition to newly developed employment areas, redevelopment and intensification of existing industrial sites will become necessary in order to meet the growing demand for new employment lands in the future. In order to meet Objective E1, the City will:

E1 POLICIES: General

- E1.1 Ensure a sufficient supply of employment lands in Surrey, including designated industrial lands, to meet the current and future needs of the local and regional economy.
- E1.2 Monitor the utilization and availability of industrial lands in conjunction with Metro Vancouver.
- E1.3 Identify lands that may be suitable for future employment uses and that are located in areas that provide suitable access to major transportation corridors. Consider employment land needs in the preparation of all secondary plans.
- E1.4 Locate, site and design employment areas to be accessible, compatible, have access to high quality, frequent public transportation, and be well integrated into surrounding communities and neighbourhoods. Design employment districts to provide services to workers employed in those areas.



Campbell Heights employment lands area.

E1 POLICIES: Intensification

- E1.5 Encourage the full utilization and efficient use of industrial and other employment lands in order to maximize jobs and economic activity per hectare.
- E1.6 Support the infill and redevelopment of under utilized properties within Commercial, Mixed- Employment and Industrial land designations and remove regulatory or other barriers to achieving the full development capacity in these locations.
- E1.7 Develop flexible zoning regulations and by-laws to support more intensive uses of existing employment lands.
- E1.8 Ensure a positive interface between employment lands and accompanying industrial activities and surrounding uses.
- E1.9 Consider the importance of agricultural land and environmentally significant features adjacent to industrial areas, in the planning of employment areas to ensure an appropriate interface, sufficient environmental protection and suitable tree protection.
- E1.10 Ensure sufficient, convenient and appropriate access to employment lands including supply and goods movement routes and access to employment opportunities for Surrey’s workforce.

E1 POLICIES: Community Integration



Bridgeview Industrial Park and Pattullo Bridge, Surrey.

E1 POLICIES: Industrial Uses

- E1.11 Protect industrially-designated land specifically for industrial purposes, particularly industrial land accessible by water and railways.
- E1.12 Support proposals that use industrially-designated land for commercial purposes only where:
 - Commercial and retail uses are accessory uses supporting the principal industrial use AND
 - Retail uses are limited to serving the needs of industrial employees AND
 - Commercial and retail uses do not pose short or long term conflicts or threaten the conversion of industrial lands to commercial uses.
- E1.13 Prohibit the conversion of industrial, business park or mixed-employment lands (see Figure 41) to residential or other non-employment uses.
- E1.14 Locate office, business park and retail employment uses in a manner that respects the hierarchy of urban centres. Direct major commercial and institutional employment generators to City Centre and Town Centres.

Port Lands

- E1.15 Support the economic role of Surrey’s port lands as significant contributors to the economy of Surrey and the Lower Mainland as a whole.
- E1.16 Work with Port Metro Vancouver in the implementation of its Strategic Plan *Port 2050* (as amended) as it relates to the continued functioning and expansion of the Fraser Surrey Docks in ways that protect the environment and are sensitive to adjacent neighbourhoods.
- E1.17 Work with Port Metro Vancouver in determining appropriate access (either via water, rail or truck) to the Fraser Surrey Docks in order to support economic growth and development in Surrey.
- E1.18 Advocate for the replacement of the George Massey Tunnel as a means to allow for an increase in shipping access to Surrey’s port lands.

E1 POLICIES:

Surrey Fraser Docks.



Figure 41: Employment Land Types

Land use designations are used to establish a reference for where types of uses are acceptable and expected to take place. Land use designations are set regionally through the Metro Vancouver *Regional Growth Strategy* (RGS) and locally within this Official Community Plan in the Land Uses and Density Section.

The *Regional Growth Strategy* (as amended) adopted by Metro Vancouver in 2011 establishes two types of employment-based land uses: industrial and mixed employment. These uses are summarized here to establish a clear distinction between each:

Industrial Uses

Are primarily intended for heavy and light industrial activities, and appropriate accessory uses. Commercial uses are limited in this category to those that support industrial activities only. Residential uses are not intended to take place within the industrial land use designation.

Mixed Employment Uses

Mixed Employment areas are intended for industrial, commercial and other employment-related uses in order to meet regional economic needs. Uses within this designation are intended to support industrial activities as well as complement Urban Centres and Frequent Transit Development Areas. Those Mixed Employment areas located close to or adjacent to Urban Centres or Frequent Transit Development Areas are expected to be able to accommodate more office uses, retail and other, more intensive forms of commercial.

Mixed Employment areas located away from Urban Centres or Frequent Transit Development Areas should still accommodate industrial and commercial uses but office and retail uses shall be at a much smaller, lower density scale. Residential uses are not intended to take place within Mixed Employment designated areas.



Pacific Highway Commercial Truck Border Crossing

E1 POLICIES: Office and Commercial Uses

E1.19 Direct office uses, institutions and major retail centres to locations accessible by public transit such as City Centre, Town Centres and Frequent Transit Corridors. Unless otherwise indicated in approved Secondary Plans, Surrey’s Urban land use designations do not support major trip-generating commercial or industrial uses outside of Urban Centres or FTDA’s.

Mixed Employment Uses

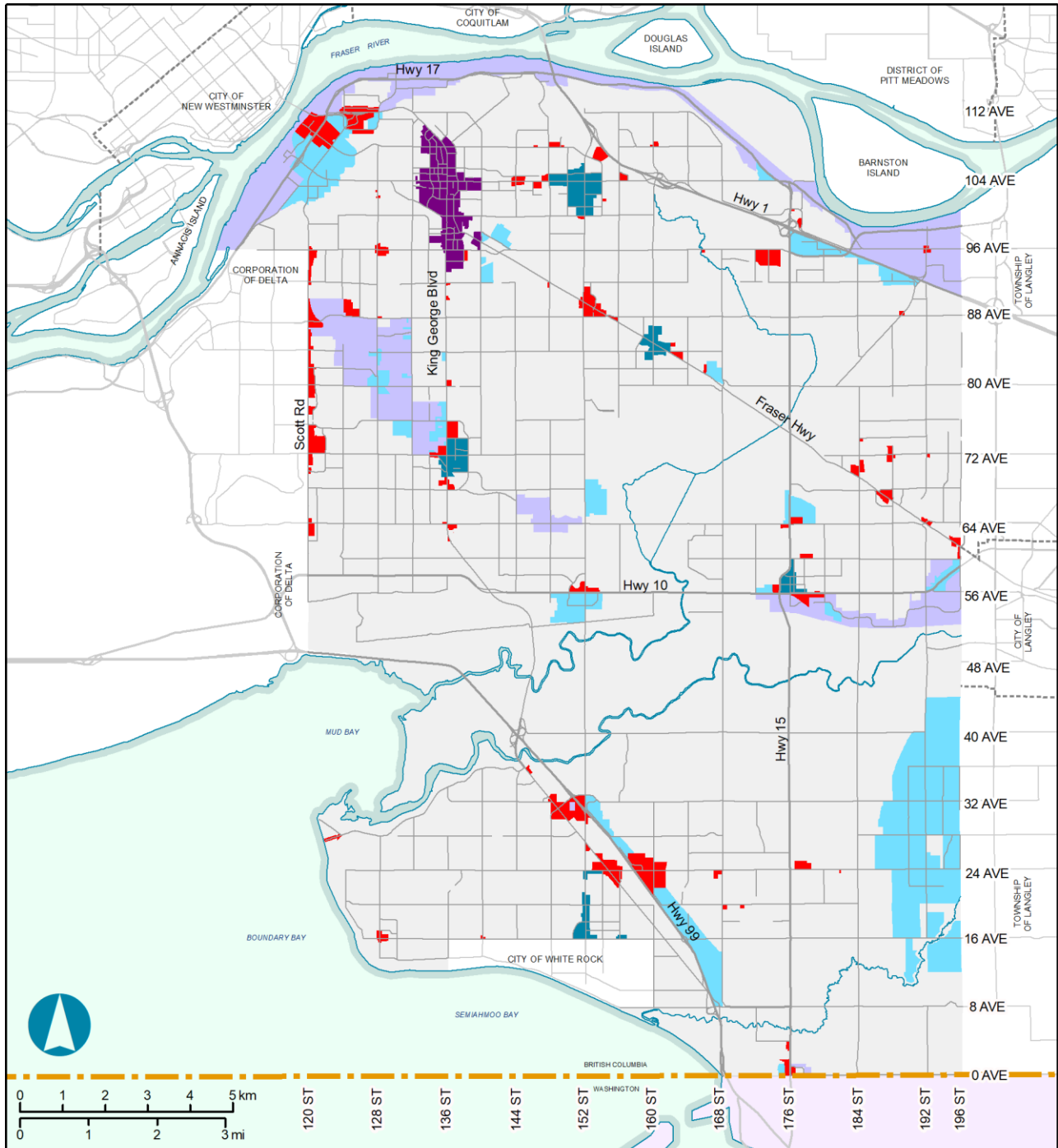
- E1.20 Encourage the continued expansion of high-tech, research and development and light manufacturing to locate within Mixed Employment areas that are accessible to major transportation corridors and/or frequent transit services.
- E1.21 Support the vitality of Mixed Employment areas and uses without compromising the role and composition of existing Town Centres.
- E1.22 Restrict large-format retail uses from Mixed Employment areas, except as specifically permitted in approved Secondary Plans.
- E1.23 Ensure that any development that takes place in the Mixed Employment areas of South Westminster, Newton, East Panorama Ridge and Highway 99 (see Figure 42) does not negatively impact adjacent residential areas.

E1 POLICIES:



Pacific Link Business Park, Surrey

Figure 42: Major Employment Areas



- BL 18423
- BL 18787
- BL 19364
- BL 19296
- BL 19996
- BL 20008
- BL 20118
- BL 20393
- BL 20935
- BL 20705
- BL 21086
- BL 20404
- BL 20833

LEGEND

 Downtown	 Mixed Employment
 Town Centre	 Industrial
 Commercial	

E1 POLICIES: Regional Infrastructure and Transportation

E1.24 Advocate and partner with Federal and Provincial governments to ensure Surrey’s regional transportation routes are maintained. This is especially important where there are key economic gate- ways and linkages to the rest of the region and beyond.

E1.25 Support initiatives to increase travel capacity across the Fraser River, including capacity for pedestrians, cyclists, buses and high-occupancy vehicles, to support regional economic development links that directly benefit Surrey’s economy.

E1.26 Seek opportunities to work with senior levels of government and private partners to deliver major infrastructure improvements, such as transportation, communication and utility infrastructure, that is pivotal to Surrey’s economic growth.

E1.27 Support goods movement routes and rail and truck access between major employment areas in Surrey and between key transportation corridors and gateways, including the Surrey Fraser Docks, the Vancouver International Airport and the US/ Canada border crossings, by planning for appropriate land uses and densities along major corridors.

E1.28 Collaborate with senior levels of government, Metro Vancouver and TransLink to advocate for the appropriate and timely delivery of major transportation infrastructure to support economic growth and development in Surrey, including the:

- Addition of Light Rail Transit in Surrey
- Replacement of the Pattullo Bridge
- Replacement of the George Massey Tunnel
- Upgrading of Highway 99
- Completing all grade separation crossings across Surrey’s rail corridors
- Upgrading or adding Port-related infrastructure
- Upgrading US/Canada border infrastructure
- Upgrading Metro Vancouver’s trunk water and sewer infrastructure
- Adding new interchanges along Highway #1 and #99 to provide access to Surrey.

E1.29 Collaborate with senior levels of government and with TransLink to reduce congestion along strategic goods movement corridors in order to support economic development.

E1.30 Complete and implement a comprehensive and sustainable strategy for truck parking, identifying permitted locations and required site enhancements.

Peace Arch/Douglas Canada/US Border Crossing



E1 POLICIES: Sustainable Development

- E1.31 Support and encourage the high quality, environmentally responsible, sustainable development of employment lands, including such considerations as alternative rainwater management, green buildings and renewable and District Energy supply systems.
- E1.32 Review parking standards to identify ways in which parking can be provided more efficiently and sustainably within Industrial and Mixed Employment development areas.
- E1.33 Support Eco-Districts and other green-friendly, neighbourhood-level approaches to development in industrial and major employment areas. Consider supporting the use of waste recycling, waste heat and water reuse, shared parking facilities and other sustainable development practices.



Green development standards at the South Surrey Rec Centre.

E2 Employment, Investment and Innovation

Ensure high-quality, business innovation and diversified employment and investment opportunities

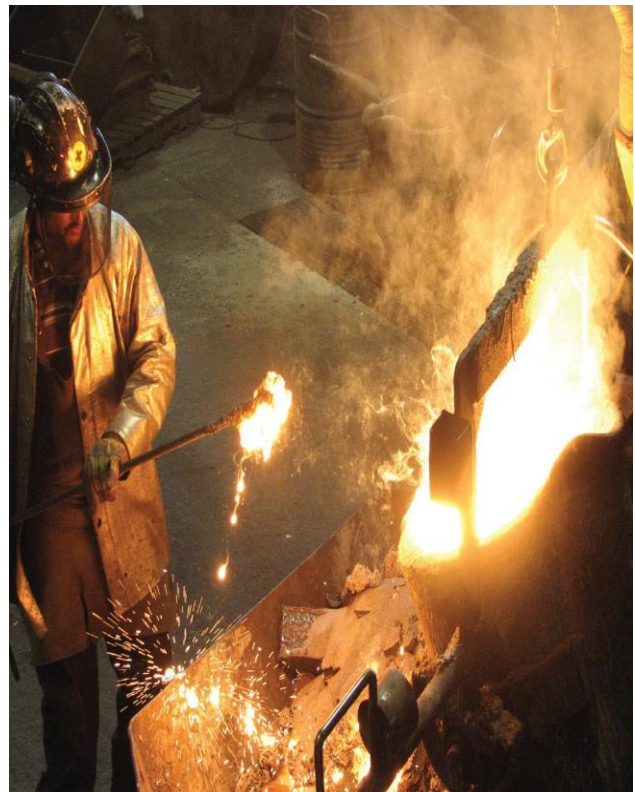
A high quality, sustainable economy delivers broad access to rewarding jobs in sectors of the economy that continue to grow and thrive into the future. Economic development policies that encourage investment and support innovation are key to attracting and retaining the businesses that drive a sustainable, thriving local economy.

The City of Surrey's economic development strategy positions the City as an attractive location for investment, emphasizes post-secondary education and skills training and focuses on growing sectors such as clean energy, intensive value-added agriculture and advanced manufacturing technologies.

In order to meet Objective E2, the City will:

E2 POLICIES: General

- E2.1 Develop, implement and maintain a coordinated economic development strategy in the context of federal, provincial and regional economic development programs, plans and objectives.



Heavy industry uses in Surrey

E2 POLICIES: Economic Development

- E2.2 Promote economic development to achieve and maintain a balanced ratio of one job for every member of Surrey’s labour force.
- E2.3 Take advantage of and market Surrey’s economic strengths and competitive advantages, including:
 - Convenient access to rapid transit, major transportation routes and gateways including the US/Canada border, highways and railways into the United States and the BC Interior, the Surrey Fraser Docks and the Vancouver International Airport
 - Proximity to post-secondary and trades and technology educational institutions
 - Available, relatively affordable and available land for business and industrial expansion
 - Available, relatively affordable housing for employees
 - Available young and educated labour force.
- E2.4 Encourage private sector investment in strategic areas of the city through the use of such tools as development incentives and economic investment zones.
- E2.5 Take advantage of strategic investment opportunities by using City-owned lands to leverage private-sector investment.

E2 POLICIES: Partnerships

- E2.6 Maintain cooperative partnerships with community organizations and business groups active within Surrey, including the Surrey Board of Trade, Business Improvement Associations, the Urban Development Institute and other industry associations, in order to foster investment and business development.
- E2.7 Maintain strategic partnerships with energy stakeholders (e.g. BC Hydro, SFU and Fortis BC) to facilitate the growth of clean energy businesses in Surrey.
- E2.8 Encourage partnerships between the Surrey City Development Corporation and the private sector, where appropriate.



Partnerships with the Surrey City Development Corporation and local companies create opportunities for local business expansion; Photo courtesy of Central City Brew Pub

E2 POLICIES: Education and Skills Training

- E2.9 Continue partnering with and supporting the efforts of local post-secondary education institutions to expand and ensure that an adequate number of seats and relevant programs are available to meet the needs of students and the local economy.
- E2.10 Support the expansion of career and skills training programs and facilities to develop the economy and provide skilled workers, especially in high technology and other growing areas of the economy.

E2 POLICIES: Infrastructure

- E2.11 Ensure that appropriate infrastructure, including transit, transportation, water and sewer, and electrical and communications networks are planned and constructed in appropriate locations to best support the growth of Surrey's economy.
- E2.12 Advocate to senior governments for increased investment in major regional transportation infrastructure including rapid transit to support economic growth and economic expansion within Surrey.
- E2.13 Ensure that services and amenities for employees, such as child care, recreation and personal services, are located near employment centres to enhance worker's quality of life and productivity.
- E2.14 Consider opportunities to expand live-work zoning and home-based business policies as a way of retaining and attracting business-related employment.



SkyTrain to Surrey

E2 POLICIES: Economic Growth Sectors

- E2.15 Achieve economic growth and job creation by supporting and developing the renewable energy and clean technology sectors within Surrey.
- E2.16 Consider establishing business incubator areas to support commercialization in potential high-growth sectors such as health sciences, clean energy technology and value-added manufacturing.
- E2.17 Encourage the growth of high value-added industries such as engineering and design services, advanced manufacturing, food processing, specialized construction materials and manufacturing and product innovations.
- E2.18 Encourage the growth of the tourism, ecotourism and hospitality sector by building on Surrey's natural and cultural features to foster a positive image of the city as a place to visit and by expanding and improving Surrey's hotel, hospitality and convention facilities.
- E2.19 Encourage the growth of the "green building" technology sector in Surrey by encouraging energy-efficient construction and the use of green building certification programs for private and public development.



Campbell Heights Employment Lands

E3 Agriculture

Support and enhance local agriculture and its necessary infrastructure

Agriculture plays an important role within Surrey. The mild climate, productive soils, extensive farmland, supportive infrastructure and the ability to produce a wide range of crops all contribute to the success of Surrey's farms.

Surrey values protecting its agricultural land and promoting sustainable food systems. For agriculture to survive and thrive in a metropolitan setting, implementing the comprehensive *Surrey Agriculture Protection and Enhancement Strategy* is important in order to appropriately address agricultural-development issues, resolve rural-urban conflicts and ensure long term agriculture viability of agriculture in Surrey.

The Agriculture and Food Security Advisory Committee (AFSAC), comprised of representatives from the local agricultural sector, City Council, the Ministry of Agriculture, Surrey's Environmental Advisory Committee and planning and engineering staff, advises Council on agricultural policy and identifies solutions to specific issues ensuring a thriving, sustainable agri-food sector in Surrey.

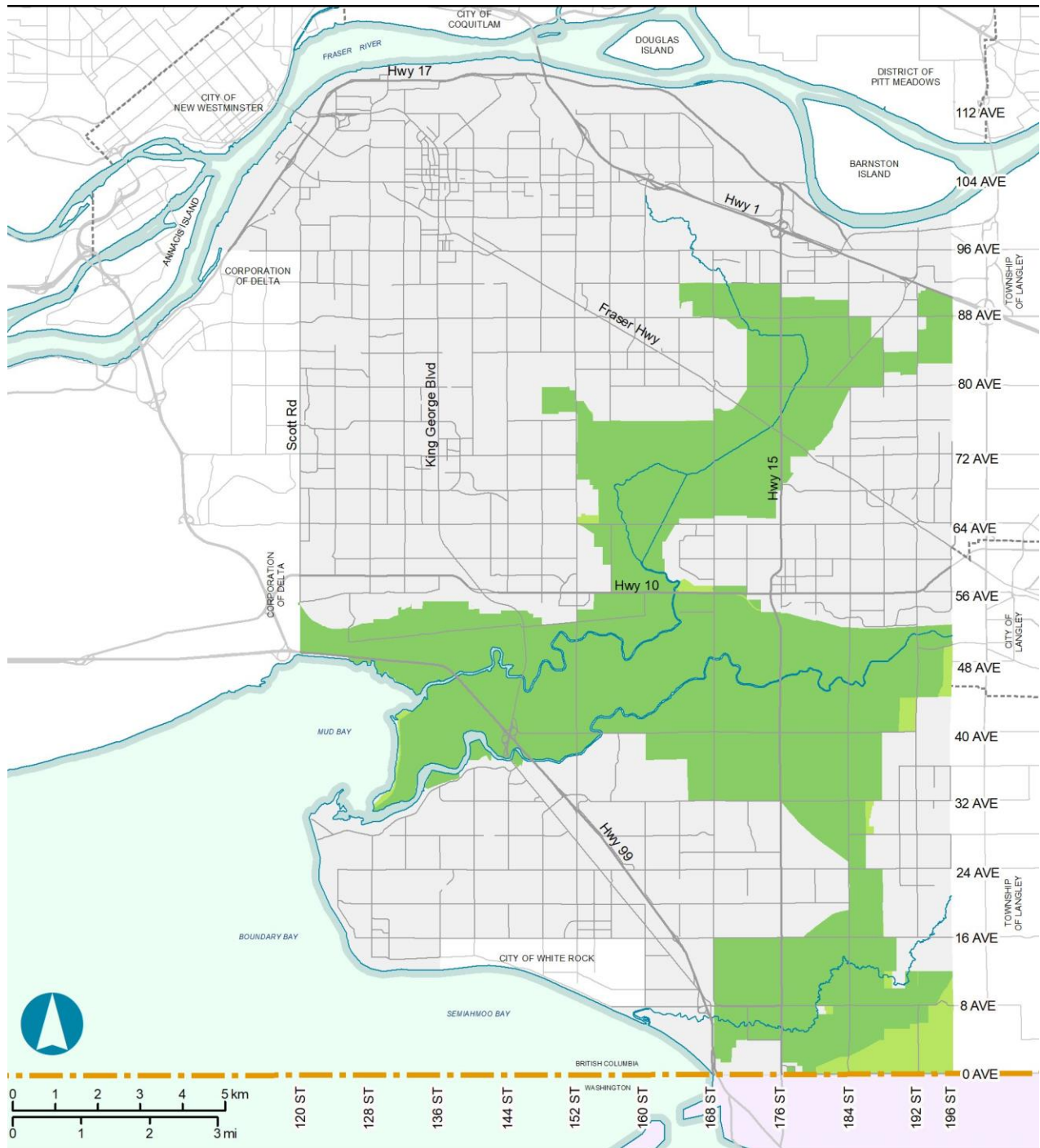
In order to meet Objective E3, the City will:

E3 POLICIES:

General

- E3.1 Maintain the integrity of the Agricultural Land Reserve (ALR) and its existing boundaries (see Figure 43).
- E3.2 Ensure all land uses within the Agricultural Land Reserve conform to the policies and regulations of the *Agricultural Land Commission Act* and the *Farm Practices Protection Act* (as amended).
- E3.3 Protect and support the continued designation and use of agricultural land for agricultural purposes regardless of soil types and capabilities. Encourage locating non-soil based agricultural structures on less productive soils, where feasible, in order to fully utilize prime soil resources.
- E3.4 Encourage the continued use of farmland located outside of the ALR for agriculture purposes, subject to the development of an approved secondary plan.
- E3.5 Avoid the fragmentation of ALR lands. Limit the subdivision of land within the ALR to greater than 4 ha (10 ac). Limit the subdivision of designated agriculture land outside the ALR to greater than 2 ha (5 ac). Encourage the amalgamation of lots in agricultural areas.
- E3.6 Require 2 ha of land, within Surrey, of equivalent or better soil capacity, to be included into the ALR for each 1 ha of land excluded from the ALR with the submission of an Agricultural Impact Assessment detailing how this conversion provides a net benefit to agriculture in Surrey.
- E3.7 Ensure that the potential fragmentation of agricultural lands by linear development such as new road construction, hydro corridors or pipeline or road upgrades, does not negatively impact the viability of farm operations or access to farm parcels.

Figure 43: Agricultural Lands



BL 19364
BL 20393

LEGEND

- Agricultural designated land in the Agricultural Land Reserve
- Agricultural designated land outside the Agricultural Land Reserve

E3 POLICIES: Innovation

- E3.8 Maximize the productive utilization of agricultural land through such means as:
- Leasing City-owned ALR land to farmers
 - Amending by-laws, policies and other regulations, as appropriate, to support agricultural production and innovation.
- E3.9 Support Surrey-based research and development of innovative production technologies and practices, such as 'vertical farming', roof-top farming and advanced irrigation and fertilization systems.
- E3.10 Encourage the diversification of agricultural operations and the production of new crops and commodities, particularly high-value crops and those serving new markets with growth potential.
- E3.11 Partner with the Province, producer groups and local businesses to develop new local and global markets and marketing strategies for local commodities.
- E3.12 Encourage innovation in the management and recycling of agriculture and food waste, including waste-to-energy projects in the agri-food sector such as anaerobic digestion and other means of converting waste into a resource.

E3 POLICIES: Reduce Barriers

- E3.13 Support programs that help new farmers overcome financial and other barriers to accessing land (includes but is not limited to incubator farms).



Innovative growing methods can be used to supplement Agricultural Land production.

E3 POLICIES: Education and Research

- E3.14 Work with local secondary and postsecondary institutions to support training and education programs in agriculture and food systems.
- E3.15 Work with post-secondary institutions to facilitate agricultural research partnerships and technology transfer to local farmers.
- E3.16 Support public awareness programs and events to highlight the importance of agriculture to Surrey's economy. Showcase local producers and increase public awareness of farming practices.

E3 POLICIES: Infrastructure

- E3.17 Ensure Surrey has a safe and convenient transportation network that supports agriculture, including access between farms and markets, farm vehicle travel routes and direct access between fields and farm operations.
- E3.18 Support land owners and senior levels of government to improve access to an adequate and safe water supply for agricultural operations such as livestock watering, crop irrigation, produce washing and food processing uses.
- E3.19 Support farming activities by managing storm-water runoff from upland development on agricultural properties in accordance with the Agriculture and Rural Development Subsidiary Agreement (ARDSA) criteria as established by the *Strategic Plan for Lowlands Flood Control*, as amended.
- E3.20 Support the development of ancillary agricultural services necessary for the viability of agricultural operations, in appropriate locations.
- E3.21 Partner with senior levels of government, commodity groups and industry associations to maintain and grow the food packaging and processing industries in Surrey.



Serpentine River used for agricultural irrigation in Surrey.

E3 POLICIES: Urban-Ag Interface

- E3.22 Protect farming and agri-food operations from adjacent urban impacts such as traffic, flooding, nuisance complaints, trespassing and noxious substances.
- E3.23 Protect and enhance the interface between urban areas and the Agricultural Land Reserve (ALR) by using fencing, landscaping buffers and building separations between urban and agricultural lands, in accordance with DP4 of the Implementation Section of this OCP.
- E3.24 Support density increases in areas adjacent to the ALR, as part of a comprehensively planned neighbourhood, where the required agricultural buffers are widened proportionally to the increase in density (see DP4 of the Implementation Section of this OCP), where cluster housing is used and where natural features such as watercourses, steep slopes, tree preservation, valuable ecosystems and sensitive soils are accommodated.
- E3.25 Support the growth and development of farm markets and stands selling local products directly to consumers.
- E3.26 Carefully plan public areas and recreational trails in areas adjacent to farmland. Provide sufficient separation and physical barriers such as fencing, plantings and topographic features, to reduce conflicts between recreational uses and farming.
- E3.27 Use Agricultural Disclosure Agreements, at various stages of development, adjacent to agricultural areas to increase awareness of owners about the presence and implications of living near agricultural activity.
- E3.28 Raise public awareness and promote and celebrate local agriculture in Surrey through initiatives such as farm tours, media materials, community events, and by encouraging partnerships with producer associations and grocery stores to promote local farm products.
- E3.29 Explore agri-tourism opportunities for producers to diversify their farm operations, create an identifiable brand, and sell more products and services directly to consumers.

Agricultural-Urban landscape buffers are used to reduce conflicts between these two uses.



E3 POLICIES: Environment

- E3.30 Encourage sound environmental agricultural and farm practices by working with farmers to participate in the Provincial Environmental Farm Plan program in order to promote clean and healthy air, water and soils.
- E3.31 Encourage the management and recycling of manure and agricultural wastes in a sustainable manner.
- E3.32 Support programs and opportunities that help support pollinators and their valuable role in the continued viability of farming operations.
- E3.33 Encourage Best Management Practices for riparian areas and streams in agricultural areas in order to support fish populations in accordance with the requirements of the Department of Fisheries and Oceans (DFO) and the Provincial *Water Act*, as amended.
- E3.34 Explore opportunities related to recovering heat, generating clean energy and other resources and reducing greenhouse gas (GHG) emissions by recycling food wastes and other organic materials through pilot projects and partnerships.



Farm areas are clearly defined in Surrey's Agricultural Land

E4 Aggregates

Identify and protect aggregate resources in appropriate locations

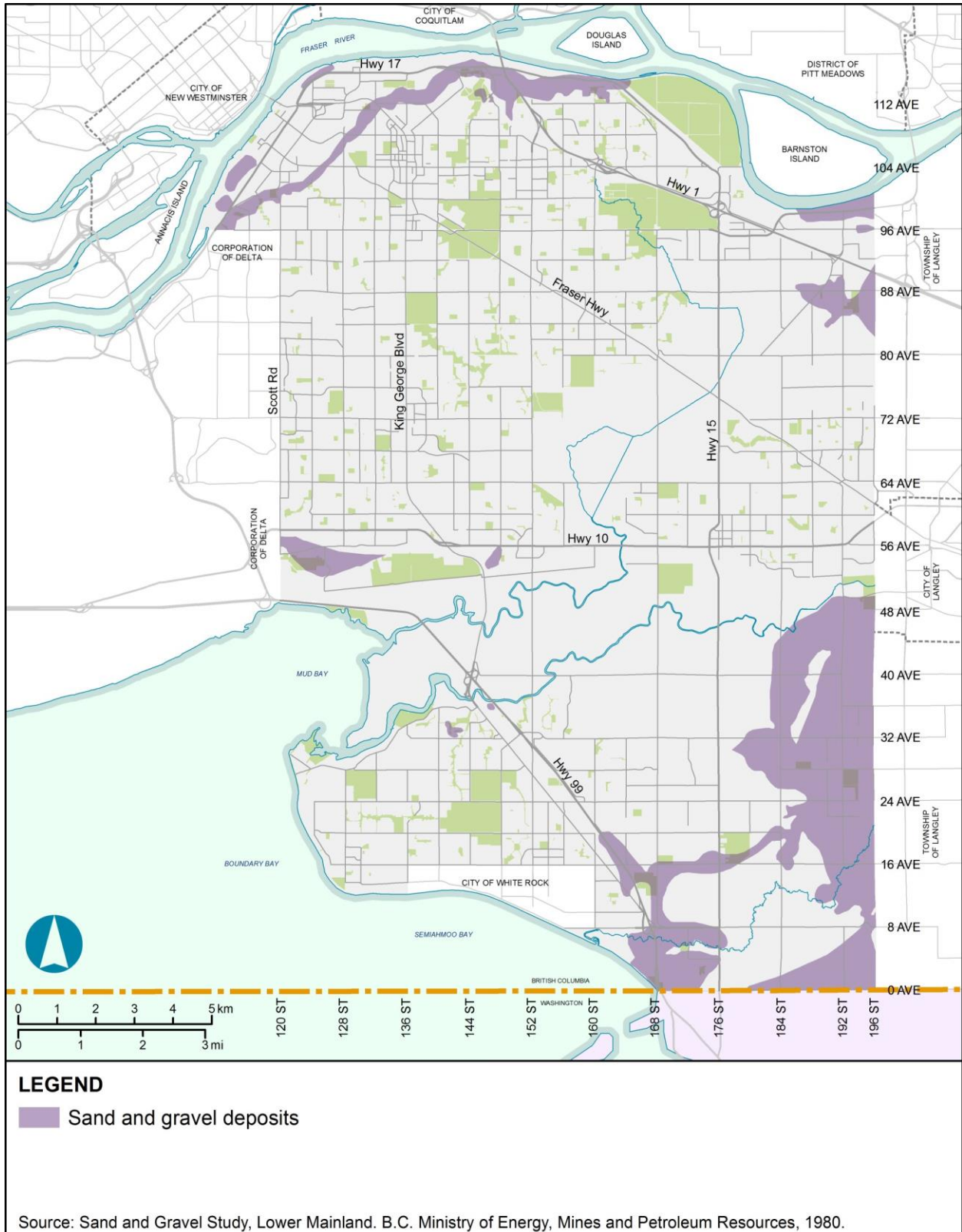
The *Local Government Act* requires municipalities to designate areas of existing and future sand and gravel extraction. Areas that contain appropriate aggregate deposits that are available for extraction are limited in Surrey with only the Southwest corner of the city providing opportunities for sand and gravel extraction. The City works to ensure that the environment is protected, that impacts on the community are minimized during extraction and that the land is restored to productive uses when extraction is complete.

In order to meet the Objective E4, the City will:

E4 POLICIES: General

- E4.1 Implement the *Soil Conservation and Protection By-law*, as amended, to limit and set conditions for sand and gravel extraction and deposition.
- E4.2 Limit sand and gravel extraction to the excavation, screening and storage of materials. No additional processing of any kind is permitted, including the crushing or manufacturing of asphalt, concrete or other materials. Such processing activities are permitted only in industrial areas or where permitted by Temporary Use Permits, under conditions imposed by City Council.
- E4.3 Sand and gravel extraction practices must consider the environmental impacts on adjacent properties and areas downstream. Extraction cannot threaten or destroy human health, watercourses, significant habitats, riparian areas, municipal utilities or the soil stability of adjacent or down- stream properties.
- E4.4 Ensure there is adequate and timely rehabilitation and/or reforestation of the sand and gravel extraction areas (shown in Figure 44) as set out in an extraction permit issued by the City of Surrey.
- E4.5 Strongly encourage the replacement of fertile top soil that is removed from development sites to agricultural areas in need of rehabilitation.

Figure 44: Aggregate Deposits





THEME F: SOCIETY AND CULTURE



THEME F: Introduction

As Surrey continues to grow and evolve, it faces new challenges and opportunities for building a safe, healthy, inclusive and culturally vibrant city. Population growth places pressure on Surrey's existing social services, protection services, affordable housing stock, recreation facilities, childcare and education centres, libraries and cultural facilities. Growth, however, can also contribute to sustainability and increased liveability by enhancing the City's social and cultural infrastructure and by increasing the range and quality of services available to Surrey residents.

Surrey's demographics are changing and social-cultural services need to anticipate and adapt to these changes. Surrey's population is becoming more culturally and linguistically diverse, with a growing proportion of seniors and a continuous growth in the numbers of children, youth and young families. In response, this Official Community Plan places increased emphasis on partnerships to address affordable housing, skills training and education, healthy communities and childcare issues. This Official Community Plan also recognizes the increasing importance of cultural services (arts and heritage) in attracting investment and economic growth.

The nine objectives within Theme F aim to leverage growth in order to improve the quality of life for all of Surrey's current and future residents and visitors and to build a safe, healthy and inclusive city.

THEME F: Objectives

1. Provide inclusive citizen engagement opportunities.
2. Support a comprehensive and innovative life-long learning system in Surrey.
3. Strive to provide appropriate and affordable housing for everyone.
4. Provide healthy and accessible active living opportunities.
5. Improve access to healthy, local food.
6. Ensure accessible and inclusive civic facilities, programs and community services.
7. Support a vibrant arts sector.
8. Protect and celebrate community heritage.
9. Ensure a safe community through effective crime prevention and emergency response.

THEME F: SOCIETY AND CULTURE

Build a Safe, Healthy and Inclusive
Community

Sections

F1 Citizen Engagement

F2 Learning

F3 Affordable Housing

F4 Healthy Living

F5 Food Security

F6 Accessibility and Inclusivity

F7 Culture: Arts

F8 Culture: Heritage

F9 Community Safety



F1 Citizen Engagement

Provide inclusive citizen engagement opportunities

An important aspect of healthy community development is involving citizens in the planning process at all levels. Sufficient opportunities for public participation in the decision-making process ensures that decisions about land use, development and the allocation of public infrastructure and services reflect and address public concerns and achieve balance between private and public interests.

The City uses a variety of formats such as open houses, public hearings, Council Committees, public meetings, youth engagement activities and focus group consultations to increase public participation in civic life. In recent times, the City has increased opportunities for engagement by using new technologies such as video link, web-based surveys and digital engagement platforms to increase public access to the City’s planning and decision-making processes. The City is committed to continuing to improve citizen engagement and widen participation in the development of the community.

In order to meet Objective F1, the City will:

F1 POLICIES: General

- F1.1 Continue to encourage citizen involvement by actively coordinating the planning process with relevant community organizations, agencies, neighbourhood associations and volunteer groups.
- F1.2 Seek new ways to engage the full spectrum of the community in planning processes, including seniors and new immigrants, and those residents who live in a lower socio-economic or educational level, by using new media, on-line surveys, targeted group discussions and other means necessary to allow for full engagement and participation.
- F1.3 Support civic engagement of neighbourhood groups and social service agencies to better engage with the general public and increase public participation and citizen engagement in the growth and evolution of Surrey.



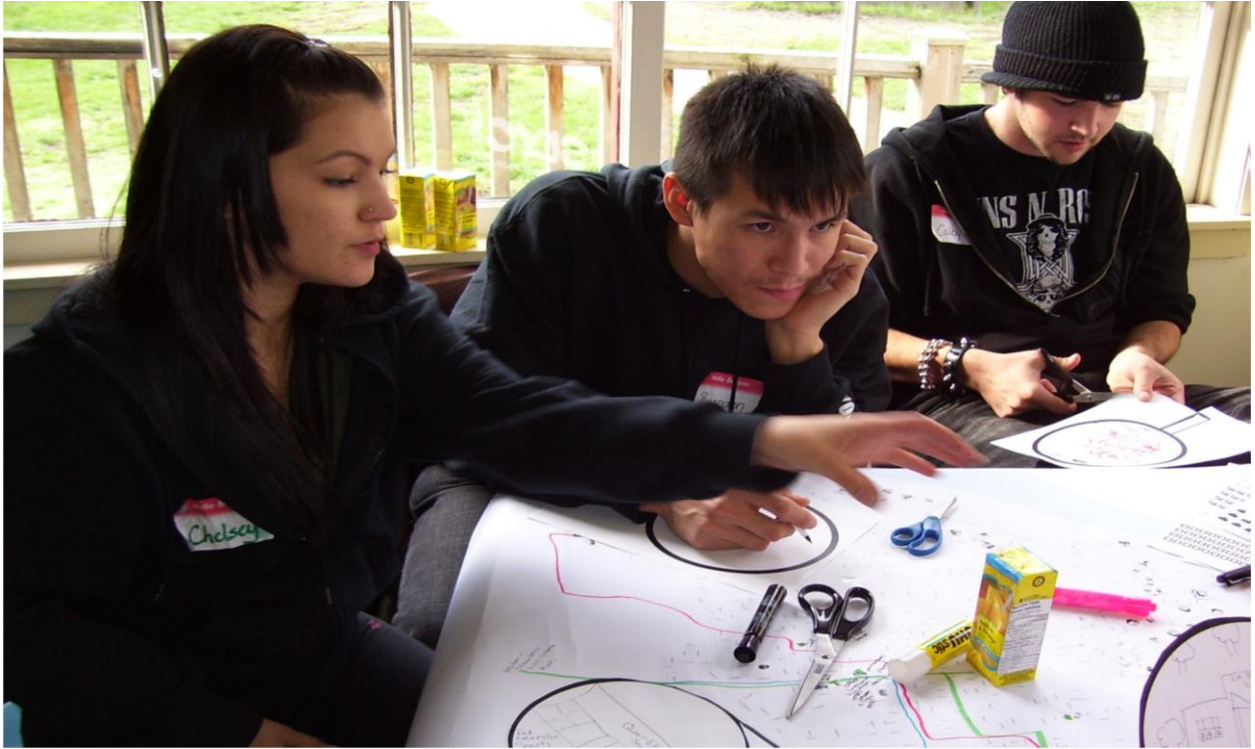
The City of Surrey website is available in separate interfaces dedicated to library programs and for direct child interaction.

F1 POLICIES: Consultation

- F1.4 Encourage neighbourhood and community associations to participate with Council to define land uses, densities, forms of development, and the scale of housing appropriate to their neighbourhood.
- F1.5 Provide ample and diverse opportunities for residents' participation in local planning and government. Additional care will be made to reach groups (e.g. less mobile, physically disabled, English as a Second Language) who have particular difficulty participating in standard methods of public consultation (e.g. open houses).
- F1.6 Take into account the diversity of languages spoken in Surrey and develop a communication strategy that will aid in sharing information with stakeholders and the general public.
- F1.7 Provide a range of volunteer opportunities for residents of all ages and abilities. Ensure these opportunities are publicized effectively, including the opportunity for citizens to become involved in City Committees, Boards and Commissions.

F1 POLICIES: Children and Youth

- F1.8 Develop consultation processes specifically created for and targeted to children and youth. Use Terms of References for planning projects where there is an identifiable impact on children and youth.
- F1.9 Support and enhance youth engagement processes where youth have a meaningful and on-going role in providing input into City services, programs and policy development.



F2

Learning

Support a comprehensive and innovative lifelong learning systems in Surrey

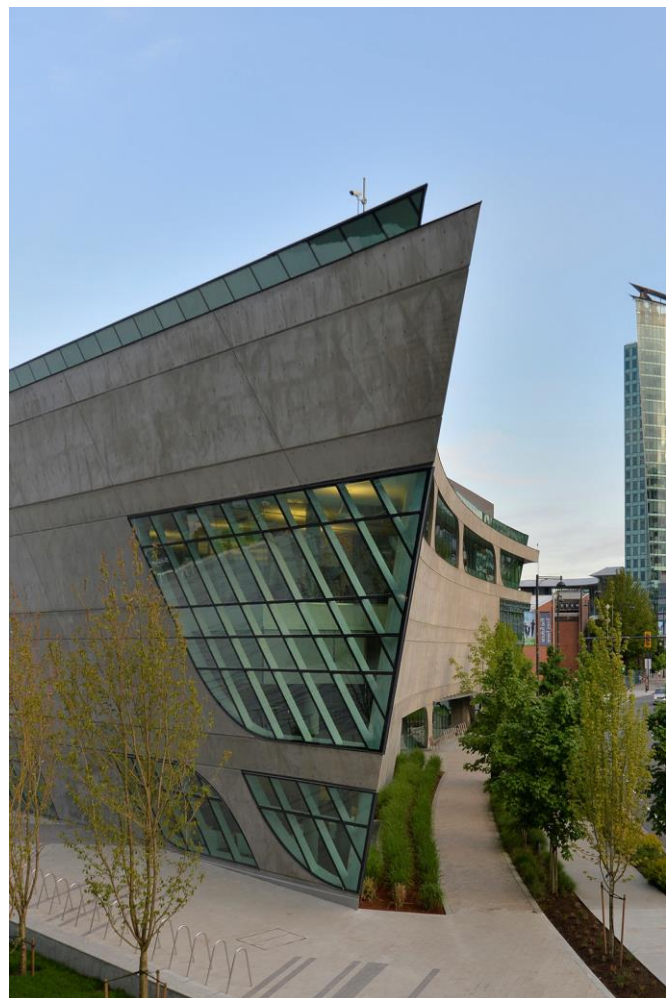
Surrey recognizes that the economic and social well-being of its residents depends on access to quality education and life-long learning in order to thrive in a rapidly changing world. Education enriches cultures, creates mutual understanding and enhances life and social skills. Education is a vital investment in the sustainable development of a community and its human potential. Knowledge-based economies require innovative education systems aimed at fostering accessibility, diversity and sustainability.

Surrey residents have access to a multitude of educational opportunities (see Figure 45) from a School District with specialized elementary and secondary programs, two post-secondary institutions, extensive private trade and career facilities, museums and archives and nine public library branches, Surrey is well positioned to offer extensive educational options.

In order to meet Objective F2, the City will:

F2 POLICIES: General

- F2.1 Support the *Community Literacy Plan* (as amended) to ensure that community-based literacy providers have the resources they need to meet the needs of Surrey's diverse community.
- F2.2 Support the delivery of adequate, licensed, affordable and high-quality child care spaces in locations conveniently accessible to families.
- F2.3 Create early learning opportunities for Surrey residents by implementing the recommendations found in Surrey's *Learning for Life Strategy* (as amended).



City Centre Library

F2 POLICIES: Structured Learning

- F2.4 Work with School District No. 36 to address the following:
- Providing early years children with access to learning opportunities appropriate to neighbourhood-specific needs
 - Providing appropriate equitable funding, per student, for schools and programs in all new neighbourhoods
 - Planning of new school locations to reduce overcrowding
 - Developing a comprehensive and holistic approach to using school facilities for community development and recreational initiatives
 - Jointly planning for coordinated and shared facilities between the School District and the City of Surrey, where appropriate.
- F2.5 Support quality, accessible post secondary education as a direct measure of a healthy community. Support the expansion of post secondary educational programs in Surrey.
- F2.6 Continue to partner with and support secondary educational institutions, such as Simon Fraser University and Kwantlen Polytechnic University, to:
- Expand and support relevant programs that continue to meet the evolving needs of students and the community
 - Lobby for additional funding to expand programming and student spaces
 - Provide opportunities to physically expand school facilities
 - Expand research programs.

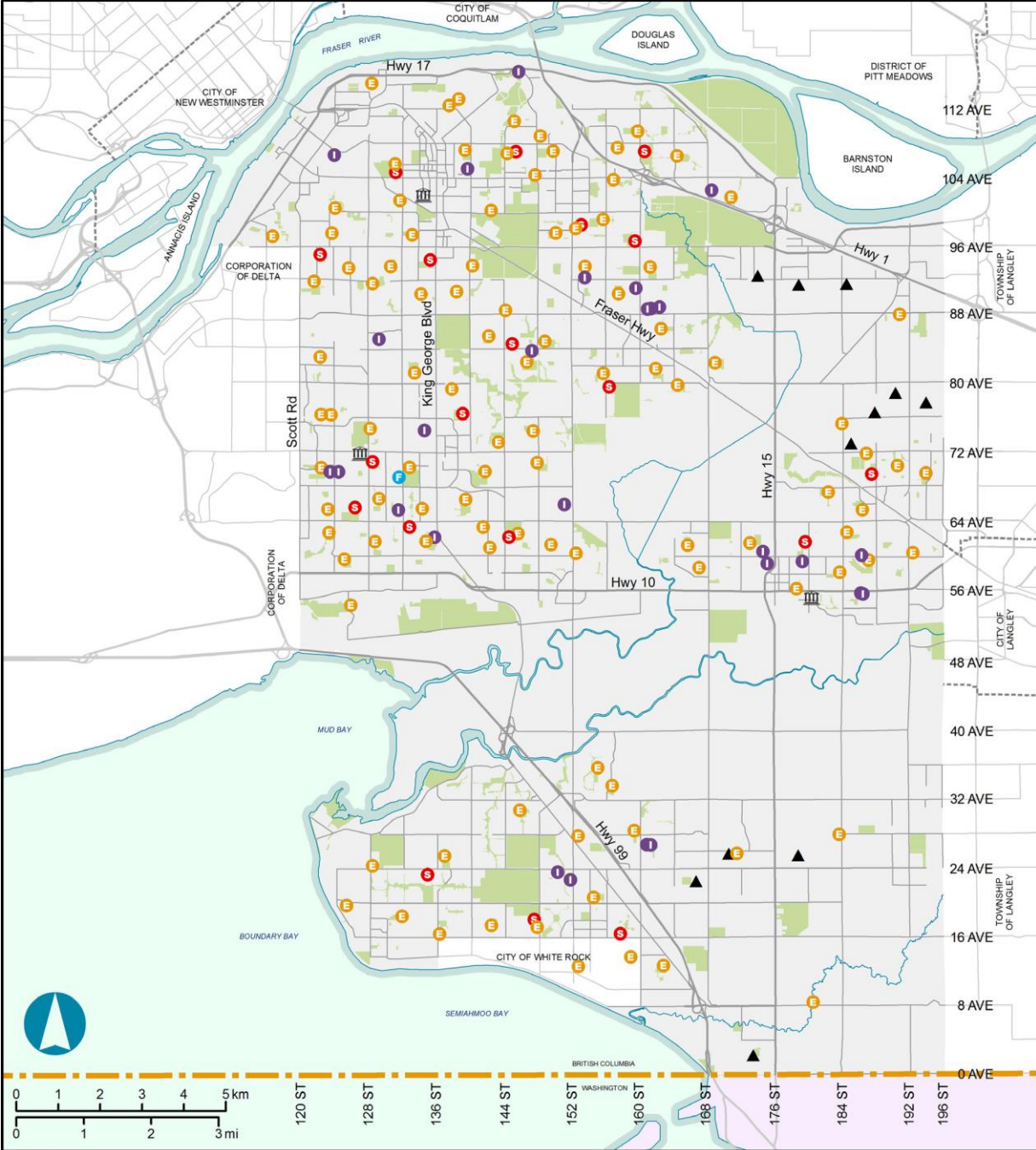
F2 POLICIES: Life-Long Learning

- F2.7 Continue to advocate for improved transit services within Surrey so that residents are better able to access learning services in the City.
- F2.8 Continue to enhance library services and programs by determining their role in the continued development of community health and then by assessing the needs for additional libraries or expanded library services throughout the city in order to meet those identified needs.
- F2.9 Support museums, art galleries and archives as community education centres.
- F2.10 Provide literacy funding on a community level to evenly distribute access to literacy services more thoroughly throughout the city.
- F2.11 Provide more opportunities for residents of all ages and ethnicities to engage in learning activities that also promote social interaction and reduce isolation.



School District No. 36 head office in Surrey's City

Figure 45: Schools



LEGEND

- Public elementary schools
- Public secondary schools
- Francophone schools
- Independent schools
- Future schools (general locations)
- Post-secondary institutions

Note: This map is for general reference only.

F3

Affordable Housing

Strive to provide appropriate and affordable housing for everyone

Adequate, affordable and appropriate housing is fundamental to building strong, healthy communities, allowing people to live with honor and dignity regardless of income level or abilities. A complete community enhances livability in neighbourhoods by providing a variety of housing choices in terms of cost and type.

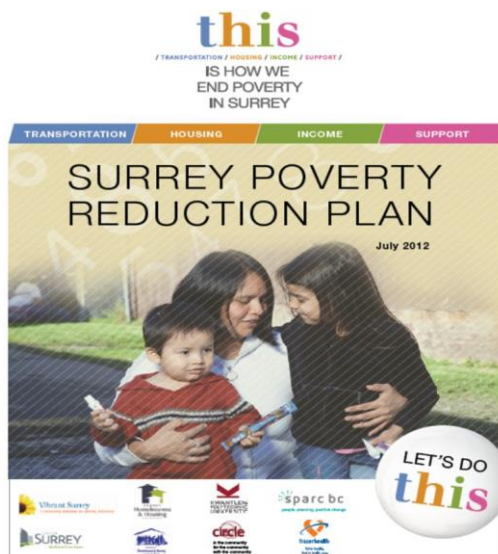
Appropriate housing meets the diverse needs of households in terms of size, location and design. Affordable housing is generally considered to be housing (either for rent or for ownership) which meets the needs of individuals or families in the low-to-moderate income range and which costs less than 30% of gross household income. The affordable housing spectrum includes non-market, supportive housing as well as affordable market housing provided by the private sector. The City has a role to play in ensuring there's an adequate supply of housing to meet demographic needs, and to make that happen, to work in partnership with the development and home-building industry, senior levels of government and community-based housing societies.

In order to meet Objective F3, the City will:

F3 POLICIES:

General

- F3.1 Complete, maintain and regularly update an Affordable Housing Action Plan to guide City policy and actions supporting the provision of affordable housing.
- F3.2 Work with government agencies, community groups and private developers to accommodate a full range of affordable and accessible housing that meets the needs of Surrey's diverse population.
- F3.3 Support affordable housing projects that:
 - strengthen and enhance the well-being of the community
 - address the need for clean, safe, suitable and affordable housing
 - demonstrate partnerships with other service providers and businesses.
- F3.4 Affordable housing projects should generally be located:
 - on transit routes
 - in close proximity to schools
 - in close proximity to shopping and medical services
 - where support services, such as counseling and training, are accessible to residents
 - where there is no overconcentration in



Surrey's Poverty Reduction Plan (2012) was developed as part of Surrey's Poverty Reduction Project which focuses on transportation, housing, income and support.

F3 POLICIES: Non-Market Rental

BL 18787

- F3.5 Actively advocate to senior levels of government for housing services to address homelessness, including homeless outreach, addictions and mental health services, food services, employment services and rent supplements.

F3.6 Coordinate with BC Housing and Provincial and Federal agencies to establish a full range of non-market housing to meet the current and future needs of residents.

F3.7 Support the development and on-going operation of a range of non-market housing, including emergency shelters, transitional housing, supportive housing, low-income housing and co-op housing.
- F3.8 Support non-market housing for a diversity of low-income households, including: mental health and addictions, youth, seniors, people with disabilities, women, low-income singles, families with children, urban Aboriginals and refugees.

F3.9 Support the development of non-market housing through actions such as expediting applications, decreasing parking requirements, waiving development fees or charges, contributing land, etc.

F3.10 Continue to support the Surrey Homelessness and Housing Society and Fund.

F3.11 Continue to support community and/or regionally-focused forums for addressing housing and homelessness issues in Surrey.

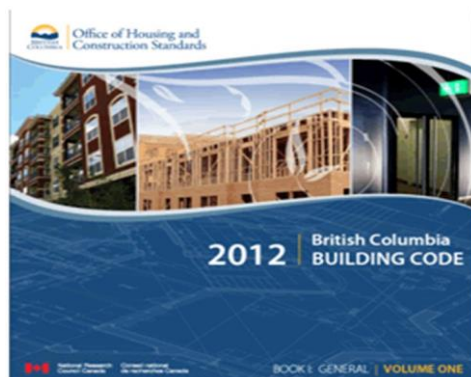


Timber Grove Apartments, 2010 Olympic Park Legacy

F3 POLICIES: Affordable Market Rental

- F3.12 Ensure an adequate supply of rental housing units in all areas of the city.
- F3.13 Restrict the demolition or strata conversion of existing rental units unless city-wide vacancy rates are higher than four percent (4%).
- F3.14 Encourage and support the development of new purpose-built rental apartments through such actions as expedited development applications and reduced parking requirements, where appropriate.
- F3.15 Advocate to senior governments for policy and taxation changes that will encourage the construction of purpose-built rental apartments.
- F3.16 Coordinate and partner with landlords and provincial agencies to ensure that adequate and appropriate rental housing is available throughout the city. Ensure adequate standards of building maintenance and management are followed.
- F3.17 Recognize secondary suites as a significant form of rental housing by supporting secondary suites in Single Family Zones where the size, depth and width of a lot supports the additional parking requirements of a secondary suite and by encouraging homeowners to legalize their existing suites to sensitively integrate into the neighbourhood.
- F3.18 Encourage innovative zoning, design and development solutions for affordable housing units, particularly for infill development, that is sensitive to neighbourhood context (see Figure 19).
- F3.19 Where a density bonus is granted in exchange for community benefits or amenities, the first priority community benefit shall be to provide affordable housing (or cash-in-lieu), in conjunction with market housing residential units.
- F3.20 Where redevelopment of a Manufactured Home Park is proposed, ensure that adequate consultation and compensation for relocated residents is implemented as part of the development application.
- F3.21 Encourage the development of accessible and adaptable units using the *BC Building Code* Adaptable Housing Standard (as amended) for all new apartment housing, to allow for "Aging in Place" and to create greater housing options for persons with disabilities.
- F3.22 Promote affordable family housing in City Centre, Town Centres and other locations accessible to frequent transit service by encouraging a mix of unit types including two-bedroom and three-bedroom apartments in new developments.

BL 20231



Building Codes play a key role in ensuring appropriate, safe and adequate housing is provided for all of Surrey's

F4

Healthy Living

Provide healthy and accessible active living opportunities

There is a growing recognition that encouraging active living is fundamental to achieving better individual and community health. This has implications for the quality of life of Surrey’s residents and for the fiscal sustainability of Canadian society in an era of rising healthcare costs. Local government plays a supporting role to senior government in healthy community initiatives, along with a range of community partners, including the local health authority, school district, business groups, neighbourhood associations and the non-profit sector.

The City of Surrey has a role in facilitating active living by providing high quality parks, facilities and programs and interconnected greenways, cycle routes, local trails and walkways. The City may also plan and regulate land uses and urban design in ways that encourage residents to walk and cycle for both recreational purposes as well as for a wide range of everyday activities.

In order to meet Objective F4, the City will:

F4 POLICIES: General

- F4.1 Provide a high level of recreational and leisure time facilities throughout Surrey, including those for youth and specific targeted age groups.
- F4.2 Incorporate active living and public health considerations into City planning for land use, transportation, public works and facilities and into the review of development applications.
- F4.3 Actively encourage all members of the community to walk or cycle for leisure, recreation and transportation, whenever and wherever they can as part of their daily lives, to encourage healthy living and reduce air pollution.
- F4.4 Promote opportunities for regular, direct contact with natural spaces and areas throughout Surrey to realize the health and therapeutic benefits of outdoor recreation and nature appreciation.



Darts Hill Park, Surrey.

F4 POLICIES: Programs and Facilities

- F4.5 Collaborate with partners, including Fraser Health Authority, School District No. 36, Provincial agencies and the non-profit sector to coordinate the delivery of programs, special events and facilities that encourage healthy lifestyles.
- F4.6 Consult with residents, sports groups, associations and other stakeholders to plan the delivery of parks and playgrounds, recreational facilities and programs that meet the needs of each local community.
- F4.7 Provide and maintain high-quality parks, community recreation centers, facilities and programs in each community in order to provide equitable and convenient access for all Surrey residents (as shown on Figure 31 and 46).
- F4.8 Coordinate the planning of indoor recreation facilities and programs with parks and outdoor recreation opportunities, wherever possible.

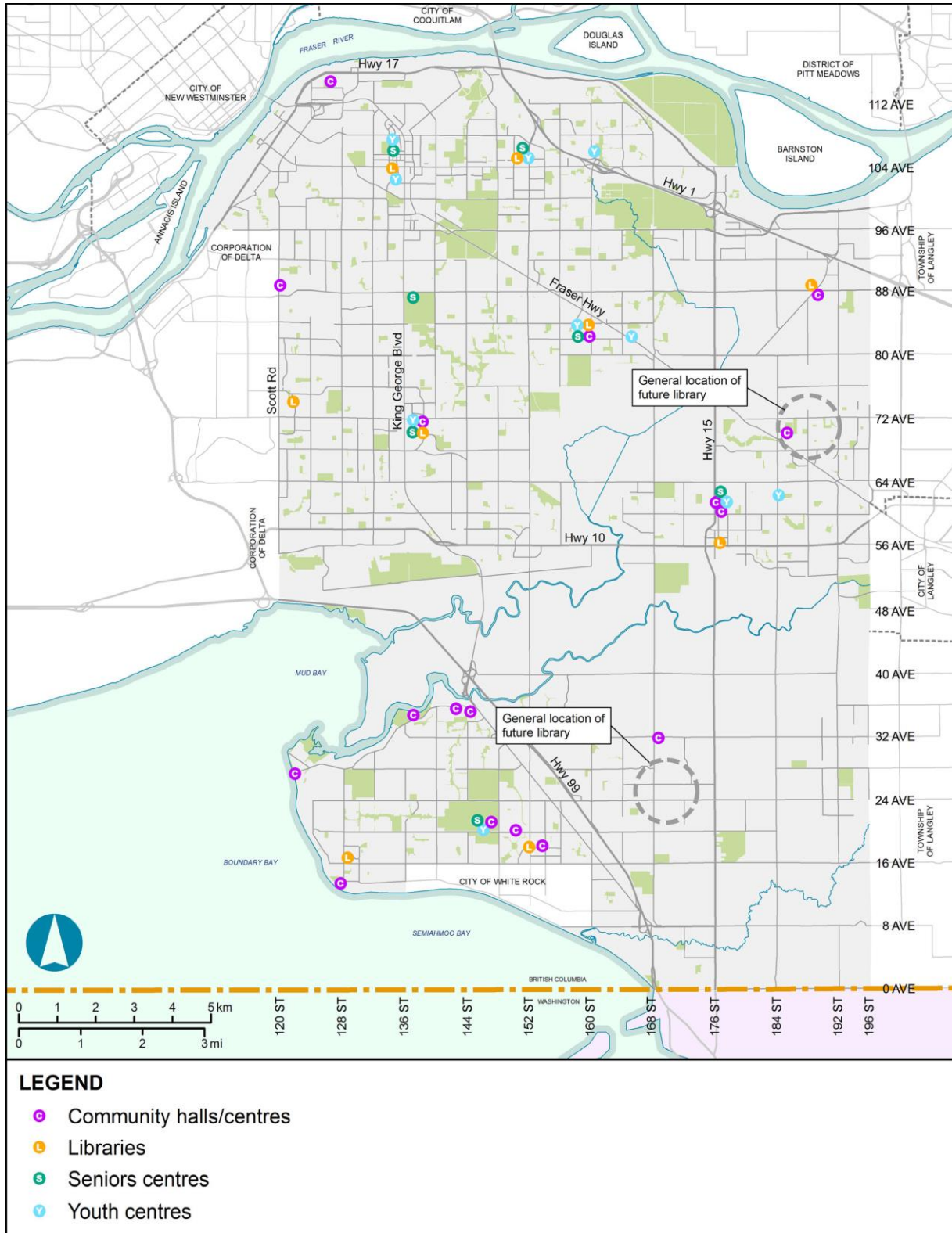
F4 POLICIES: Access to Recreation

- F4.9 Identify and develop strategies to remove physical, cultural, geographic and socio-economic barriers to accessing City recreation and leisure programs and services, including those specific to children, youth, seniors, low income residents and persons with special needs.
- F4.10 Pursue partnerships with public agencies, non-profit organizations and private businesses to deliver recreational and leisure programs in an efficient, cost-effective and accessible manner.
- F4.11 Work with private sector and non-profit groups to secure recreation facilities and programs within residential developments and places of work as a supplement to public recreation facilities.



Structured child play areas are located throughout Surrey in all neighbourhoods.

Figure 46: Community Facilities



F5

Food Security

Improve access to healthy, local food

Access to healthy, affordable food is a significant aspect of quality of life, especially among low income and other vulnerable groups. When healthy food is also produced, processed and distributed locally, there is potential for local economic activity, local employment and reduced greenhouse gases associated with food transport. The concept of food security has several facets, including individual and household health and affordability, along with community resilience to food availability and cost pressures.

The City of Surrey is able to play a role in facilitating access to healthy and local food by creating supportive land use policies for locating food stores, community gardens, urban agriculture and community kitchens and by supporting non-profit agencies whose focus is on distributing healthy, affordable food.

In order to meet Objective F5, the City will:

F5 POLICIES: General

- F5.1 Improve access to healthy and affordable food, particularly in lower-income neighbourhoods, through the planning and implementation of community gardens, farmer's markets, urban agriculture projects, community kitchens, neighbourhood grocery stores and food assistance programs, as appropriate.
- F5.2 Create a Food Policy Council and Surrey Food Charter to help develop formalized structures for tackling food security issues. Work in conjunction with the Agricultural and Food Security Advisory Committee.
- F5.3 Encourage and support the development of local on-farm markets and urban farmer's markets for local, direct food distribution in appropriate locations.
- F5.4 Examine means of how to integrate locally supported agriculture (e.g. community gardens, community orchards, urban agriculture and small farms) into existing neighbourhoods and new areas of urban development.
- F5.5 Develop an inventory of public lands, including parks, boulevards, Rights-of-Way and rooftops, that could potentially be used for urban agriculture. Develop criteria for selecting optimal locations for new community gardens or other urban agriculture opportunities on public lands.
- F5.6 Partner with local neighbourhood associations, school districts and non-profit groups to encourage, organize, develop and manage community gardens, rooftop farming, fruit trees and other edible landscapes on public and private lands, in suitable urban locations while also using appropriate integrated pest management practices.

F5 POLICIES: General (cont.)

- F5.7 Encourage mixed-use neighbourhood design and redevelopment to include small and mid-size grocery stores (e.g. 300—1,850 sq. m), seasonal farmer’s markets and open space for community gardens.
- F5.8 Support the efforts and programs of Fraser Health and other non-governmental organizations that promote access to healthy nutritious foods.
- F5.9 Assist non-profit agencies and public/private non-profit partnerships engaging in anti-hunger, nutrition and agriculture activities by sharing data for planning and by implementing and evaluating programs.
- F5.10 Work with non-profit organizations such as food banks, food agencies, food gleaners, and food hamper supporters to ensure healthy food options are available to all residents of Surrey regardless of location or income level.

Dunsmuir Farm Park, Community Garden, Crescent Beach



F6

Accessibility and Inclusivity

Ensure accessible and inclusive civic facilities, programs and community services

A healthy and complete community provides opportunities, spaces and community-based programs for the social development, personal growth and leisure of all residents. Providing these facilities in an accessible and inclusive manner, both physically and culturally, and distributing them throughout neighbourhoods, enhances the quality of life in Surrey. A truly accessible community is one where everyone, regardless of their ability, shares equal access to the key elements of life such as work, play, access and participation.

While the City does not have a mandate to play a lead role in the delivery of social services, it can facilitate access to these services through land use planning, urban design, civic facility programming and advocacy. The City participates in a wide range of multi-stakeholder “tables” that plan for the equitable and sustainable delivery of community services and often plays a role in facilitating community-based partnerships.

In order to meet Objective F6, the City will:

F6 POLICIES: General

- F6.1 Commit to the full and meaningful participation of all residents in City services, functions and facilities. Actively work toward eliminating and preventing discrimination based on age, race, gender, national or ethnic origin, colour, language, sexual orientation, culture and religious beliefs.
- F6.2 Work cooperatively with senior levels of government to ensure that Surrey receives an equitable portion of social infrastructure investments, grants and program funding, as the Region’s second major urban centre, including:
- hospitals and health care facilities
 - major convention and sport facilities
 - higher education and training facilities
 - transportation infrastructure
 - cultural amenities
 - government offices
 - community and social services.
- F6.3 Prioritize the equitable allocation and distribution of services, civic facilities, programs and amenities for all Surrey residents.
- F6.4 Address financial, physical and perceptual barriers to accessing services and facilities allowing all members of society to be fully engaged.



Users of Senior's Centre at Chuck Bailey Rec

F6 POLICIES: Universal Access

- F6.5 Continue to upgrade existing public infrastructure, including sidewalks and parks, to current accessibility standards. Design recreational paths and greenways to be universally accessible, where possible.
- F6.6 Use Universal Design principles to incorporate accessibility into all new civic buildings and facilities, including associated outdoor spaces such as walkways, plazas, play spaces, transit stops and parking lots. Use enhanced way-finding measures and auditory traffic signals, where warranted.
- F6.7 Ensure the accessibility of private services and amenities by applying principles of Universal Design at the site and building level.
- F6.8 Encourage new multi-family residential developments to meet or exceed the minimums identified in the *BC Building Code's* Adaptable Housing Standards (as amended).

F6 POLICIES: Accessible Locations

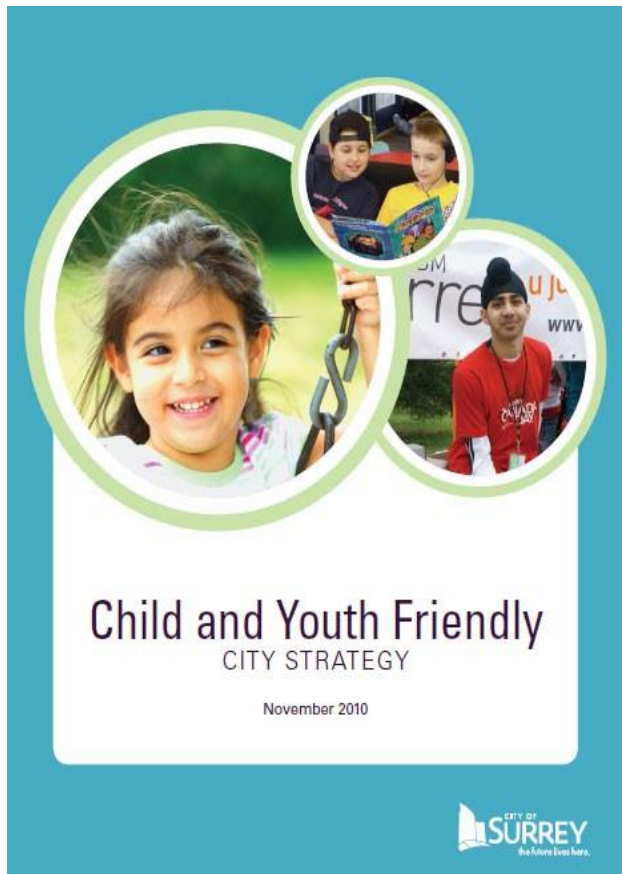
- F6.9 Encourage the location of social services in areas where they are easily accessible to persons needing them.
- F6.10 Encourage the distribution of social services in each of Surrey's Town Centres in order to ensure wide access to services by residents in all parts of the city and to avoid the over-concentration of social service agencies.
- F6.11 Consider locating community services in locations accessible by transit in compact, walkable urban centres in order to improve access for all segments of the population, especially those with limited mobility.
- F6.12 Encourage, where appropriate, the co-location of public facilities, including Federal, Provincial, Municipal and School District facilities, in central locations accessible by safe, accessible sidewalk networks and frequent, accessible transit service.
- F6.13 Encourage and facilitate the provision of community services through cooperation, liaison and, where appropriate, joint provision and participation.
- F6.14 Continue to work with School District No. 36 and other community partners to support and develop programs that provide social, health, recreational, cultural, educational and library services at the neighbourhood level through the use of schools and other community facilities.

Ensuring persons with mobility challenges have easy and convenient access is a factor to leading a healthy and productive life.



F6 POLICIES: Child and Youth

- F6.15 Ensure the City’s child care facility policies and regulations do not create unnecessary operational barriers.
- F6.16 Make provision for childcare in all large community recreation centres.
- F6.17 Encourage large employers and the developers of commercial and multi-family developments to provide an on-site amenity space for child care.
- F6.18 Ensure civic services and programs promote the developmental needs of children and youth of all ages.
- F6.19 Partner with other government and community agencies to ensure that children and youth have access to a range of opportunities, services and programs.
- F6.20 Plan and design parks, playgrounds, civic facilities and public spaces to encourage use by families with children and youth of all ages and abilities. Incorporate interactive and engaging features for children and youth in and around civic facilities. Use street furniture and other design elements in public spaces to facilitate unstructured play.
- F6.21 Develop and implement design guidelines for outdoor play areas in private developments including multiple family projects.
- F6.22 Plan, design and retrofit neighbourhoods so that schools, parks and playgrounds are safely and conveniently accessible by walking and cycling.
- F6.23 Ensure amenities for youth are provided in all neighbourhoods, with larger youth parks located in Town Centres.
- F6.24 Support the placement of community office space in commercial areas for youth to use for recreation purposes. This is particularly important in those areas where recreation centres are not accessible to youth.



F6 POLICIES: Inclusivity

- F6.26 Engage in collaborative community partnerships to address the particular needs of vulnerable groups including, but not limited to, new immigrants, government-assisted refugees, urban Aboriginals and low-income persons.
- F6.27 Assist with the integration of new residents into Canadian culture and society by supporting Surrey library services and programs for this target group.
- F6.28 Bring communities together and promote social cohesion by animating community spaces through arts and culture and by supporting events and festivals.



Surrey Library.

F7

Culture: Arts

Support a vibrant arts sector

Surrey has many citizens who are known for their artistic abilities and expertise and their significant contribution to the community in art and cultural expression.

Surrey is committed to supporting a healthy arts community, illustrated in the Vision and Goals established in the *Surrey Cultural Plan*. This Plan works to mobilize resources and enhance Surrey by supporting the growth of visual and performing arts and by recognizing the role of public art in creating dynamic, sustainable and socially cohesive public spaces. Visual and performing arts contribute to Surrey's character, community life and economic development and serve as catalysts for engagement among the City's diverse cultural communities.

In order to meet Objective F7, the City will:

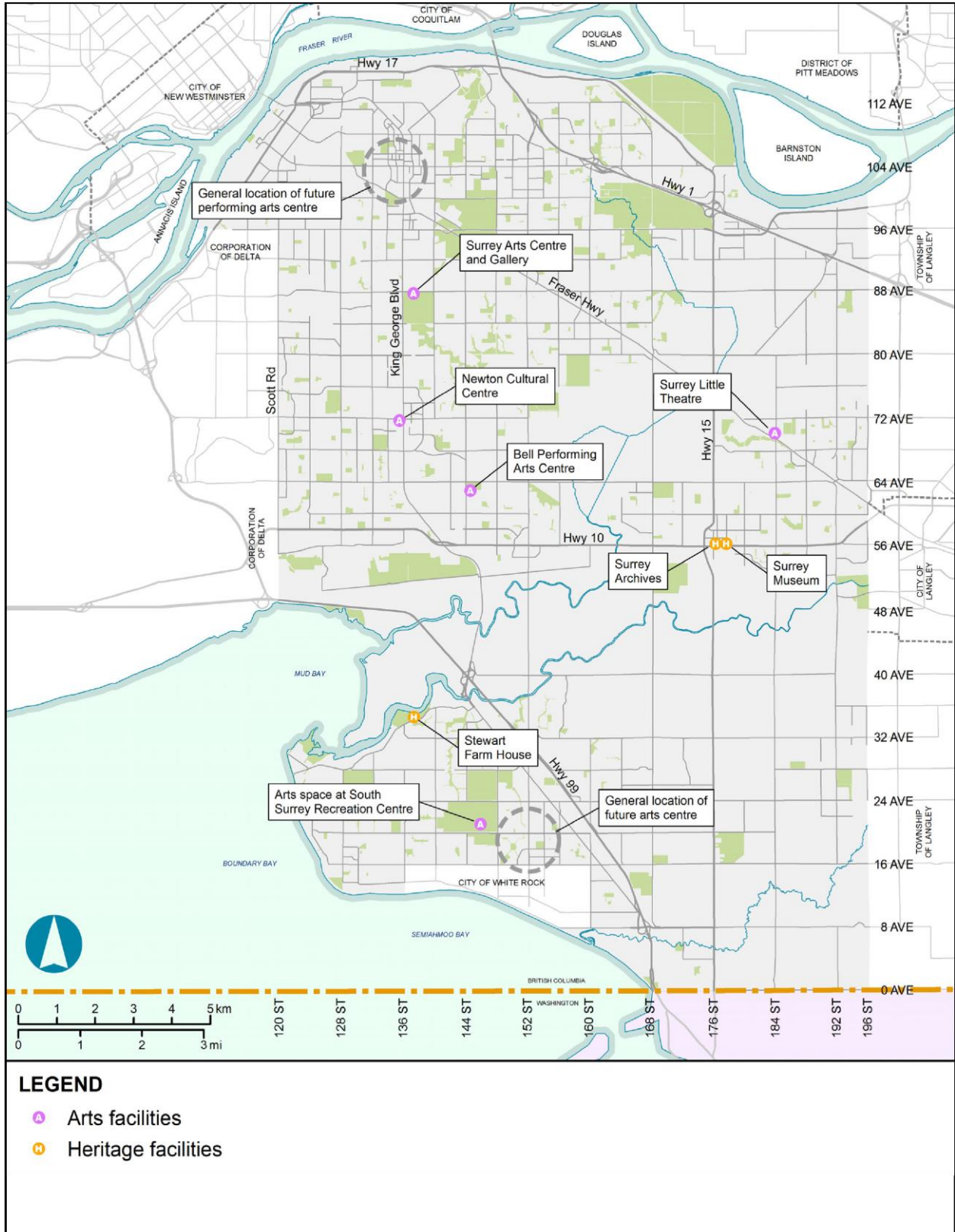
F7 POLICIES: General

- F7.1 Implement Surrey's Public Art Policy, *Public Art Master Plan*, *Cultural Plan*, *Art Gallery Strategic Plan* and *Youth Arts Strategy* (as amended) as effective tools in establishing and maintaining arts and culture programs and services and facilities in Surrey.
- F7.2 Support artistic expression as being critical to developing a unique identity for Surrey by heightening character, countering anonymity, distinguishing one place from another and creating a sense of meaning and belonging for people.
- F7.3 Integrate the arts into the physical development and evolution of Surrey to create a highly-desired urban environment with an enhanced Sense of Place.
- F7.4 Develop a range of venues to support cultural expression, education and the enhancement of community life through art galleries, libraries, museums and facilities for performing arts (see Figure 46 and 47).
- F7.5 Create a premier arts and culture node in City Centre including a Cultural Corridor along King George Boulevard.



Fleetwood Park, Surrey.

Figure 47: Arts and Heritage Facilities



F7 POLICIES: General (cont.)

- F7.6 Use major public art installations, art spaces and cultural facilities within Surrey City Centre to create a distinct Sense of Place and serve as a model for public space design and community development.
- F7.7 Locate arts and culture facilities in each of Surrey's Town Centres to reinforce a distinct identity and to expand the range of cultural activities and opportunities for all Surrey residents.
- F7.8 Promote art and excellence in design throughout the city by encouraging public art placements at locations visible to the public.
- F7.9 Develop a Cultural Marketing Plan and a Cultural Tourism Strategy to increase and enhance the social and economic benefits of cultural tourism in Surrey.
- F7.10 Enhance partnerships with the Surrey Arts Council, educational institutions and the private sector to leverage improved arts and heritage initiatives with Surrey's City Centre and Town Centres.
- F7.11 Review and adjust the Zoning By-law to enable the use or construction of affordable spaces for cultural industry including general studio space and live/studio residential options.
- F7.12 Identify and pursue all available tools to secure new cultural spaces including senior government funding, land donations, contributions through site development, private donations and regulation changes, as appropriate.



Surrey Arts Centre

F8

Culture: Heritage

Protect and celebrate community heritage

Surrey has a significant heritage legacy. Beginning with the early Semiahmoo and Kwantlen First Nations, this land was established with thriving communities for more than 6,000 years with European settlers first arriving in the mid-1800s. Surrey was incorporated as a municipality in 1879 and gained official city status in 1993. Surrey’s heritage includes built, natural, cultural and transportation resources that have been—and continue to be—important to the development of the city as a complete community.

Surrey has established a significant heritage inventory including heritage sites and landmarks as well as museums, archives and exhibition centres. There is also strong interest and dedication within the community to preserve the city’s heritage and to continue to use the tools available to acquire, retain, preserve, interpret, revitalize and celebrate features of Surrey’s past for the benefit of residents and visitors alike.

In order to meet Objective F8, the City will:

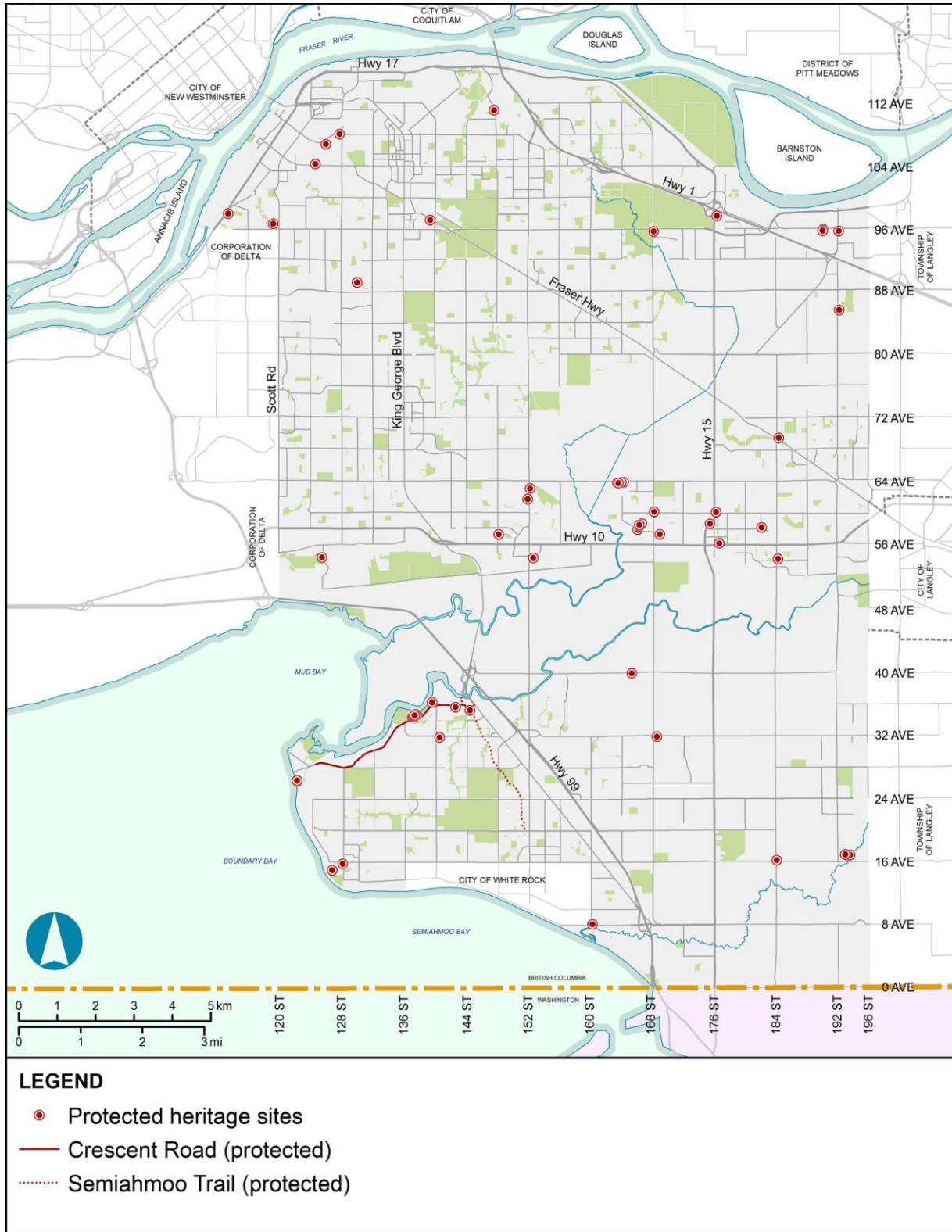
F8 POLICIES: General

- F8.1 Employ tools under the authority granted through the *Local Government Act*, *Community Charter* and *Heritage Conservation Act* (as amended) to identify, retain, preserve, re-use, protect, integrate, maintain and revitalize built, natural and cultural sites, features, and landscapes that have important historical, architectural or cultural significance (see Figure 48).
- F8.2 Refer issues and matters of heritage designation and preservation of buildings, sites and features to the Surrey Heritage Advisory Commission (SHAC) for input and advice.
- F8.3 Work with the Provincial Government and community groups, including the Surrey Heritage Advisory Commission, to implement Surrey’s *Heritage Strategic Review* (as amended).
- F8.4 Collaborate with cultural and heritage organizations in Surrey to further the conservation of heritage resources within the City.
- F8.5 Provide opportunities for citizen engagement in the planning and development of heritage policy, through the Surrey Heritage Advisory Commission, and the maintenance and management of heritage facilities.



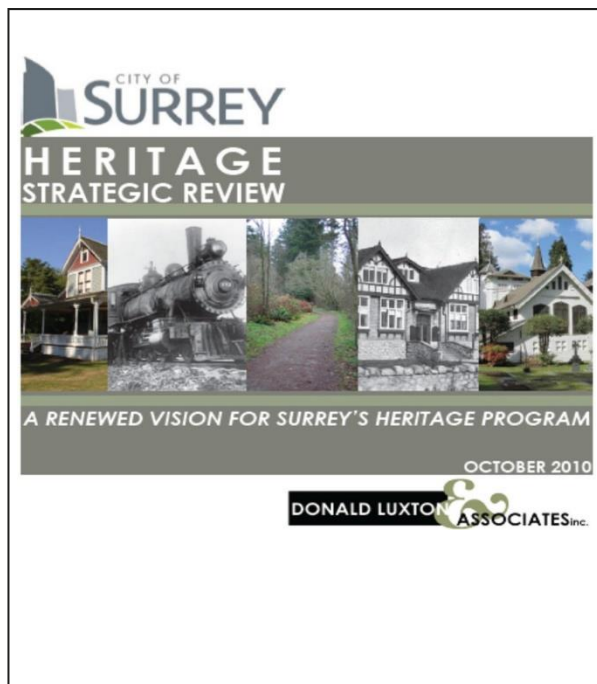
Stewart Farm House, Surrey.

Figure 48: Protected Heritage Sites and Routes



F8 POLICIES: Conservation Planning

- F8.6 Prohibit the destruction, excavation or alteration of archaeological sites without a permit as specified in the Provincial *Heritage Conservation Act* (as amended).
- F8.7 Integrate heritage resources into the physical development and evolution of Surrey to create a highly-desired urban environment and strong Sense of Place.
- F8.8 Enhance Surrey’s historical resources by encouraging development to sensitively incorporate the preservation of heritage resources and artifacts into their projects.
- F8.9 Maintain and regularly update Surrey’s Heritage Register in accordance with provisions of the *Local Government Act*, the *Heritage Conservation Act* and *Community Charter* (as amended).
- F8.10 Evaluate buildings, sites and features on Surrey’s Heritage Register on an on-going basis. Work with owners of Registrar properties to develop Heritage Revitalization Agreements (HRAs), Conservation Covenants and Heritage Designation By-laws.
- F8.11 Maintain and update an evaluation framework to more concisely review the heritage value of a property and to help serve as a guideline for determining how heritage resources should be managed.
- F8.12 Incorporate heritage assessments and planning into the development review of secondary plans, park plans, transportation plans and the construction and maintenance of City infrastructure and capital projects.
- F8.13 Ensure the preservation and retention of heritage resources takes place equally throughout the entire city.
- F8.14 Wherever possible, preserve heritage buildings in their original location. Where this is not possible, and as a last resort, encourage relocation and restoration on an appropriate alternative site.
- F8.15 Retain historically and culturally-significant view corridors, focal points, trails, view-points, landmarks and vistas at a city-wide and neighbourhood level.
- F8.16 Protect farmland as a source of heritage for Surrey as a distinct landscape within the community.
- F8.17 Protect identified heritage and significant trees, such as along King George Boulevard and Semiahmoo Trail, particularly during road works projects.



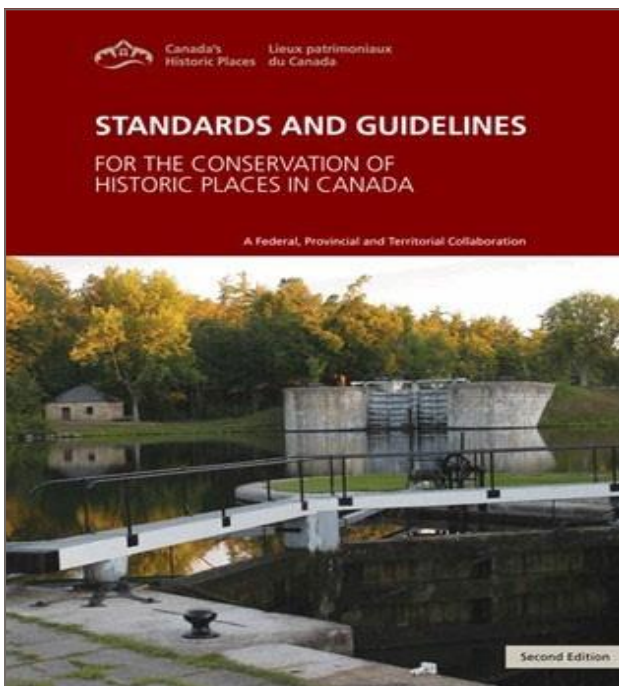
Surrey Heritage Strategic Review (2010) guides the direction of heritage conservation in the City of Surrey.

F8 POLICIES: Interpretation

- F8.18 Work toward increasing community awareness of Surrey’s heritage resources through heritage interpretation programs, events, heritage storyboards, signage and markers.
- F8.19 Support museums and other heritage sites in the stewardship of Surrey’s heritage resources, artifacts and archival documents.
- F8.20 Support heritage tourism as a substantial and significant component of Surrey’s economy.
- F8.21 Identify and mark historic street names throughout the city to help connect with Surrey’s past and to help create unique identities and a Sense of Place.

F8 POLICIES: Implementation

- F8.22 Implement the *Surrey Cultural Plan* (as amended) as an effective tool in establishing and maintaining heritage programs, services and facilities in Surrey.
- F8.23 Use tax exemptions, as established in the *Community Charter* (as amended), as a heritage conservation tool. Continue to use financial assistance programs to support owners maintaining, restoring and protecting heritage sites.
- F8.24 Use application prioritization where there are heritage resources on a proposed development site. Use multiple heritage conservation techniques including heritage zone overlays, setback adjustments and land use relaxations, as authorized in the *Local Government Act* (as amended).
- F8.25 Investigate using Heritage Conservation Areas (HCA) as a means to conserve identifiable areas with buildings, sites and features that have significant heritage value worthy of preservation.
- F8.26 Support, develop and grow a strong heritage community by using cultural granting programs and by employing and using new technologies to provide remote access to heritage information and resources.



Canadian National Standards and Guidelines

F9

Community Safety

Ensure a safe community through effective crime prevention and emergency response

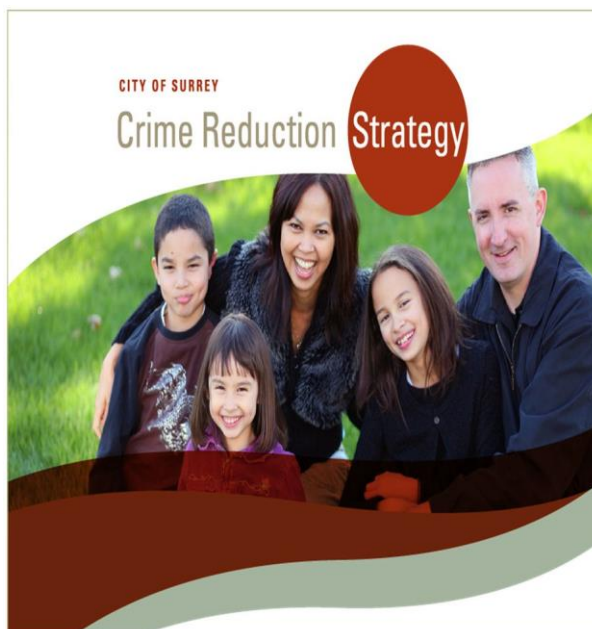
A safe environment is fundamental to a high quality of life for Surrey residents and is closely associated with the City’s image as a great place to live, visit and conduct business. The City’s *Crime Reduction Strategy* provides a comprehensive blueprint to reduce crime and improve community safety, increase public involvement in crime reduction and enhance public awareness about the reality and perception of crime.

Achieving a safe city involves a community-wide effort and working with partners such as law enforcement, emergency response personnel, social agencies, education and literacy groups and neighbourhood associations. The policies in this section focus on ensuring these partnerships are optimized for the benefit of Surrey’s residents and visitors, making Surrey a safe and secure place to enjoy.

In order to meet Objective F9, the City will:

F9 POLICIES: General

- F9.1 Continue to allocate resources to police and protection services in proportion to the City’s overall growth.
- F9.2 Update and implement the recommendations of the *Crime Reduction Strategy* (as amended) in consultation and partnership with the RCMP, School District No. 36, Provincial government agencies and community groups, as the foundation for creating safe communities in Surrey.
- F9.3 Consider community safety issues, including crime reduction, traffic safety and emergency response in all City planning processes and development reviews.



Surrey’s Crime Reduction Strategy (2007) focuses on four strands that frame all issues: prevent and deter crime, apprehend and prosecute offenders, rehabilitate and reintegrate, and the reality and perception of

F9 POLICIES: Crime Reduction

- F9.4 Support safe community interventions, including the use of community action groups, graffiti reduction policies, neighbourhood unsightly premises maintenance programs, and the continued enhancement and expansion of a city beautification strategy.

F9.8 Support after school and community-based recreation and environmental programs as part of a holistic crime reduction strategy.
- F9.5 Support and encourage participation of residents and property owners in crime reduction programs such as the Crime Free Multi-Housing Program or other similar initiatives.

F9.9 Support and encourage participation in local neighbourhood and business safety initiatives.
- F9.6 Collaborate with the RCMP, senior government ministries and agencies, the Fraser Health Authority, social service providers and non-profit organizations to identify and address housing and social service gaps that may contribute to crime and public safety issues.

F9.10 Coordinate with Surrey RCMP and the non-profit sector to address youth-specific issues in order to reduce nuisance crime and youth victimization.
- F9.7 Support the community policing concept and encourage the continued use and expansion of small local-serving policing offices, particularly in City Centre and within Town Centres.

F9.11 Incorporate Crime Prevention Through Environmental Design (CPTED) principles and practices into the review of all development applications and at the design stage of all City capital projects.
- F9.12 Conduct CPTED and crime prevention audits of public spaces and, where feasible, implement recommendations arising from these reviews.



Design of public facilities take s into account crime reduction techniques and design principles.

F9 POLICIES: Traffic Safety

- F9.13 Coordinate with the Surrey RCMP to address traffic safety issues in a holistic way, particularly around schools and critical accident locations.
- F9.14 Ensure traffic safety is considered in the planning of neighbourhoods throughout the city including land use relationships, street and pathway networks and intersection design.
- F9.15 Work with local neighbourhoods to address specific safety issues, with an emphasis on elementary schools.

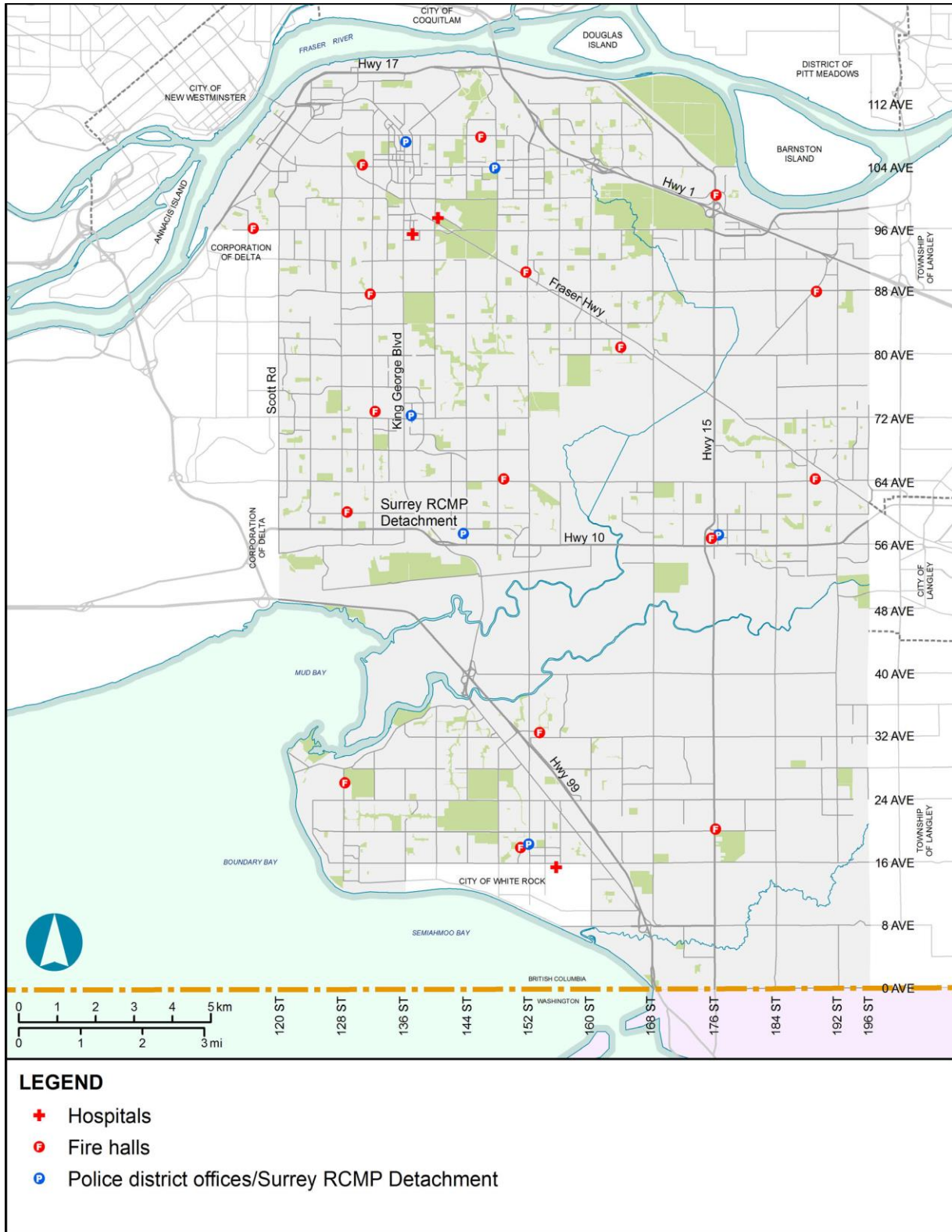
F9 POLICIES: Emergency Response and Management

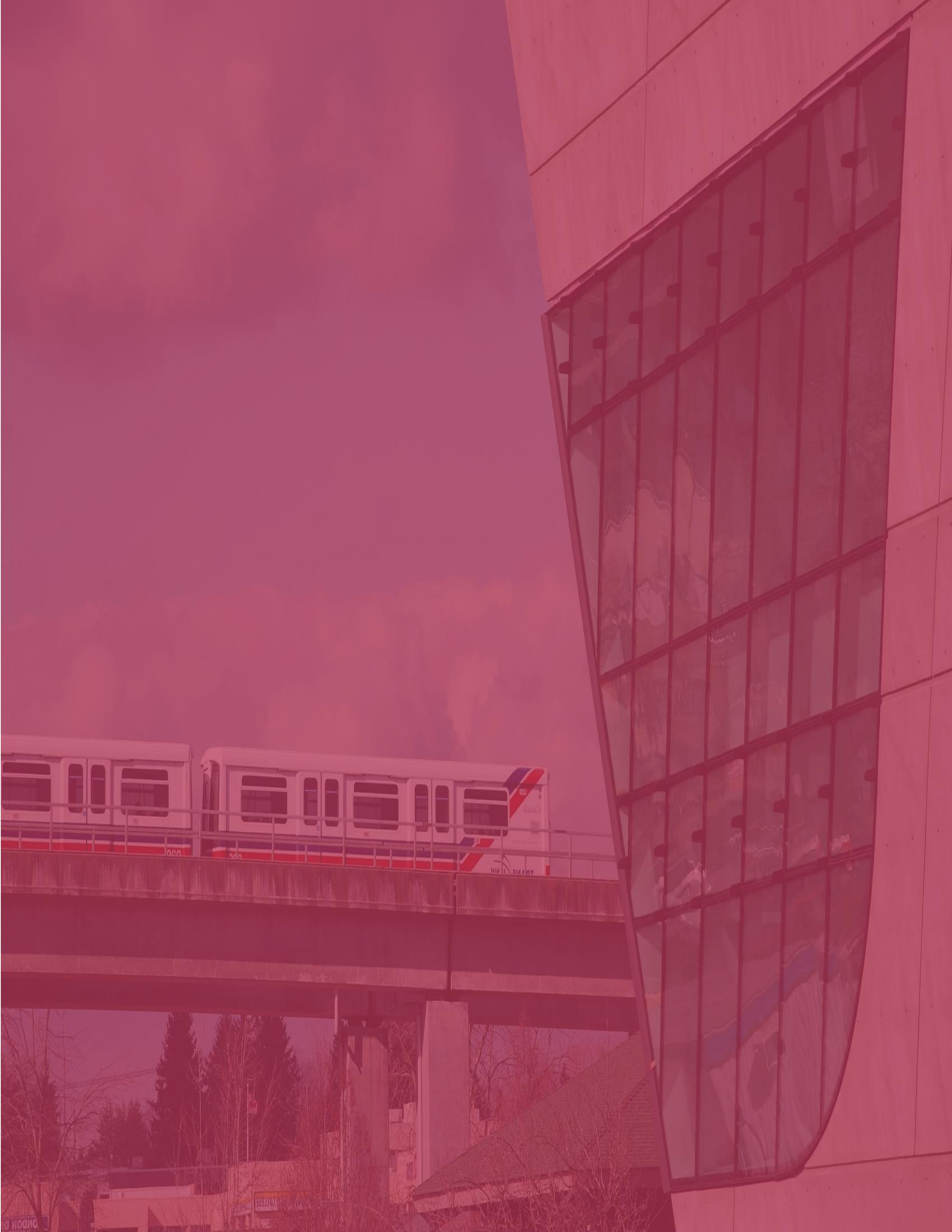
- F9.16 Ensure that emergency management plans are in place and ready to be implemented in response to emergencies including fire, accidents, hazardous material spills, flooding and natural disasters.
- F9.17 Incorporate emergency response planning into secondary plans, capital works and development application review processes.
- F9.18 Strategically locate community safety facilities, including fire halls and community policing offices to deliver effective and timely emergency response services throughout the city (see Figure 49).
- F9.19 Ensure emergency social service systems, preparation plans and logistical needs are established in advance of emergencies.



Surrey fire Hall No. 14, South Surrey.

Figure 49: Public Health and Safety

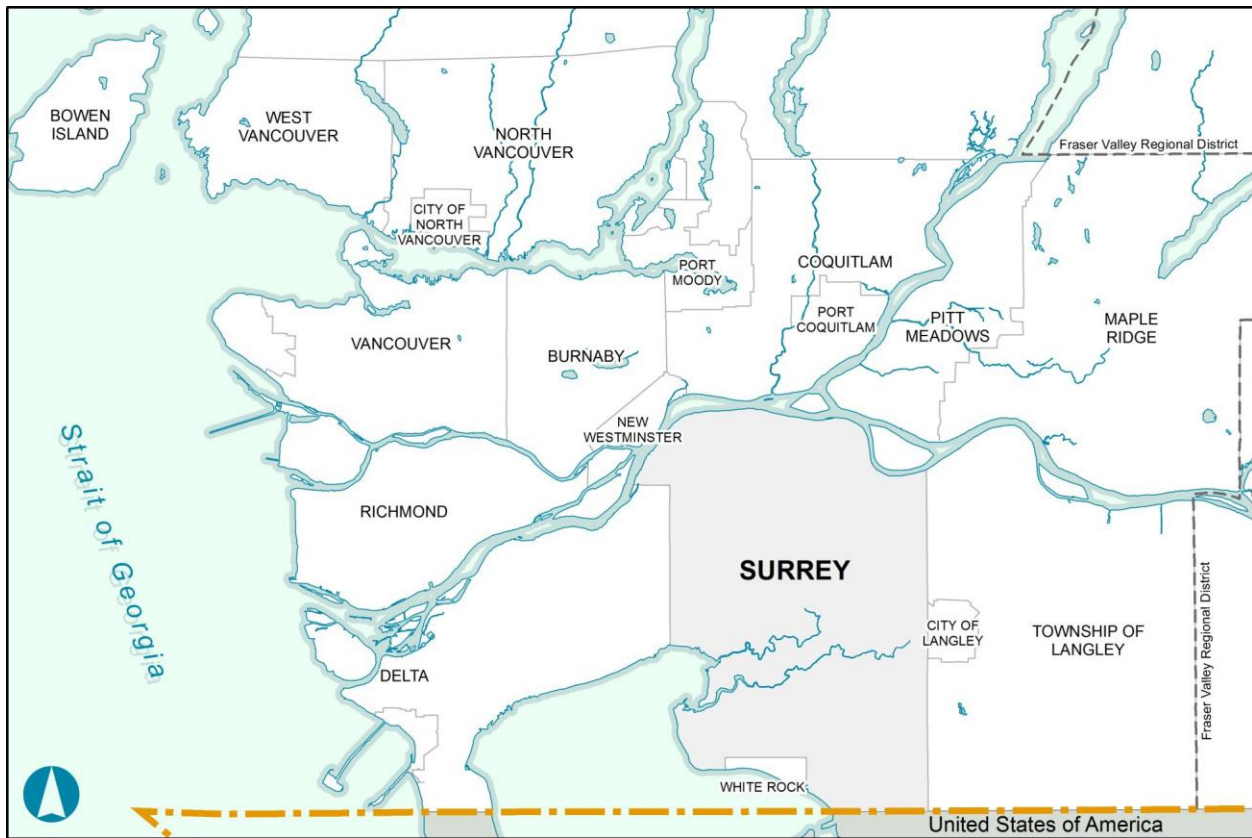




REGIONAL CONTEXT STATEMENT



Figure 50: RCS: Surrey's Regional Location



AUTHORITY

Under the provisions of the *Local Government Act* (LGA) (Sections 428 and 429) regional districts are required to prepare a regional-scale land use plan called a *Regional Growth Strategy* (RGS). This *Strategy* must cover a period of a least 20 years and include a comprehensive statement on the future of the region, including social, economic and environmental objectives, population and employment projections, economic development, and actions proposed regarding housing, transportation, regional services, parks and natural areas.

In July 2011, Metro Vancouver adopted *Metro Vancouver 2040* as an official Regional Growth Strategy. Member municipalities, including the City of Surrey (see Figure 50), are required by Section 446 of the *Local Government Act*, to submit a Regional Context Statement (RCS) for approval by the Metro Vancouver Board. The RCS is intended to indicate how a community's Official Community Plan (OCP) meets, or will meet, the goals and objectives of the *Regional Growth Strategy* in developing a stable, environmentally responsible, transit-oriented city.

REGIONAL CHALLENGES

Metro Vancouver's *Regional Growth Strategy* outlines the significant challenges that face the Lower Mainland including how to: 1) accommodate growth to advance livability and sustainability; 2) build healthy, complete communities; 3) support economic prosperity; 4) protect the natural environment; 5) respond to climate change impacts and natural hazard risks; and 6) protect agricultural land to support food production.

These challenges are addressed in five goals that are intended to address the region's challenges. These goals include:

1>Create Compact Urban Areas:

Commit to a compact region that works against sprawling development which increases costs to human and environmental health and creates inefficiencies in land use and resource consumption.

2|Support a Sustainable Economy:

Build on economic strengths from a regional perspective and create diversity in commercial and industrial industries.

3|Protect the Environment and Respond to Climate Change Impacts:

Address significant climatic issues through environmental conservation and protection.

4|Develop Complete Communities:

Create and support communities that offer all aspects of a healthy lifestyle to all residents.

5|Support Sustainable Transportation Choices:

Align land use and transportation decisions to create a truly sustainable region.

Surrey's Regional Context Statement indicates how the policies contained in this Official Community Plan align, respond to and will meet the goals and strategies of the RGS.

INTRODUCTION

The City of Surrey, through its Official Community Plan, supports the vision of Metro Vancouver's *Regional Growth Strategy* for a sustainable region and aligns its own future vision in support of this goal. This includes:

- developing Surrey City Centre as the Region's second Metropolitan Centre
- focusing additional growth capacity to Town Centres and transit corridors
- maintaining the Urban Containment Boundary and the Agricultural and Conservation/ Recreation land use designations
- planning urban land uses to reduce dependence on the private automobile
- committing to the development and protection of employment areas that underpin the regional economy.

Within Surrey's evolutionary context lies regional issues that can only be addressed appropriately through the collaborative efforts of multiple municipal jurisdictions. These efforts have been categorized into a Regional Growth Strategy which identifies the specific efforts that need to be made on a local level in order to reach a regional goal of sustainable growth and development.

For Surrey to assist in achieving these regional objectives and to meet the share of population growth anticipated in the RGS, significant infrastructure needs to be delivered through regional, provincial and national partnerships. Major transit and transportation improvements are required to support Surrey's projected growth including appropriate replacement of the Pattullo Bridge and George Massey Tunnel; addition of rapid transit and bus service expansions in Surrey; improvements to Highway 99 including new and expanded interchanges at 32 Avenue, 24 Avenue and 10 Avenue; and improvements to Highway 1 (Trans Canada Highway) at 192 Street. These major infrastructure projects are beyond the scope or mandate of the City of Surrey and require regional, provincial and federal funding to realize. Surrey's share of Regional population growth also requires appropriate and timely Provincial and Federal investment in hospitals, schools, affordable housing, supportive housing, support services for children and for immigrants and refugees. Without these regional and government investments, Surrey will not be able to meet the growth projections set out in the RGS and as shown in Tables 8 through 15 of the Regional Context Statement.

RGS and OCP ALIGNMENT

The City of Surrey is proposing to amend the *Regional Growth Strategy* Land Use Designation from Industrial to Mixed Employment for eight properties, totaling 8.5 hectares (21 acres). Until these applications are complete, existing land designations are shown in this Regional Context Statement and Official Community Plan. It is anticipated the RGS and Surrey's OCP will become aligned at the conclusion of the RGS amendment process.

OCP AMENDMENTS

Amendments to Official Community Plan Land Use Designations that fall within Surrey's Urban Containment Boundary, as illustrated on Figure 51, that are not consistent with the Metro Vancouver *Regional Growth Strategy* Land Use Designations, shown in Figure 51, shall be permitted consistent with Policy 6.2.7: Providing for Appropriate Municipal Flexibility of the Metro Vancouver RGS.

Amendments to Official Community Plan Urban Centres and Frequent Transit Development Areas boundaries, as illustrated in Figures 52 through 60, shall be permitted consistent with Policy 6.2.8 of the Metro Vancouver RGS.

GOAL 1: CREATE A COMPACT URBAN AREA

The first goal of Metro Vancouver's *Regional Growth Strategy* is achieved by reducing sprawl, expanding infrastructure smartly and cost-efficiently, creating urban areas well serviced by multi-modes of transportation and by focusing development to a higher density that optimizes a constrained urban land base.

Strategies

- 1.1 Contain urban development within the Urban Containment Boundary.
- 1.2 Focus growth in Urban Centres and Frequent Transit Development Areas.
- 1.3 Protect rural areas from urban development.



1.1 Urban Containment

Contain Urban Development within the Urban Containment Boundary

In order to meet Strategy 1.1, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

1.1.3(a) GROWTH CONTAINMENT

Surrey's Official Community Plan identifies an Urban Containment Boundary (UCB) (see Figure 51) illustrating where urban development will be contained in order to support a growing, yet compact and sustainable, community. This Urban Containment Boundary is consistent with the RGS Regional Land Use Designations (see Figure 51). Policies within Surrey's OCP supporting the maintenance of the Urban Containment Boundary include: A1.1, A1.2, A1.3 and A1.4.

1.1.3(b) OVERALL POPULATION, DWELLING AND EMPLOYMENT PROJECTIONS

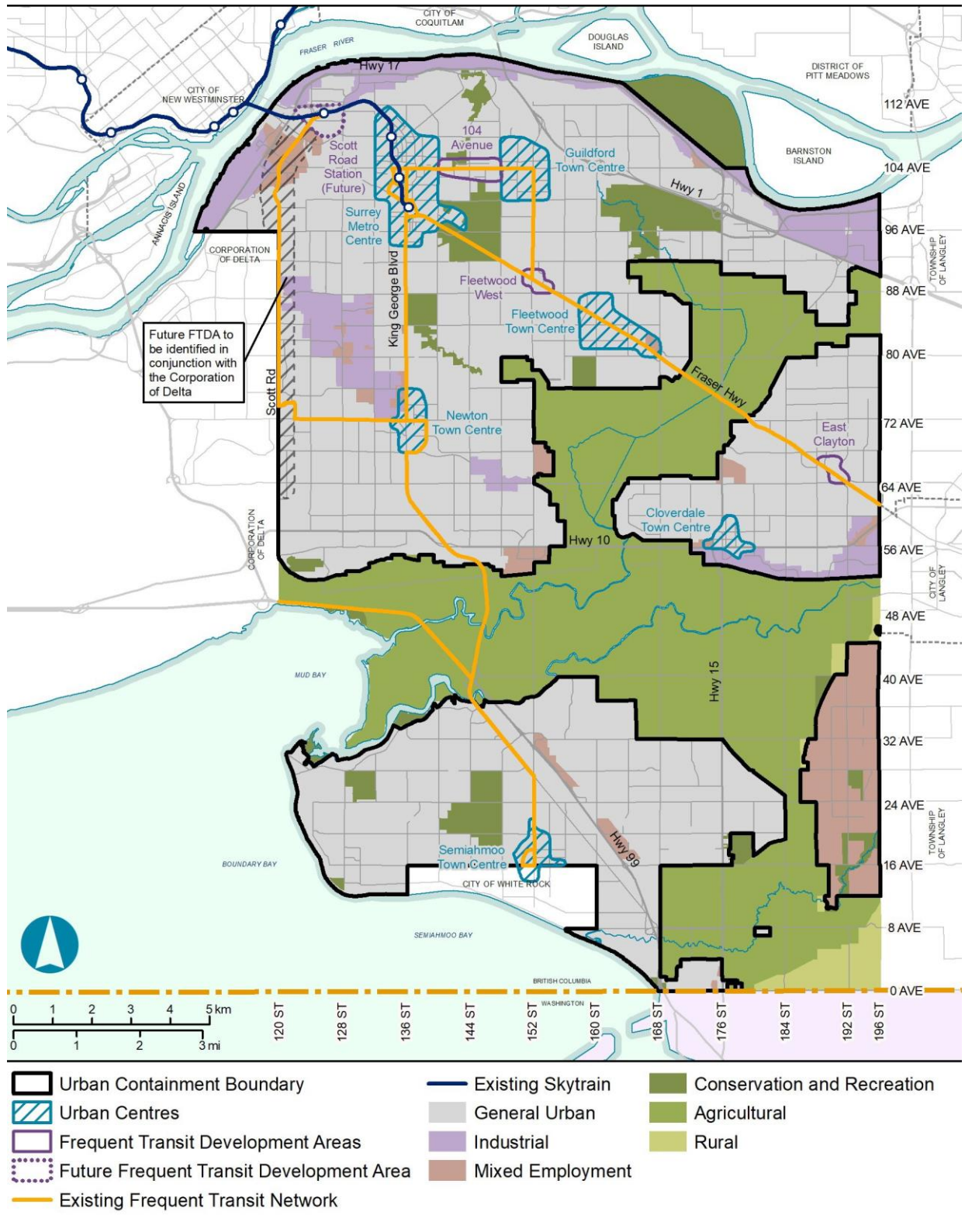
Surrey is expected to accommodate a large proportion of the Region's population growth over the coming decades. This population increase will also coincide with an increase in construction of dwelling units and employment to support new residents. Population, dwelling unit and employment projections shown in Table 8 and 9 are generally consistent with population and employment projections shown in Table A1 of the RGS and with policies within Surrey's OCP including: A1.3, A2.1, A4.1, A4.2, A4.4 and B1.1.



Park Place under development, Surrey City Centre

Figure 51: Regional Growth Strategy Land Use Designations

BL 18928
BL 20393



CITY-WIDE PROJECTS

TABLE 8: RGS STRATEGY 1.1.3(b) SURREY POPULATION PROJECTIONS	
<u>Year</u>	<u>Population</u>
2012	502,725
2017	553,475
2021	593,600
2026	639,600
2031	685,250
2036	727,750
2041	770,200

SOURCE: City of Surrey and Metro Vancouver

TABLE 9: RGS STRATEGY 1.1.3(b) SURREY DWELLING AND EMPLOYMENT PROJECTIONS		
<u>Year</u>	<u>Dwelling Units</u>	<u>Jobs</u>
2012	164,935	171,200
2017	184,385	195,200
2021	199,950	214,200
2026	219,450	236,200
2031	238,800	257,950
2036	256,800	277,450
2041	274,900	296,600

SOURCE: City of Surrey and Metro Vancouver
NOTE: Dwelling Units refer to occupied units.

1.2

Focused Growth

Focus Growth in Urban Centres and Frequent Transit Development Areas

In order to meet Strategy 1.2, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

1.2.6(a)

URBAN CENTRE AND FREQUENT TRANSIT DEVELOPMENT AREA DWELLING UNIT AND EMPLOYMENT PROJECTIONS

Urban Centres and identified Frequent Transit Development Areas (FTDA) within Surrey are anticipated to accommodate a large portion of Surrey's dwelling unit and employment increases (shown in Tables 10 through 15). While dwelling unit and employment in Surrey's Urban Centres and FTDA's are expected to grow at a higher rate compared to the rest of the city, the availability of significant industrial and employment lands and for new urban neighbourhoods in Surrey compared to the rest of the region, results in growth shares in Urban Centres that are lower than the regional targets shown in Table 2 of the RGS. It is expected that as available, planned undeveloped lands are absorbed, that the share of population and employment in Urban Centres will increase. Policies within Surrey's OCP supporting focused growth in Urban Centres include: A1.3, A2.1, A2.2, A2.3, A2.4, and B1.1.

1.2.6(b)(i)

URBAN CENTRE LOCATIONS, BOUNDARIES AND TYPES

The *Regional Growth Strategy* identifies six Urban Centres within Surrey (City Centre, Cloverdale, Guildford, Fleetwood, Newton and Semiahmoo) that are planned for regional growth. The locations and boundaries of these Urban Centres are shown in Figures 52 through 57.

1.2.6(b)(ii)

URBAN CENTRE DEVELOPMENT AND GROWTH

Surrey's Urban Centres are planned to accommodate regional growth well into the future. Further planning reviews of Urban Centres is anticipated to enhance the capacity for growth. Policies within Surrey's OCP supporting this growth include: B2.1 and B2.2.

1.2.6(b)(iii)

URBAN CENTRE OFFICE DEVELOPMENT

Surrey's Urban Centres are intended to accommodate transit oriented office development to support future population increases. Surrey's OCP illustrates support for this use in the Land Uses and Densities Section, on Figure 3 and in policies A2.4, A2.5, B1.1, B1.6 and B1.7.

SURREY METRO CENTRE PROJECTIONS

TABLE 10: RGS STRATEGY 1.2.6(a) SURREY METRO CENTRE DWELLING UNIT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Dwelling Units	-	5,730	7,270	7,740	20,740
Proportion of City Total	-	16.4%	18.7%	21.4%	18.9%
Total Dwelling Units	11,760	17,490	24,760	32,500	-
Proportion of City Total	7.1%	8.7%	10.4%	11.8%	-
Dwelling Units City-Wide	164,935	199,950	238,800	274,900	109,965

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect the Surrey Metro Centre boundary as shown on Figure 52.

Dwelling Units refer to occupied units.

Over the course of the life of this Official Community Plan, projections are expected to be adjusted accordingly.

TABLE 11: RGS STRATEGY 1.2.6(a) SURREY METRO CENTRE EMPLOYMENT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Jobs	-	3,670	4,540	5,260	13,470
Proportion of City Total	-	8.5%	10.4%	13.6%	10.7%
Total Jobs	23,180	26,850	31,390	36,650	-
Proportion of City Total	13.5%	12.5%	12.2%	12.4%	-
Jobs City-Wide	171,200	214,200	257,950	296,600	125,400

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect the Surrey Metro Centre boundary as shown on Figure 52.

Over the course of the life of this Official Community Plan, projections are expected to be adjusted accordingly.

SURREY TOWN CENTRE PROJECTIONS

TABLE 12: RGS STRATEGY 1.2.6(a) SURREY TOWN CENTRE DWELLING UNIT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Dwelling Units	-	4,760	5,050	5,650	15,460
Proportion of City Total	-	13.6%	13.0%	15.7%	14.1%
Total Dwelling Units	15,710	19,930	24,980	30,630	-
Proportion of City Total	9.2%	10.0%	10.5%	11.1%	-
Dwelling Units City-Wide	164,935	199,950	238,800	274,900	109,965

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect the Surrey Town Centre boundaries are shown on Figures 53-57.

Dwelling Units refer to occupied units.

Over the course of the life of this Official Community Plan, projections are expected to be adjusted accordingly.

TABLE 13: RGS STRATEGY 1.2.6(a) SURREY TOWN CENTRE EMPLOYMENT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Jobs	-	6,730	7,030	6,300	20,060
Proportion of City Total	-	15.7%	16.1%	16.3%	16.0%
Total Jobs	22,310	29,040	36,070	42,370	-
Proportion of City Total	13.0%	13.6%	14.0%	14.3%	-
Jobs City-Wide	171,200	214,200	257,950	296,600	125,400

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect the Surrey Town Centre boundaries are shown on Figures 53-57.

Over the course of the life of this Official Community Plan, projections are expected to be adjusted accordingly.

SURREY FTDA PROJECTIONS

TABLE 14: RGS STRATEGY 1.2.6(a) SURREY FTDA DWELLING UNIT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Dwelling Units	-	1,870	2,150	2,250	6,270
Proportion of City Total	-	5.3%	5.5%	6.2%	5.7%
Total Dwelling Units	750	2,620	4,770	7,020	-
Proportion of City Total	0.5%	1.3%	2.0%	2.6%	-
Dwelling Units City-Wide	164,935	199,950	238,800	274,900	109,965

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect Surrey's FTDA boundaries are shown on Figures 58 through 60.

Dwelling Units refer to occupied units.

As additional FTDA's are added over time, projections will be adjusted accordingly.

TABLE 15: RGS STRATEGY 1.2.6(a) SURREY FTDA EMPLOYMENT PROJECTIONS					
	<u>2012</u>	<u>2021</u>	<u>2031</u>	<u>2041</u>	<u>Total Increase</u>
New Jobs	-	1,650	1,830	1,840	5,320
Proportion of City Total	-	3.8%	4.2%	4.8%	4.2%
Total Jobs	3,810	5,460	7,290	9,130	-
Proportion of City Total	2.2%	2.5%	2.8%	3.1%	-
Jobs City-Wide	171,200	214,200	257,950	296,600	125,400

SOURCE: City of Surrey and Metro Vancouver.

NOTES: Projections illustrated here reflect Surrey's FTDA boundaries are shown on Figures 58 through 60.

As additional FTDA's are added over time, projections will be adjusted accordingly.

Figure 52: RCS: Urban Centre Boundary—Surrey Metro Centre

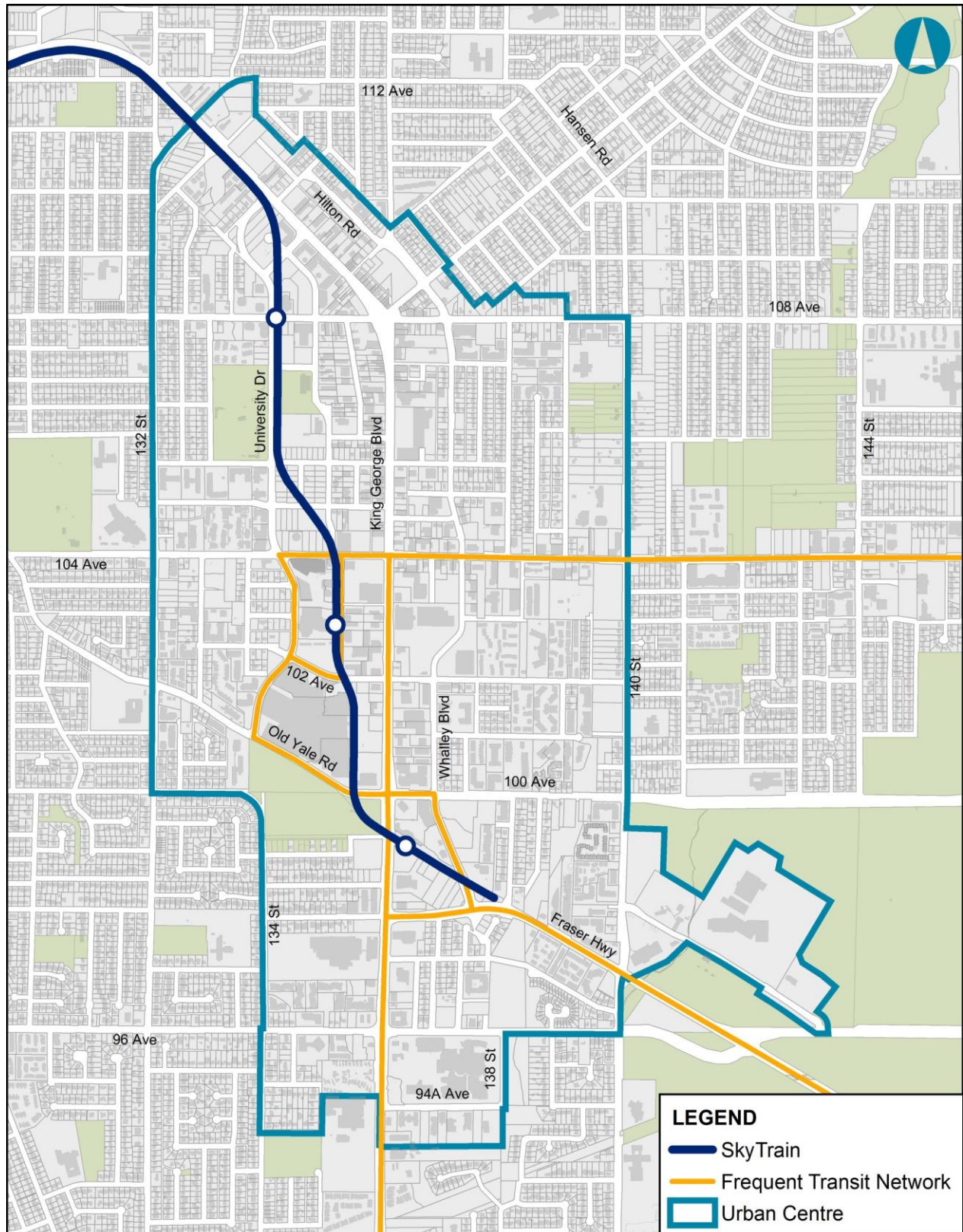


Figure 53: RCS: Urban Centre Boundary—Cloverdale Town Centre

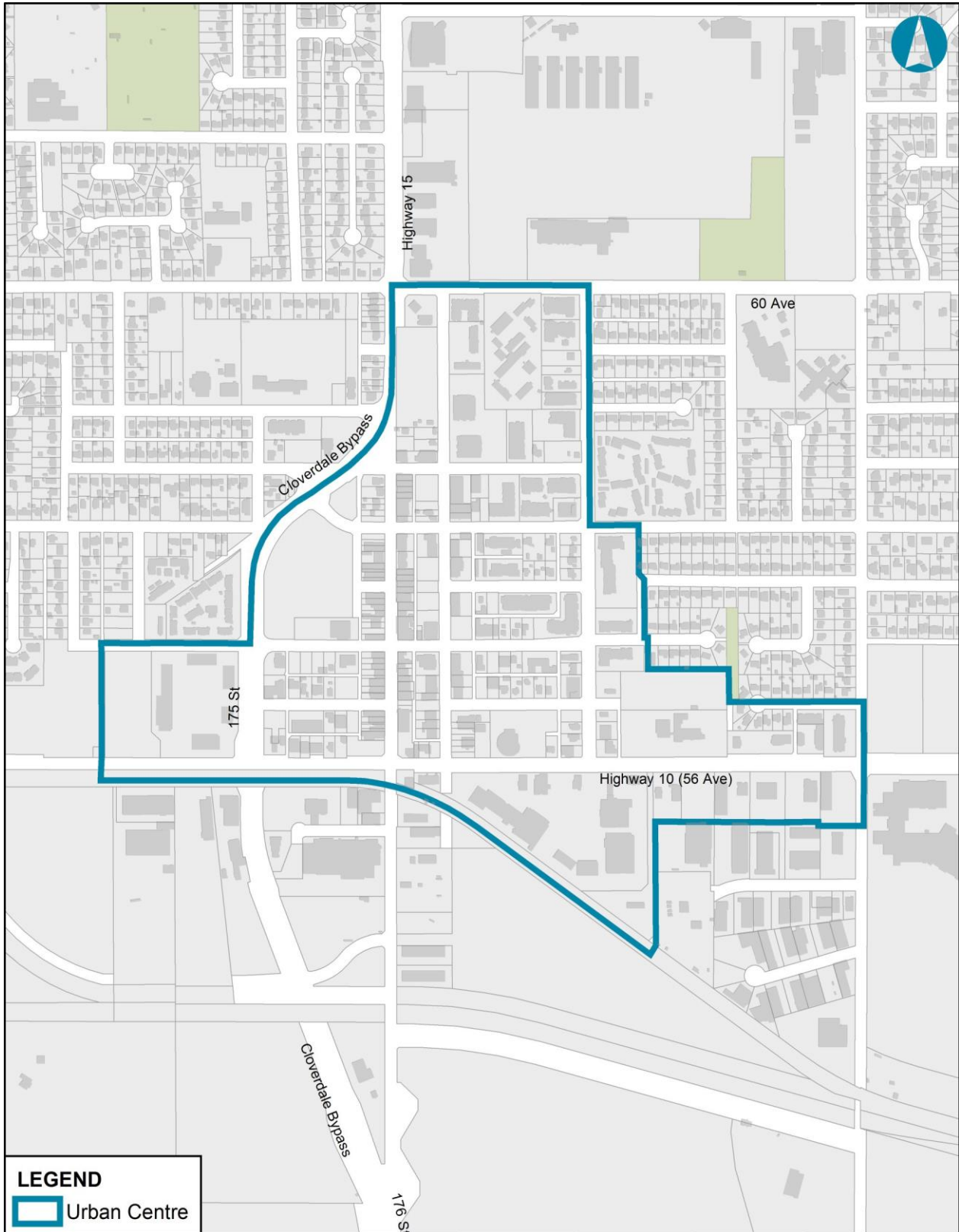


Figure 54: RCS: Urban Centre Boundary—Fleetwood Town Centre

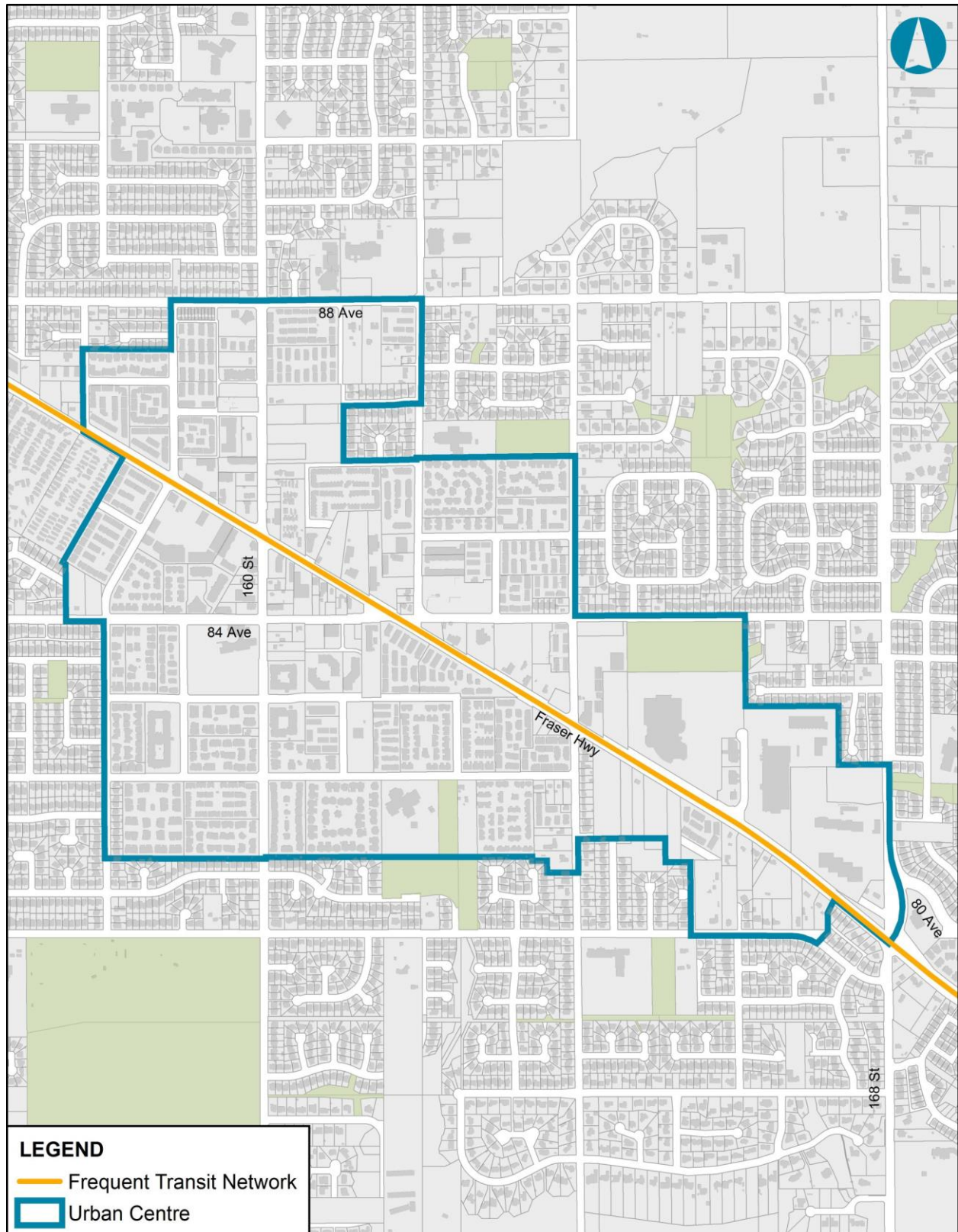


Figure 55: RCS: Urban Centre Boundary—Guildford Town Centre

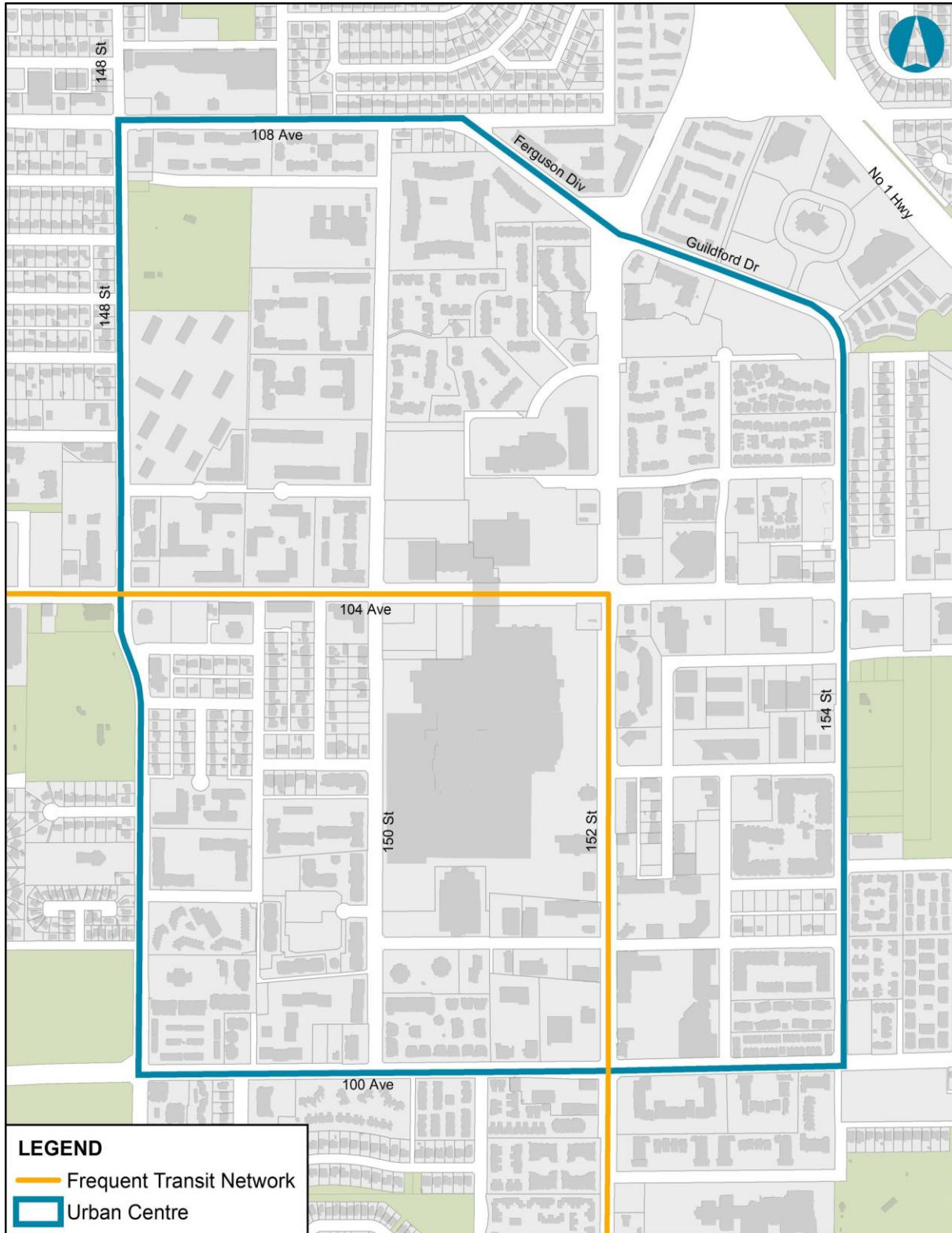


Figure 56: RCS: Urban Centre Boundary—Newton Town Centre

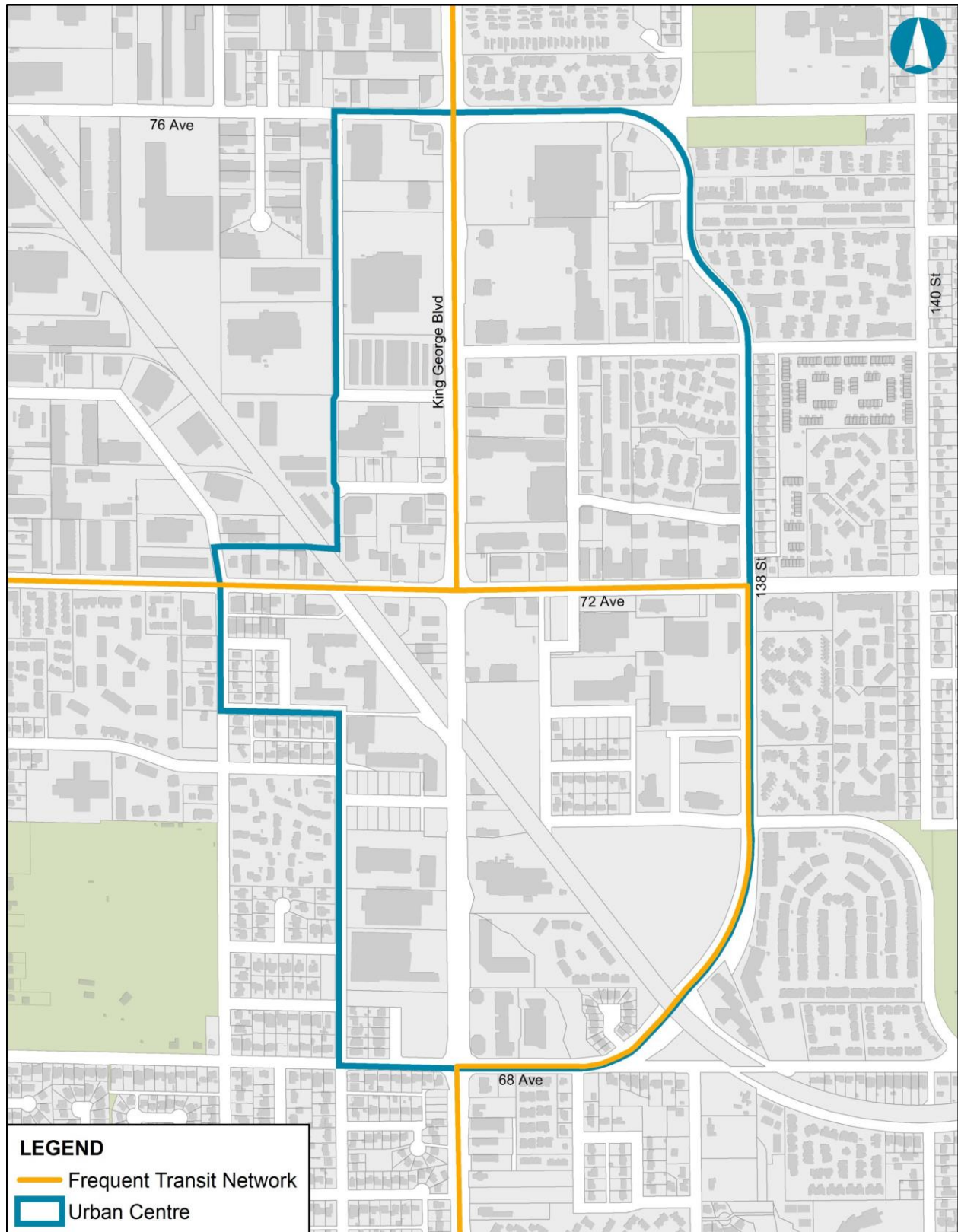
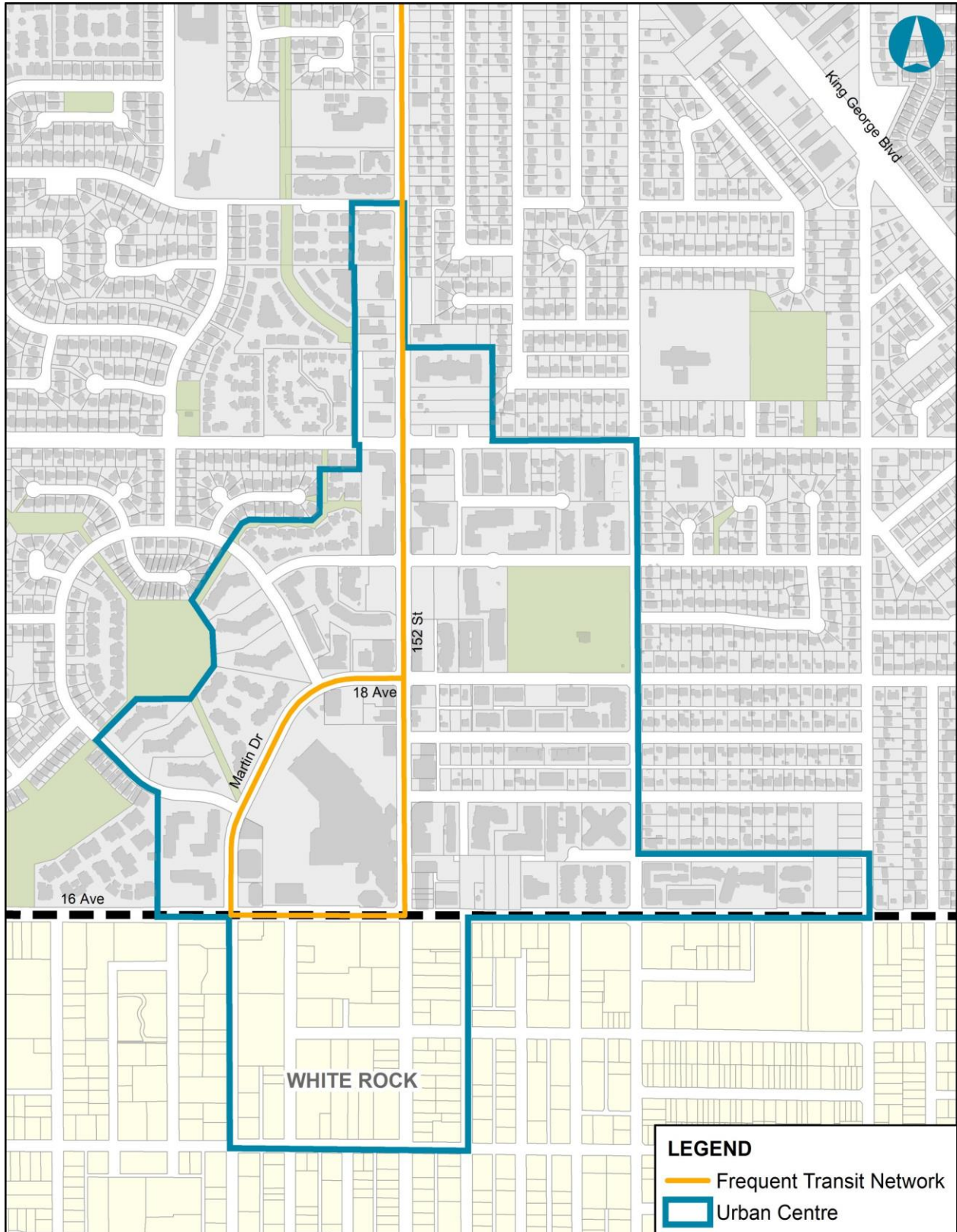


Figure 57: RCS: Urban Centre Boundary—Semiahmoo Town Centre



1.2 Focused Growth (cont.)

1.2.G(b)(iv)

URBAN CENTRE PARKING REQUIREMENTS

To support sustainable development practices in Urban Centres, it is essential that the right balance is struck between reducing residential and commercial parking requirements and increasing the availability, convenience and use of public transit. OCP policies supporting a reduction in parking requirements in locations well served by transit include: B1.34, B2.12, C2.45 and C2.46.

1.2.G(c)(i)

FREQUENT TRANSIT DEVELOPMENT AREA LOCATIONS AND BOUNDARIES

Three FTDA have been identified for Surrey. Additional FTDA locations will be examined in the future, including, if appropriate, along the Scott Road Corridor. In addition to Urban Centres, FTDA are intended to accommodate higher densities where a high level of public transit is, or will be, available. These areas are shown in Figures 58 through 60 and supported by policy within Surrey's OCP including: A2.6 and B3.6.

1.2.G(c)(ii)

FREQUENT TRANSIT DEVELOPMENT AREA DEVELOPMENT AND GROWTH

The *Regional Growth Strategy* identifies Frequent Transit Development Areas where regional growth will be accommodated. The Land Use and Densities Section of the OCP indicates where higher densities are available in FTDA. Policies within Surrey's OCP supporting these areas include: A1.3, A2.6, B3.2, B3.5 and B3.9.

1.2.G(c)(iii)

FREQUENT TRANSIT DEVELOPMENT AREA PARKING REQUIREMENTS

In order to support sustainable development practices in Frequent Transit Development Areas, it is essential that the right balance is struck between reducing residential and commercial parking requirements while also increasing the availability, convenience and use of public transit. Policies within Surrey's OCP supporting these adjustments include: B1.34, C2.45 and C2.46.

1.2.G(d)(i)

URBAN LOCATIONS AND BOUNDARIES

Surrey's General Urban areas are intended to accommodate various types of residential and commercial development to accommodate and serve Surrey's projected population increases. Surrey's OCP illustrates support for this designation in the Land Uses and Densities Section and on Figure 3.

1.2.G(d)(ii)

URBAN DENSITIES

Surrey's Urban locations fall outside of Urban Centres and Frequent Transit Development Areas and are intended to accommodate low to medium residential densities. Surrey's OCP illustrates support for this land use designation in the Land Uses and Densities Section and through policies: A1.2, A1.3, B4.2, B2.10, B4.3, B4.6, B4.11 and B4.12.

1.2.G(d)(iii)

SMALL SCALE LOCAL CENTRES

Small scale mixed-use developments are permitted throughout Surrey in the Urban designation providing local services and a broad mix of housing types. Surrey's OCP illustrates support for this designation in the Land Uses and Densities Section.

1.2.G(d)(iv)

URBAN AREA, NON-RESIDENTIAL, MAJOR TRIP-GENERATING USES

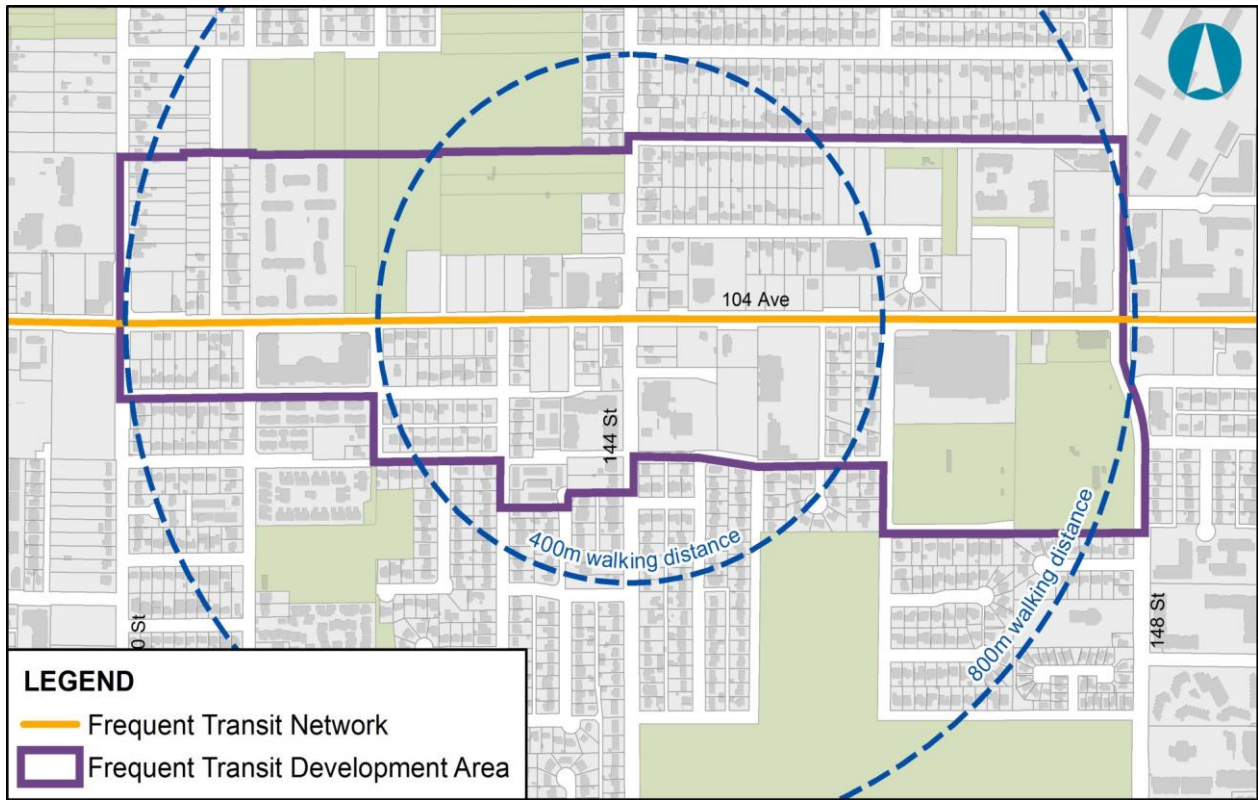
Except as indicated in approved Secondary Plans, Surrey's Urban land use designations do not support major trip-generating commercial or industrial uses outside of Urban Centres or FTDA. Major trip-generating uses are defined as: regional and major civic institutions, hospitals, government office or high density residential or commercial development in excess of 1.5 FAR. Policies within Surrey's OCP supporting Urban Area uses include: A2.2, A2.3, A2.4, B1.9, C2.47, E1.19, E1.20 and E1.23.

1.2.G(d)(v)

INFILL DEVELOPMENT

Supporting infill development is seen as an important role in efficiently using Surrey's developable land. Policies within Surrey's OCP supporting infill development include: A3.1, A3.2, A3.3 and A3.5.

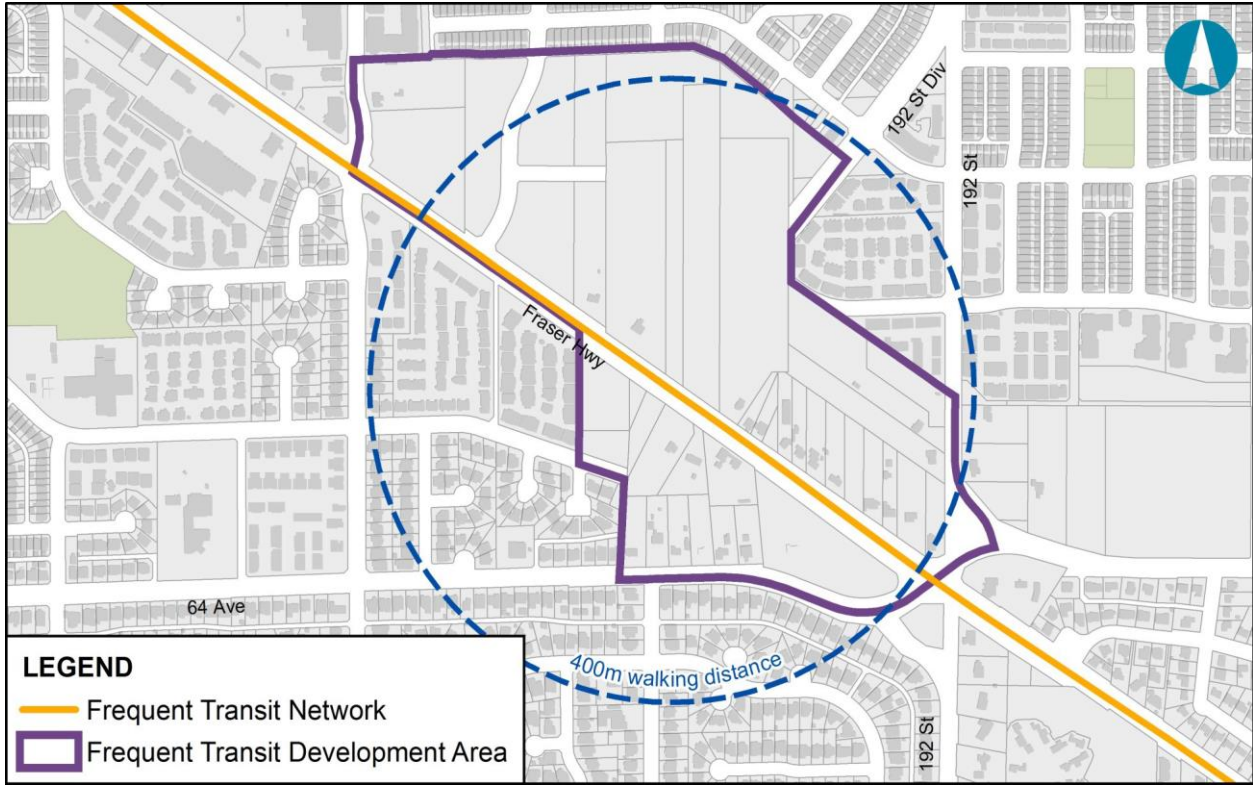
Figure 58: RCS: FTDA Boundary—104 Avenue



Conceptualization of Light Rapid Transit along 104 Avenue, Surrey.

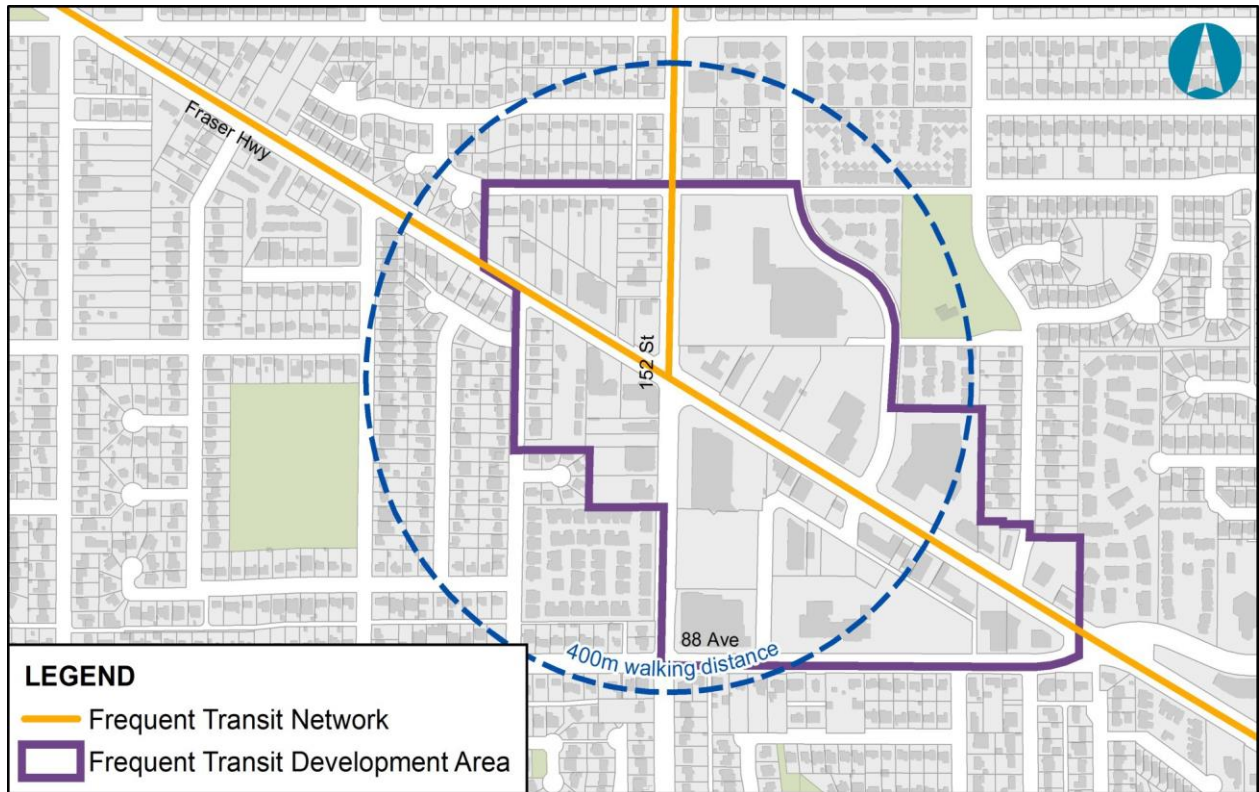


Figure 59: RCS: FTDA Boundaries—East Clayton



East Clayton, Surrey.

Figure 60: RCS: FTDA Boundary—Fleetwood West



Fleetwood Gardens located in Fleetwood Park; Surrey.



1.2 Focused Growth (cont.)

1.2.6(e) INDUSTRIAL, MIXED-EMPLOYMENT, CONSERVATION AND RECREATION LAND USE DESIGNATIONS

The *Regional Growth Strategy* supports industrial, mixed-employment and conservation and recreation uses as the primary use where they fall within Urban Centres of Frequent Transit Development Areas. Surrey's OCP illustrates support for these designations in the Land Uses and Densities Section and on Figure 3. Policies within Surrey's OCP supporting these land use designations in Urban Centres and FTDA's include: E1.1, E.12, E1.13, and E1.14.

1.2.6(f)(i) INDUSTRIAL/URBAN CONFLICTS

Industrial activities can be negatively impacted by other urban uses. Surrey's OCP supports the reduction of industrial and urban conflicts, illustrated in the following policies: E1.3 and E1.8.

1.2.6(f)(ii) WALKING, CYCLING AND TRANSIT

Urban Centres, Frequent Transit Development Areas and general Urban designated areas should provide ample opportunities for walking, cycling and efficient transit use. There are numerous policies supporting these modes of transportation within Surrey's OCP including: A3.3, A3.7, B1.16, B1.22, B2.11, B2.15, B3.8, B4.1, B4.28, B4.29, B4.31, C2.3 and C3.25.

1.2.6(f)(iii) TRANSIT PRIORITY MEASURES

Providing transit priority measures into Surrey's infrastructure allows for a more convenient and user-friendly transit system. Policies within Surrey's OCP supporting these measures include: C2.5, C2.24 and C2.25.

1.2.6(f)(iv) DISTRICT ENERGY SYSTEMS

Surrey is committed to ensuring its citizens have access to affordable energy sources, now and into the future. Supporting District Energy within its Urban Centres is a first step in providing energy alternatives for Surrey residents. Policies within Surrey's OCP supporting District Energy use include: B1.19, B1.20, B1.21, D4.1, D4.17, D4.18, D4.19 and D4.20.



Multi-Use Surrey Parkway in City Centre, underneath the SkyTrain guide way.

1.3 Rural Protection

Protect Rural Areas from Urban Development

In order to meet Strategy 1.3, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

RURAL AREA LOCATIONS AND BOUNDARIES

Surrey's Rural locations are intended to accommodate low density residential uses as set out in the Land Uses and Densities Section of this OCP and on Figure 3.

1.3.3(b)

RURAL AREA DEVELOPMENT

Development within Rural areas in Surrey is limited to large properties that are not serviced by a sanitary sewer system. Extension of a sanitary sewer system to Rural areas is not expected within the foreseeable future. Surrey's OCP supports Rural areas as identified within the Land Use and Density Section and with policies: A1.5 and A4.3.

1.3.3(c)(i)

RURAL AREA DENSITIES AND FORM

Rural development within Surrey is limited and restricted to large lots of a minimum of 0.8 hectares (2 acres). Surrey's OCP supports Rural densities as identified within the Land Use and Density Section.

1.3.3(c)(ii)

AGRICULTURAL USES

Agricultural land comprises approximately 1/3 of Surrey's entire land base. Surrey's OCP supports Agricultural land as identified within the Land Use and Density Section, on Figure 3 and 43 and with policies: A1.4, E3.4 and E3.5.

1.3.3(a)



Rural neighbourhood; Surrey.

GOAL 2: SUPPORT A SUSTAINABLE ECONOMY

The second goal of Metro Vancouver's *Regional Growth Strategy* is achieved by building, expanding and supporting a region with a diverse economic base that takes advantage of its port and that works to support development in a sustainable manner allowing for a future with appropriate lands protected for commercial and industrial purposes.

Strategies

- 2.1 Promote land development patterns that support a diverse Regional economy and employment close to where people live.
- 2.2 Protect the supply of industrial land.
- 2.3 Protect the supply of agricultural land and promote agricultural viability with an emphasis on food production.



2.1 Diverse Economy

Promote Land Development Patterns that Support a Diverse Regional Economy and Employment Close to Where People Live

In order to meet Strategy 2.1, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

2.1.4(a)

As Surrey continues to grow and develop new residential areas, it is important that economic opportunities are supported and expanded within the city. Policies within Surrey’s OCP that support economic development in Urban Centres, Frequent Transit Development Areas, Industrial sites and Mixed Employment areas are illustrated in the Land Use and Density Section and in policies: E1.1, E1.3, E1.4, E1.5, E1.6, E1.15, E1.19, E1.21, E2.1, E2.2 and E2.3.

2.1.4(b)

OFFICE SPACE

Ensuring there is a healthy mix of employment within Surrey involves the provision of sufficient office space, particularly within Urban Centres. Policies within Surrey’s OCP supporting office uses include: A2.4, B1.9 and E1.19.

2.1.4(c)

MAJOR COMMERCIAL AND INSTITUTIONAL DEVELOPMENT

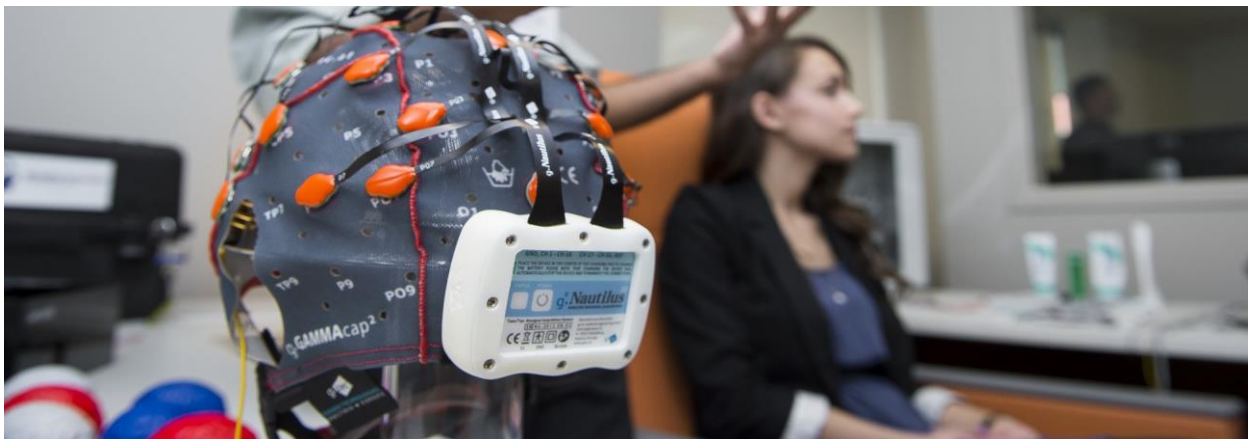
Surrey’s Urban Centres and Frequent Transit Development Areas are intended to accommodate major commercial and institutional development due to their proximity to public transit and public amenities. Policies within Surrey’s OCP supporting where these large scale uses locate include: A2.2, B1.1, B1.5, B1.6, B1.7, B1.8 and B1.9.

2.1.4(d)

SPECIAL EMPLOYMENT AREAS

Surrey does not have any Special Employment Areas but does recognize the importance of supporting institutional uses such as hospitals and post secondary education. Surrey’s OCP has several policies supporting the continued operations of these uses in the city including: B1.5, B1.6, B1.7, E2.9 and E2.10.

Innovation Boulevard business NeuroTech Lab; Surrey Memorial Hospital



2.2 Industrial Land

Protect the Supply of Industrial Land

In order to meet Strategy 2.2, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

INDUSTRIAL AREAS

The Regional Growth Strategy, (as amended) identifies Industrial areas within Surrey that will accommodate regional industrial growth well into the future. These are illustrated in the Land Use and Densities Section and Figure 51.

2.2.4(b)(i)

INDUSTRIAL PROTECTION

As populations increase within all areas of the Lower Mainland, pressures increase to accommodate residential construction on lands designated for commercial or industrial purposes. Surrey has a significant portion of the Region’s available industrial base and in order to ensure land exists in the future for well-paying employment opportunities, existing industrial land needs to be retained for future industrial development. Policies within Surrey’s OCP supporting the protection of industrial land include: E1.1, E1.2 and E1.11.

2.2.4(b)(ii)

INDUSTRIAL ACCESSORY USES

Limited non-industrial, accessory uses within industrial areas may be beneficial to the overall operation of an industrial business. Support for these accessory uses can be found within Surrey’s OCP in the Land Use and Densities Section and in policy: E1.12, E1.21 and E1.22.

2.2.4(b)(iii)

INDUSTRIAL LAND USE EXCLUSIONS

Industrial areas are often under pressure to accommodate non-industrial uses, including big-box retail, and residential or stand-alone offices, as initial land prices in industrial areas tend to be cheaper than areas designated for commercial or residential uses. Policies within Surrey’s OCP that discourage non-industrial uses in industrial areas include: E1.11, 1.12 and E1.13.

2.2.4(a)



Endurance Wind Power; Campbell Heights Industrial Park; Surrey

2.2 Industrial Land (cont.)

2.2.4(b)(iv)

INDUSTRIAL LAND INTENSIFICATION

Current industrial land uses tend to be developed with large footprint, low-density, single-storey buildings. To ensure sufficient industrial land well into the future, intensification of industrial sites is desirable. Policies within Surrey's OCP supporting increasing land intensification for industrial buildings include: E1.5, E1.6 and E1.7.

2.2.4(c)

MIXED EMPLOYMENT AREAS AND BOUNDARIES

The Figure 3, General Land Use Designations map of this OCP and Figure 51 identify Mixed Employment areas within Surrey that are consistent with the areas shown in the RGS.

2.2.4(d)(i)

MIXED USES IN MIXED EMPLOYMENT AREAS

Mixed Employment areas are intended to accommodate a variety of uses including industrial, commercial, office and other employment uses that are appropriate for industrial areas. Surrey's OCP illustrates these uses in the Land Uses and Densities Section and with policies: E1.21, E1.22 and E1.23.

2.2.4(d)(ii)

LARGE AND MEDIUM FORMAT RETAIL

Several types of retail businesses require large buildings for their stock or storage needs. These uses are retail in nature but require warehouse-type buildings and market to both consumers and businesses. These uses are not permitted in most Mixed Employment areas in Surrey except in select areas as permitted in approved Secondary Plans, as documented in the Land Use and Densities Section of this OCP and in policy: E1.23.

2.2.4(d)(iii)

MAJOR TRIP-GENERATING USES

The *Regional Growth Strategy* (as amended) supports locating major and/or regional trip-generating uses within Urban Centres or Frequent Transit Development Areas. Policies in Surrey's OCP supporting this objective include: A2.2, B1.5, B1.8, B1.9 and E1.19.

2.2.4(d)(iv)

MIXED EMPLOYMENT IN URBAN CENTRES AND FREQUENT TRANSIT DEVELOPMENT AREAS

There are several small instances of overlap between Mixed Employment uses and Urban Centres and Frequent Transit Development Areas in the OCP. In these areas, higher density employment uses, such as office retail, may be permitted as supported in the Land Use and Densities Section and policies: E1.19 and E1.23.

2.2.4(d)(v)

INFILL AND DENSITY INCREASES

Where current land use plans identify densities or where Mixed Employment areas are accessible to transit, density increases are permitted. Surrey's OCP supports these increases in the Land Use and Densities Section and in policies: E1.3 and E1.4.

2.2.4(d)(vi)

EXCLUDED RESIDENTIAL USES

Residential uses are not permitted within Mixed Employment areas. Policies within Surrey's OCP to support this restriction are identified within the Land Use and Density Section and in policy: E1.13.

2.2.4(e)

REDUCE ENVIRONMENTAL IMPACTS

Ensuring industrial and manufacturing industries include sustainable processes and energy efficiency into their operations is a key component businesses can achieve to help Surrey reach its sustainability goals. For those areas within Surrey's jurisdiction, the OCP identifies policies supporting reducing environmental impacts including: E1.31, E1.32 and E1.33.

2.3 Agricultural Land

Protect the Supply of Agricultural Land and Promote Agricultural Viability with an Emphasis on Food Production

In order to meet Strategy 2.3, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

AGRICULTURAL AREAS AND BOUNDARIES

The *Regional Growth Strategy* (as amended) identifies Agricultural areas in the Agricultural Land Reserve in Surrey that are required to accommodate food production well into the future. This OCP designates Agricultural areas that encompass the RGS Agricultural designation as well as additional areas outside of the ALR. This is shown in the Land Use and Density Section and in Figures 3 and 43.

2.3.6(b)(1)

REGIONAL AGRICULTURAL VIABILITY

Surrey has designated over one third of its land for agricultural uses. Continued protection of this land from development encroachment and from non-farm uses is important for the future longevity of the industry. Policies within Surrey's OCP supporting agricultural land include: E3.1, E3.2, E3.7, E3.10 and E3.24.

2.3.6(b)(ii)

AGRICULTURAL LAND SUBDIVISION

The City of Surrey and its Agricultural and Food Security Advisory Committee discourage the fragmentation of agricultural land through subdivision. Policies within Surrey's OCP discouraging farm fragmentation include: E3.5 and E3.7.

2.3.6(b)(iii)

AGRICULTURAL INFRASTRUCTURE SUPPORT

The success of Surrey's agricultural industry is not only related to having available land for production, it also requires having infrastructure that supports farm operations in terms of adequate transportation routes, drainage and access to water. Policies within Surrey's OCP supporting agricultural infrastructure systems include: E3.19, E3.20, E3.21, E3.22 and E3.23.

2.3.6(a)



Agricultural farm workers in Surrey.

2.3 Agricultural Land (cont.)

2.3.6(b)(iv) AGRICULTURAL INTERFACE AREAS

Surrey has many areas where urban uses and agricultural land physically connect. As a means to protect agricultural uses from adjacent urban development, Surrey supports ensuring that this interface is appropriately managed. Policies within Surrey’s OCP supporting ag-urban interface management include: E1.9, E3.24, E3.25, E3.26, E3.27, E3.28, E3.29, and E3.30 as well as a Development Permit Area and Guidelines specific to the ag-urban interface.

2.3.6(b)(v) AGRICULTURAL ECONOMIC DEVELOPMENT

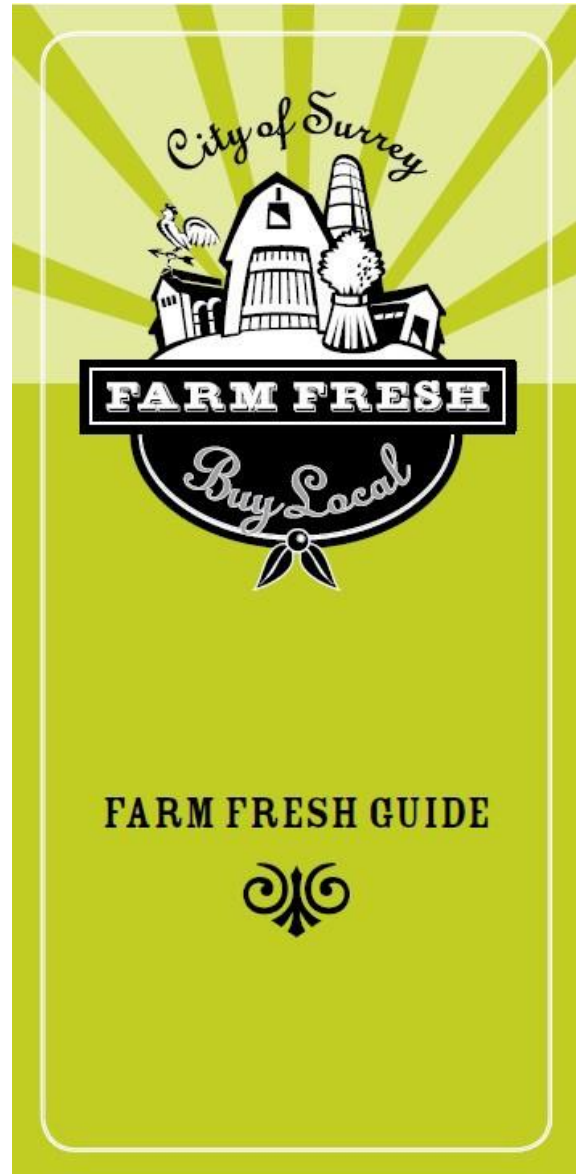
Surrey recognizes the value agricultural activities bring to the economic well-being of the City. Ensuring the agricultural industry is supported is an important goal identified within Surrey’s *Agricultural Plan* and *Agricultural Protection and Enhancement Strategy* (as amended). Policies within Surrey’s OCP supporting agricultural economic development include: E3.8, E3.9, E3.10, E3.11 and E3.15.

2.3.6(b)(vi) AGRICULTURE AND FOOD PRODUCTION

Surrey has established local food production as a priority within its *Agricultural Protection and Enhancement Strategy* (as amended). The definition and regulation of permitted agricultural uses within the ALR is the responsibility of the Agricultural Land Commission. The City of Surrey has limited ability to prioritize food production over other permitted agricultural uses such as ornamental horticulture or equestrian uses. Ensuring Surrey’s agricultural land is available and able to accommodate food production is an important aspect in maintaining a healthy agricultural industry in Surrey as well as establishing an important connection between food produced for, and consumed by, Surrey residents. Policies within Surrey’s OCP encouraging increased food production on agricultural land include: F5.1, F5.3, F5.4 and F5.5.

2.3.6(b)(vii) AGRICULTURAL EDUCATION

Surrey has long supported using educational programs to connect the public with the agricultural industry. Policies within Surrey’s OCP illustrating the continued commitment include: E3.17 and E3.18.



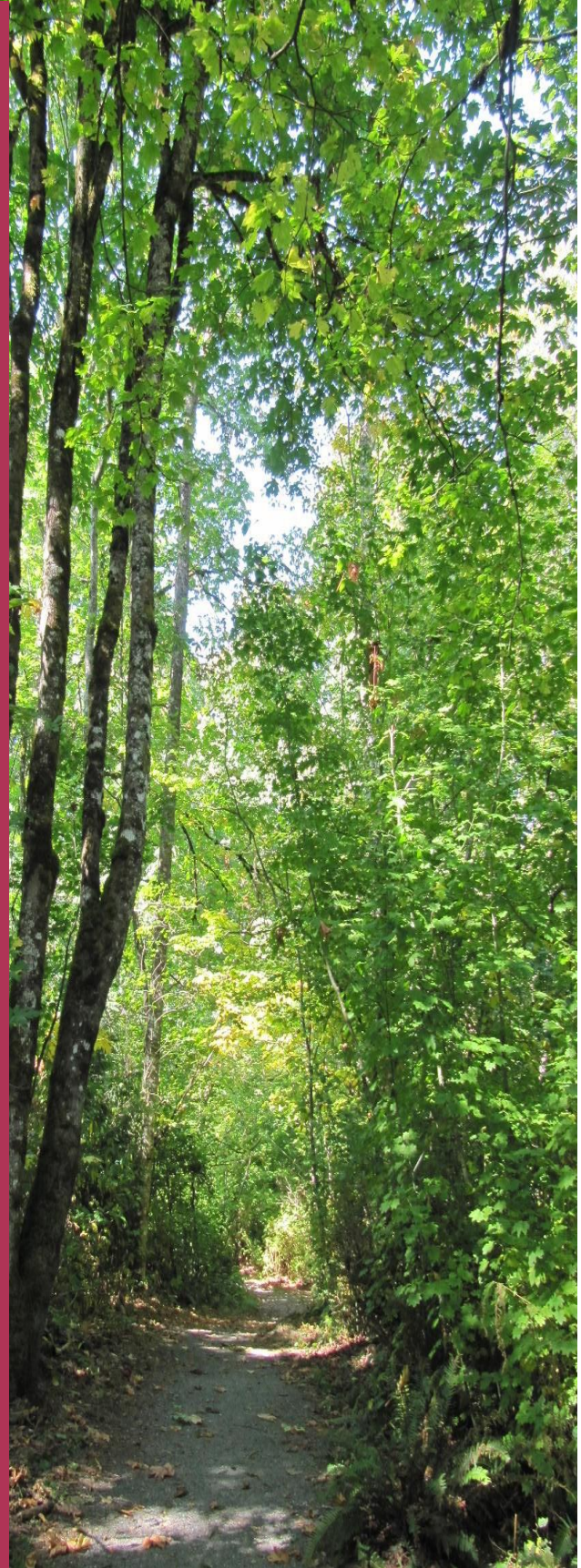
Surrey's Farm Fresh Guide lists local farms and details the type of products grown and sold at each.

GOAL 3: PROTECT THE ENVIRONMENT AND RESPOND TO CLIMATE CHANGE IMPACTS

The third goal of Metro Vancouver's *Regional Growth Strategy* is achieved by recognizing the natural environment's importance in supporting a livable and sustainable region that boasts a very distinct and recognizable Sense of Place. This goal also addresses the importance of accommodating climate change and adaptation and mitigation into development policies.

Strategies

- 3.1 Protect Conservation and Recreation lands.
- 3.2 Protect and enhance natural features and their connectivity.
- 3.3 Encourage land use and transportation Infrastructure that reduce energy consumption and greenhouse gas emissions and improve air quality.
- 3.4 Encourage land use and transportation infrastructure that improve the ability to withstand climate change impacts and natural hazard risks.



3.1 Conservation and Recreation Land

Protect Conservation and Recreation Lands

In order to meet Strategy 3.1, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

3.1.4(a) CONSERVATION AND RECREATION AREA BOUNDARIES

The *Regional Growth Strategy* (as amended) identifies Conservation and Recreation Areas that are required to be identified within Surrey on a municipal level. These are illustrated in the Land Use and Densities Section and on Figure 3.

3.1.4(b)(i) INFRASTRUCTURE SUPPORT AND PROTECTION

Metro Vancouver watersheds that supply Surrey's drinking water are not located within Surrey, therefore protecting lands for this purpose is not part of Surrey's Official Community Plan.

3.1.4(b)(ii) ENVIRONMENTAL CONSERVATION AND PROTECTION

The City of Surrey has identified many goals for achieving environmental conservation and protection which are established in Surrey's *Sustainability Charter* (as amended). The City also supports management of the natural environment by preparing Integrated Stormwater Management Plans (ISMP) for each of Surrey's watershed catchment areas. These goals are illustrated in Surrey's OCP and include: C1.9, C1.16, D1.1, D1.2 and D1.7.

3.1.4(b)(iii) RECREATION AND OUTDOOR SPACE

Surrey is well known for its provision of extensive outdoor recreation opportunities. Continuing to support and expand on large, regionally significant outdoor recreation parks (e.g. coordinating the completion of the Regional Recreation Greenway Network) is a goal of the City and is supported through OCP policies that include: C3.6, C3.9, C3.13 and C3.20.



Elgin Park, Surrey

3.1 Conservation and Recreation Land (cont.)

3.1.4(b)(iv) CONSERVATION AND RECREATION EDUCATION, RESEARCH AND TRAINING

Surrey supports City residents having ready access to programs and facilities that provide education, research, or training in conservation or recreation-related issues. Policies in Surrey’s OCP supporting these programs include: D1.13 and D1.14.

3.1.4(b)(v) CONSERVATION AND RECREATION USES AND AMENITIES

The lands identified as Conservation and Recreation in Surrey (see Figure 3) are not generally available for commercial recreation uses. Where such uses and amenities are considered, physical facilities and infrastructure are limited to parks, consistent with policy: C3.21.

3.1.4(b)(vi) CONSERVATION, RECREATION AND AGRICULTURAL USES

The Conservation and Recreation lands identified in Figure 3 are not generally suitable for agricultural uses, with the exception of portions of Tynehead Regional Park, under the management of Metro Vancouver. The Conservation/Recreation designation in the Land Use and Densities Section does not preclude agriculture as a use provided it does not compromise the primary objective of ecological protection.

3.1.4(c) CONSERVATION AND RECREATION BUFFER AREAS

In certain instances, it is necessary to ensure there is an appropriate natural or landscaped buffer between areas designated for conservation and recreation from other adjacent uses. Policies within Surrey’s OCP supporting this buffer include: D1.2, D1.6, D1.7 and D1.17.



Urban-Agriculture Vegetated Buffer with recreation path.

3.4 Natural Features

Protect and Enhance Natural Features and Their Connectivity

In order to meet Strategy 3.2, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

3.2.4

ECOLOGICAL AREAS MANAGEMENT

The City of Surrey is completing the preparation of a (draft) *Biodiversity Conservation Strategy* detailing Surrey’s sensitive ecosystems and how they are to best be managed in a growing and rapidly changing city. Policy within Surrey’s OCP supporting the protection of sensitive ecosystems include: D1.1, D1.3, D1.4, D1.6 and D1.7 as well as Development Permit Area Guidelines established for Hazard Lands and Sensitive Ecosystems.

3.2.5

REGIONAL AND GREENWAY DEVELOPMENT AND MANAGEMENT

Surrey works collaboratively with other levels of government to maintain parkland and greenways; specifically, Surrey is working on completing its portion of the Experience the Fraser Initiative, Regional Recreation Greenway Network (*Regional Growth Strategy—Map 9*). Policies in Surrey’s OCP supporting this work include: C3.26, C3.27, C3.28 and C3.29.

3.2.6

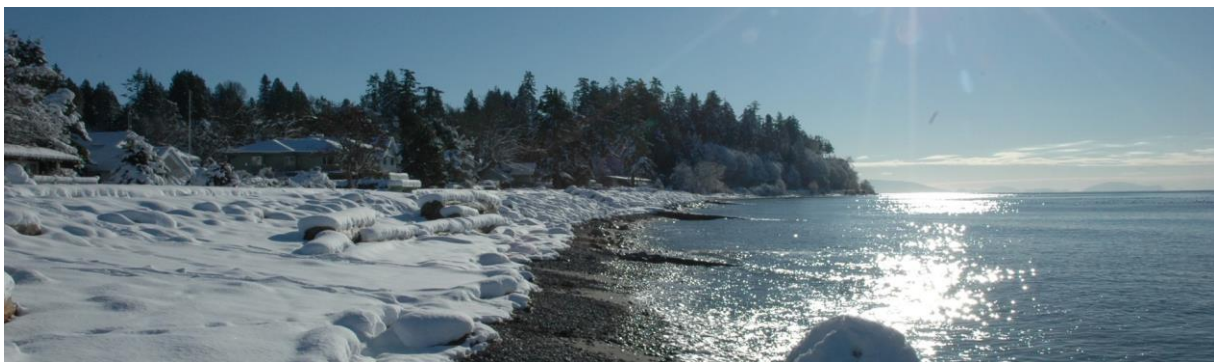
ECOLOGICAL PROTECTION, ENHANCEMENT AND RESTORATION

Surrey has many ecologically-important areas including Sensitive Fisheries Zones and the estuaries of Mud, Boundary and Semiahmoo Bays. Protecting, enhancing and restoring these sensitive ecosystems is a priority for Surrey and is reflected in OCP policies that include: D1.1, D1.2, D1.3, D1.7, D1.10, D1.12, D3.1 and D3.15.

3.2.7

WATERSHED MANAGEMENT

Surrey supports the protection of its watersheds and ecosystems. Creating a Development Permit Area for Sensitive Ecosystems and requiring the preparation of Integrated Stormwater Management Plans for development are ways in which Surrey works to protect watersheds. This is illustrated in Surrey’s OCP through policy that includes: C1.8, C1.9, C1.12, D3.6, D3.7 and D3.8.



Crescent Beach

3.3 Energy and Emissions

Encourage Land Use and Transportation Infrastructure that Reduce Energy Consumption and Greenhouse Gas Emissions, and Improve Air Quality

In order to meet Strategy 3.3, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

3.3.4(a)

GREENHOUSE GAS REDUCTION TARGETS

With Surrey's rapidly growing population, reducing per capita greenhouse gas emissions is a challenging task. Surrey has identified several ways in which GHGs will be reduced as development proceeds. Policies in Surrey's OCP that support GHG reduction (to below 2007 levels by 33% by 2020 and by 80% by 2050) include: D4.1, D4.2, D4.4, D4.13 and D4.14.

3.3.4(b)

ENERGY CONSUMPTION REDUCTIONS

Surrey is fully committed to developing affordable energy options for its residents well into the future, including supporting the development of its first District Energy system. Policies within Surrey's OCP and within Surrey's *Community Energy and Emission Plan* (CEEP) (adopted November 2013) supporting changes in energy use include: D4.6, D4.8, D4.9, D4.17 and D4.21.

3.3.4(c)

URBAN CENTRE AND FREQUENT TRANSIT DEVELOPMENT AREA INFRASTRUCTURE

Surrey's OCP focuses future high-density development within Urban Centres and in Frequent Transit Development Areas. Guidelines for development that provide for greater pedestrian movements and improved amenity spaces are identified with this OCP's DP1 Form and Character Development Permit Area. Additional policies supporting this type of development include: A1.3, A2.1, A2.4, A2.6, B1.3, B1.22, B1.28, B3.1 and B3.4.

3.3.4(d)

STORMWATER MANAGEMENT AND WATER CONSERVATION

Surrey is committed to developing in a sustainable manner including the use of natural drainage systems and water conservation in development. Policies in Surrey's OCP supporting this type of development include: C1.5, C1.8, C1.9, C1.11 and C1.12.



Surrey supports the use of alternative transportation modes including Cycling, Bus Service, SkyTrain and Light Rapid Transit.

3.4 Climate Adaptation and Hazards

Encourage Land Use and Transportation Infrastructure that Improve the Ability to Withstand Climate Change Impacts and Natural Hazard Risks

In order to meet Strategy 3.4, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

3.4.4 CLIMATE CHANGE AND SETTLEMENT PATTERNS

Surrey has many areas with development potential that also fall within potentially hazardous areas such as steep slopes or floodplains. Surrey is proposing to regulate development in these areas with policies that include: D2.1, D2.4, D2.5, D2.6, D2.8, D3.2 and D4.21 and with DP2 Development Permit Area for Hazard Lands in the Implementation Section of this OCP.

3.4.5 MUNICIPAL UTILITIES AND CLIMATE CHANGE

The City of Surrey is responsible for the installation and maintenance of City-owned municipal infrastructure systems. Ensuring these assets are managed to reduce exposure to natural hazards and the effects of climate change is supported through Surrey's OCP policies including: C1.12, C1.14, C1.16, C1.20, C1.21, D4.21 and D4.23.

High water protection efforts along the Nicomekl River, Surrey.



GOAL 4: DEVELOP COMPLETE COMMUNITIES

The fourth goal of Metro Vancouver's *Regional Growth Strategy* is achieved by establishing the basis for walkable, mixed-use, transit-oriented communities to thrive and support the variety of residents found within Surrey. This goal focuses on providing appropriate, affordable housing is key as well as designing neighbourhoods and urban areas so they are physically and socially accessible to all ages.

Strategies

- 4.1 Provide diverse and affordable housing choices.
- 4.2 Develop healthy and complete communities with access to a range of services and amenities.



4.1 Housing

Provide Diverse and Affordable Housing Choices

In order to meet Strategy 4.1, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

4.1.7(a)(i) HOUSING OPTIONS AND SUPPORT

Surrey is a fast-growing city that is required to ensure adequate housing is available for its residents. Just as there are many different types of residents so are there many different types of housing needs. Requirements for housing are also detailed in separate Secondary Plans or are specialized and specific to one housing issue. Policies within Surrey's OCP that detail how Surrey is supporting diverse housing options include: B4.5, F3.2, F3.6 and F3.7.

4.1.7(a)(ii) HOUSING STOCK SUPPLY AND DIVERSITY

Surrey is expected to accommodate a significant growth in population over the life of this OCP and well into the future. In order to do that efficiently, Surrey must ensure housing densities and designs are appropriate to adequately supply residents with places to live. Policies within Surrey's OCP supporting increasing densities and developing appropriate styles of residential buildings include: A2.1, A4.1, B1.36, B1.37, B1.38, B4.5, B4.6, F3.12, F3.14 and F3.22.

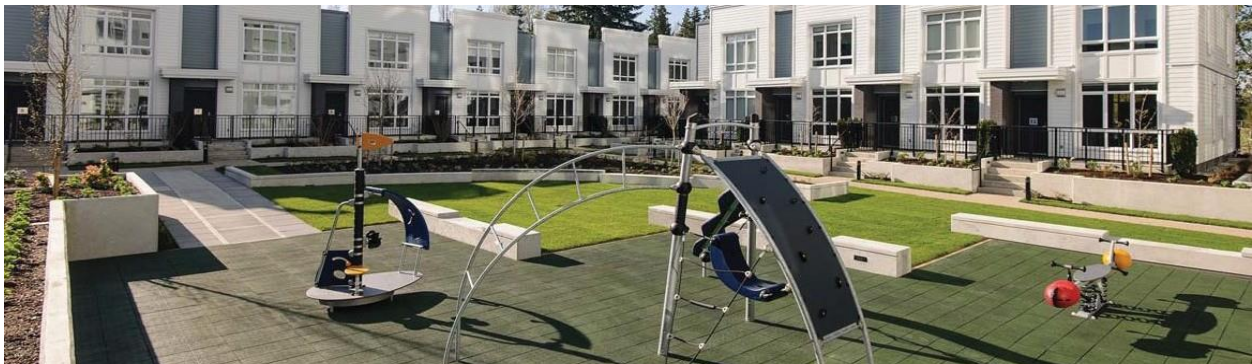
4.1.7(a)(iii) AFFORDABLE RENTAL UNIT SUPPLY

Surrey has successfully worked with higher levels of government to secure affordable rental units for low income residents. Continuing to foster these relationships to take advantage of government funding to support the construction of additional rental units is supported in Surrey's OCP through policies such as: B3.4, F3.12, F3.13, F3.14, F3.15, F3.16, F3.17 and F3.18.

4.1.7(a)(iv) AFFORDABLE HOUSING INCENTIVE MEASURES

The City of Surrey can support the provision of affordable housing in the City by adjusting Zoning requirements and by prioritizing the processing of development applications. Policies in Surrey's OCP supporting incentive measures include: B1.35, B1.37, F3.1, F3.9, F3.14, F3.15, F3.18 and F3.19.

Ultra development's housing variety and amenity space; Surrey City Centre.

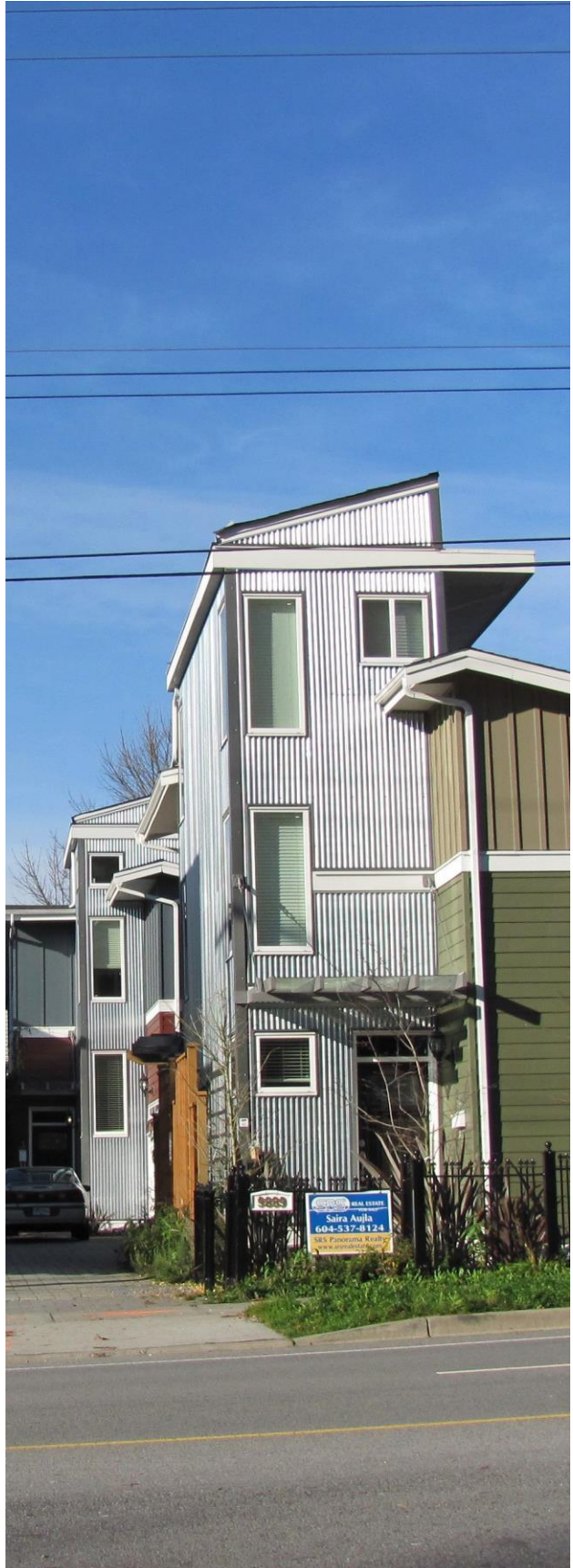


4.1 Housing (cont.)

4.1.8(a)-(f)

Surrey is in the process of preparing a Housing Action Plan. This Plan is intended to assess local housing market conditions, to identify housing priorities and to develop implementation measures including policies, financing mechanisms and partnerships. Policies in Surrey's OCP supporting this process include:

- a) Housing Market Condition Assessment (F3.1)
- b) Housing Priorities (F3.1 and F3.21)
- c) Housing Implementation Measures (F3.1 and F3.19)
- d) New Rental Housing Development (F3.1, F3.14 and F3.15)
- e) Housing Provided Through Government Partnerships (F3.1, F3.2 and F3.6)
- f) Metro Vancouver Housing Corporation (F3.1 and F3.2)



Maple Court infill development, Surrey.

4.2 Services and Amenities

Develop Healthy and Complete Communities with Access to a Range of Services and Amenities

In order to meet Strategy 4.2, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

4.2.4(a)

COMPACT URBAN SPACES

Surrey supports the creation of compact urban spaces that offer safe and convenient opportunities for active transportation. Policies within Surrey's OCP supporting compact, walkable urban developments include: A4.1 and F4.2.

4.2.4(b)

CULTURE, HEALTH AND AFFORDABILITY

Surrey is committed to ensuring its City Centre and Town Centres are the heart of cultural, educational or health services and amenities. Policies in Surrey's OCP supporting these objectives include: B1.5, B2.1, B2.4, B2.8 and B2.9.

4.2.4(c)

COMMUNITY ENGAGEMENT

Providing spaces for communities to come together to share in cultural events or regular social interaction is a key component to creating healthy neighbourhoods. Policies in Surrey's OCP supporting creating public spaces for social interaction include: B1.13, B1.15, B2.17, B4.4, B4.19, B4.20, B6.7, B6.9, F7.4 and F7.7.

4.2.4(d)

ACTIVE LIVING

Surrey is committed to providing many opportunities for its residents to lead healthy active lives. Providing opportunities to access parks, trails and recreation facilities is a key component in that goal. Policies within Surrey's OCP supporting active living include: C3.12, C3.14, C3.26, F4.2 and F4.3.

4.2.4(e)

URBAN AGRICULTURE

Increasingly communities and those residents living in urban areas are wanting to access healthy, local food. Supporting urban agriculture and community gardens is a small step in improving access to local food. Policies in Surrey's OCP supporting urban agriculture include: F5.1, F5.4 and F5.6.

4.2 Services and Amenities (cont.)

4.2.4(f)

HEALTHY URBAN ENVIRONMENTS

Surrey is increasingly considering the health impacts of development in terms of its location and access to active transportation options or if physical design can reduce some of the noise and air quality issues that can be associated with urban environments. Policies within Surrey's OCP supporting the development of healthy urban environments include: B4.1, B4.6, B6.2, B6.20, B6.22, F4.2 and F4.3.

4.2.4(g)

UNIVERSAL DESIGN

Communities are comprised of a multitude of people that have different mobility or sensory abilities. Accommodating access for all user groups is a key element in good community design. Policies in Surrey's OCP supporting universal design in development include: F6.4, F6.5, F6.6, F6.7 and F6.8.

4.2.4(h)

SMALL SCALE CENTRES

Surrey supports small scale Local Centres that do not compete with Urban Centres but that serve local neighbourhood needs and provide housing and commercial opportunities with available public transit. Policies within Surrey's OCP supporting small scale centres include: B4.3, B4.11 and B4.12.

4.2.4(i)

SPECIAL EMPLOYMENT AREAS

Surrey does not have any Special Employment Areas as illustrated in the *Regional Growth Strategy*.



Bear Creek Playground, Surrey.

GOAL 5: SUPPORT SUSTAINABLE TRANSPORTATION CHOICES

The fifth goal of Metro Vancouver's *Regional Growth Strategy* is achieved by adjusting land uses and the physical development of space to influence travel patterns and transportation systems. Having accessible, affordable and abundant public transportation infrastructure is paramount to supporting pedestrian-oriented site development and urban areas. The identification of Frequent Transit Development Areas is an important step in Surrey's move to ensuring adequate transportation options are provided for its residents and visitors.

Strategies

- 5.1 Coordinate land use and transportation to encourage transit, multi-occupancy vehicles, cycling and walking.
- 5.2 Coordinate land use and transportation to support the safe and efficient movement of vehicles for passengers, goods and services.



5.1 Sustainable Transportation

Coordinate Land Use and Transportation to Encourage Transit, Multi-Occupancy Vehicles, Cycling and Walking

In order to meet Strategy 5.1, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

TRANSPORTATION LAND USES AND POLICIES

Surrey supports transportation decisions in accordance with Surrey's *Transportation Strategic Plan* (as amended). That Plan identifies the priorities for transportation that include taking into account the needs of users, in order of priority: pedestrians, transit, cycling, commercial traffic and trucks, high occupancy vehicles and single occupancy vehicles. Policies within Surrey's OCP supporting sustainable transportation include: B1.16, B1.18, C2.1—C2.6, C2.20—C2.25, C2.29—C2.33 and C2.40—C2.46.

5.1.6(b) TRANSPORTATION DEMAND MANAGEMENT STRATEGIES

Surrey supports the use of Transportation Demand Management (TDM) in its development decision making processes. Policies within Surrey's OCP supporting the use of TDM include: C2.40, C2.41, C2.42 and C2.43.

5.1.6(c) ALTERNATIVE TRANSPORTATION MODES

Supporting the use of transit and active transportation are key elements in Surrey's efforts in creating healthy communities and neighbourhoods. Policies within Surrey's OCP supporting alternative transportation modes include: B1.16, B1.17, C2.5, C2.6, C2.23 and C2.24.

5.1.6(a)



Rendering of Light Rail Transit train car for Surrey.

5.2 Safe and Efficient Movement

Coordinate Land Use and Transportation to Support the Safe and Efficient Movement of Vehicles for Passengers, Goods and Services

In order to meet Strategy 5.2, the City will fulfill its role as a municipality in Metro Vancouver by supporting the *Regional Growth Strategy* in the following manner:

5.2.3(a) GOODS MOVEMENT

Surrey provides key linkages for goods movement within the Metro Vancouver region, as shown in Figure 27 (RCS Figure 62) and Figure 30 (RCS Figure 61). Ensuring goods can conveniently access highway, railways, ports and the Canada/US border are key elements in ensuring the Region's economy continues to operate efficiently and effectively. Policies in Surrey's OCP supporting local and regional goods movement include: C2.34, C2.35, C2.36, E1.24, E1.27 and E1.30.

5.2.3(b) EFFICIENT VEHICLE MOVEMENT

Ensuring vehicles move throughout Surrey efficiently is a key priority in the management of municipal transportation systems. Policies within Surrey's OCP supporting land use and transportation decisions include: C2.1, C2.8, C2.9, C2.10, C2.11, C2.12 and C2.14.

5.2.3(c) LOCAL AND REGIONAL TRANSPORTATION SYSTEMS

Surrey's local roads are part of a larger regional transportation system. Optimizing existing capacity and managing the traffic that uses these roads is important for Surrey's overall development. Policies in Surrey's OCP supporting the City's role in optimizing existing capacity in the overall transportation system (see Figure 27 (RCS Figure 62) and Figure 30 (RCS Figure 61)) include: C2.2, C2.8, C2.12, E1.24, E1.26 and E1.27.

5.2.3(d) RAILWAY AND WATERWAY PROTECTION

Railway and waterway protection within Surrey (see Figure 30) is a vital component of the local and regional economies of this area. Policies within Surrey's OCP supporting sustainable railway and waterway uses include: C2.26, C2.27, C2.28, C2.34, C2.39,



Scott Road transportation corridor, Surrey.

Figure 61: RCS: Goods Movement

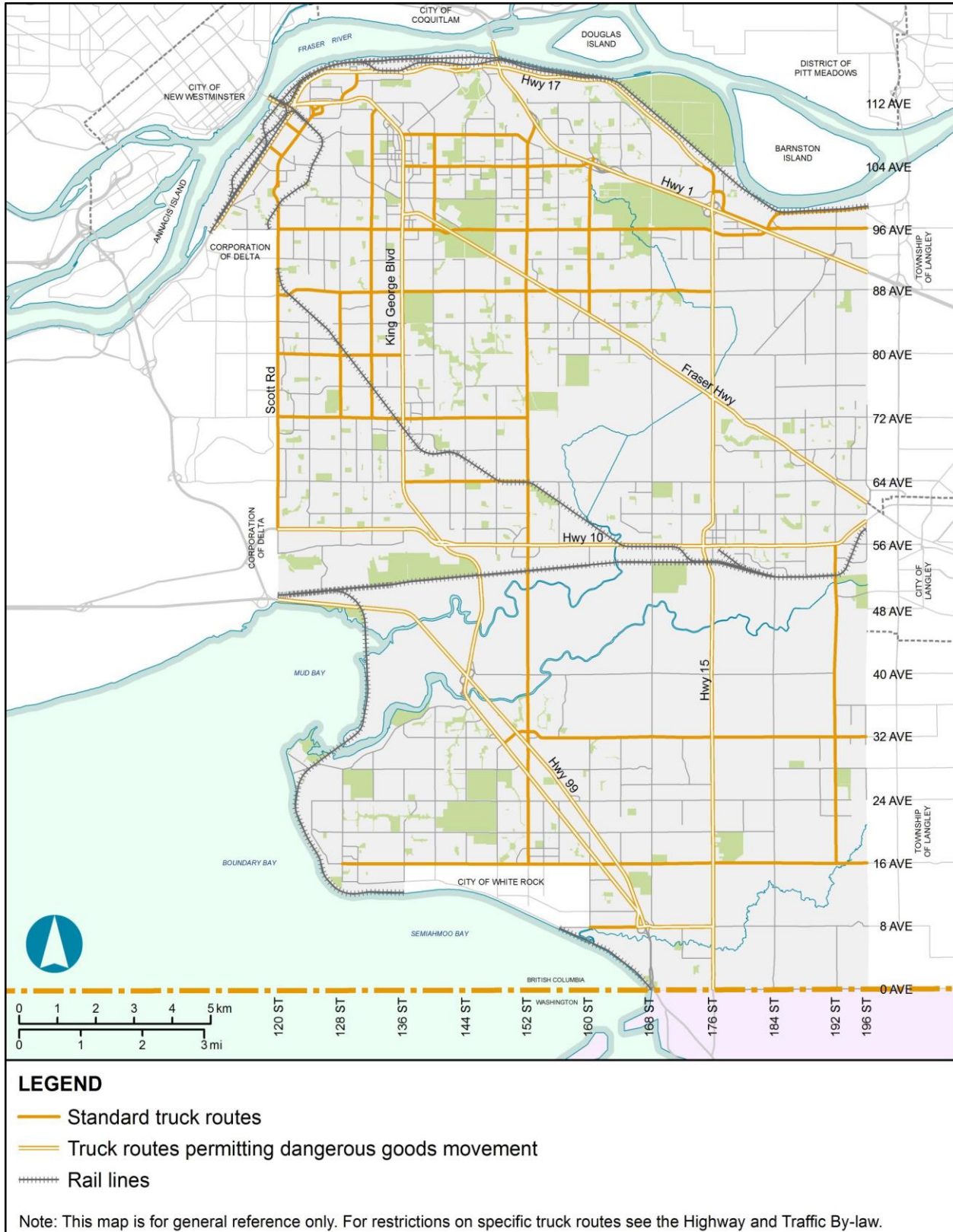
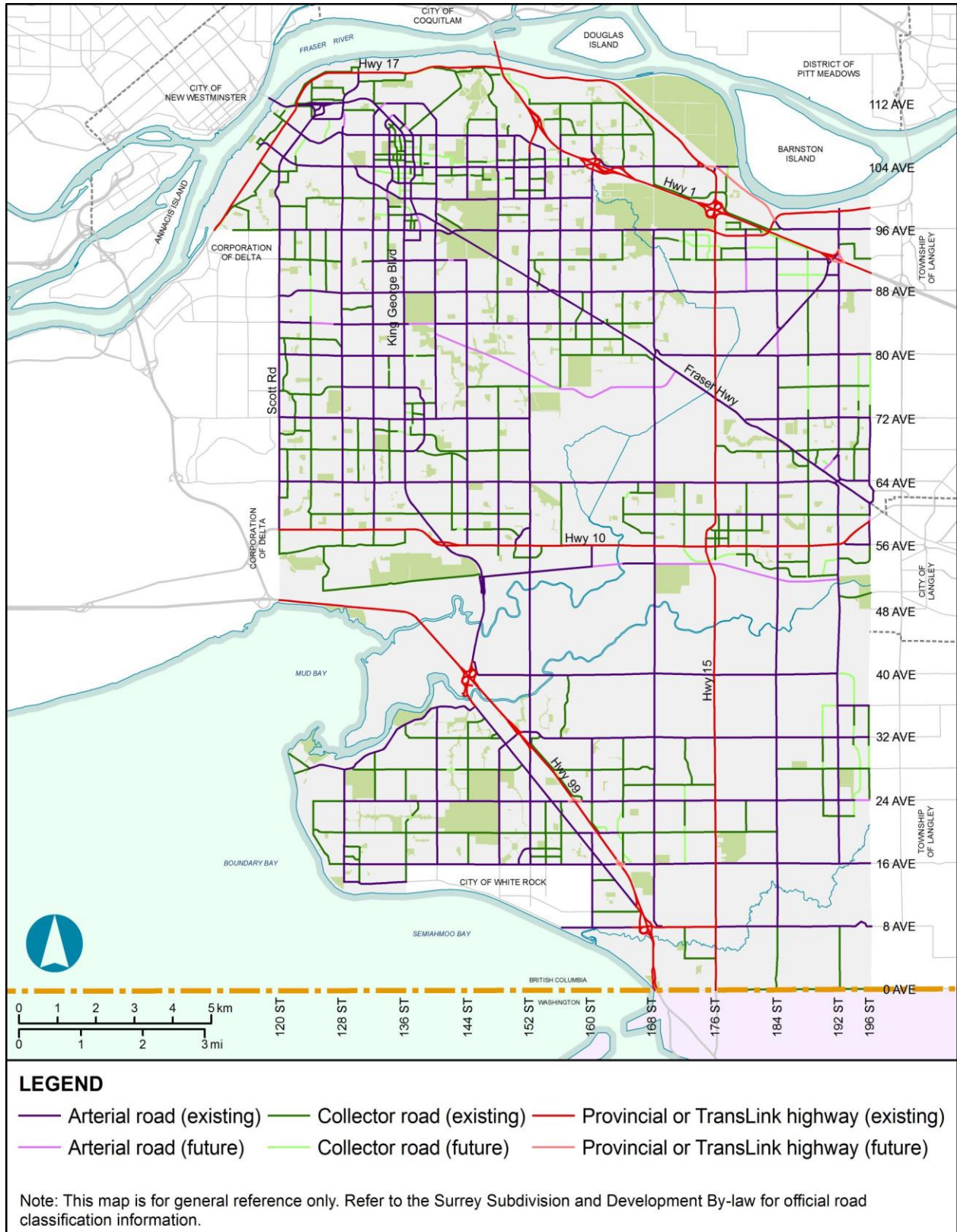


Figure 62: RCS: Major Road Classifications



I. OCP IMPLEMENTATION

Overview

The policies in this Official Community Plan are implemented through Development Approval Information Areas, Secondary Plans, Council Policies, regulatory By-laws, Development Permits, Temporary Use Permits, and Master Plans and Strategies. This section sets out the relationship between the OCP and these implementation instruments, sets out the process for monitoring the implementation of the policies found within this document and identifies under what conditions the OCP can be amended.

Consistency with By-laws, Plans and Policies

The *Local Government Act* (as amended) states that all By-laws enacted and works undertaken by Council shall be consistent with the provisions of an Official Community Plan. This applies to By-laws such as Surrey's Zoning-By-law, Subdivision and Development By-law and Development Cost Charge By-law. This principle also extends to other plans, strategies and programs such as Secondary Plans, Surrey's *Transportation Strategic Plan*, *Parks, Recreation and Culture Master Plan*, *Economic Development Strategic Plan* and Surrey's *Ten-Year Servicing Plan* (as amended). The concepts, policies and operational measures in these plans must be consistent with the community planning, land use and urban design policies in Surrey's Official Community Plan.

Zoning designations applied to a lot must also be consistent with an OCP Land Use designation. Upon adoption of an OCP, if existing zoning designations become inconsistent with the Land Use designation, the existing zoning may continue; however, any redevelopment or rezoning after the OCP is adopted must be consistent with the applicable Land Use designation.

The *Local Government Act* states that a municipality is not obligated or compelled to carry out any of the projects specified within an OCP. For example, a City may postpone utility installation extensions to a particular part of the city because growth may be viewed as premature, not in accordance with the desired development pattern, out of sequence with existing areas undergoing urbanization, or because it is not economically feasible.

II(a). IMPLEMENTATION INSTRUMENTS

Secondary Plans Overview

The Official Community Plan sets out broad policy objectives and general land use designations to guide the growth and development of the City. Secondary Plans provide more detailed guidance on land uses, densities, road networks and other development considerations for specified areas. Secondary Plans comprise several different types, including:

- Neighbourhood Concept Plans (NCP) - for the development of new urban neighbourhoods
- Local Area Plans (LAP) - for the development of new employment areas
- Infill Plans - for redevelopment and infill within established urban neighbourhoods
- Urban Centre Plans - for development within Surrey's City Centre or Town Centres.

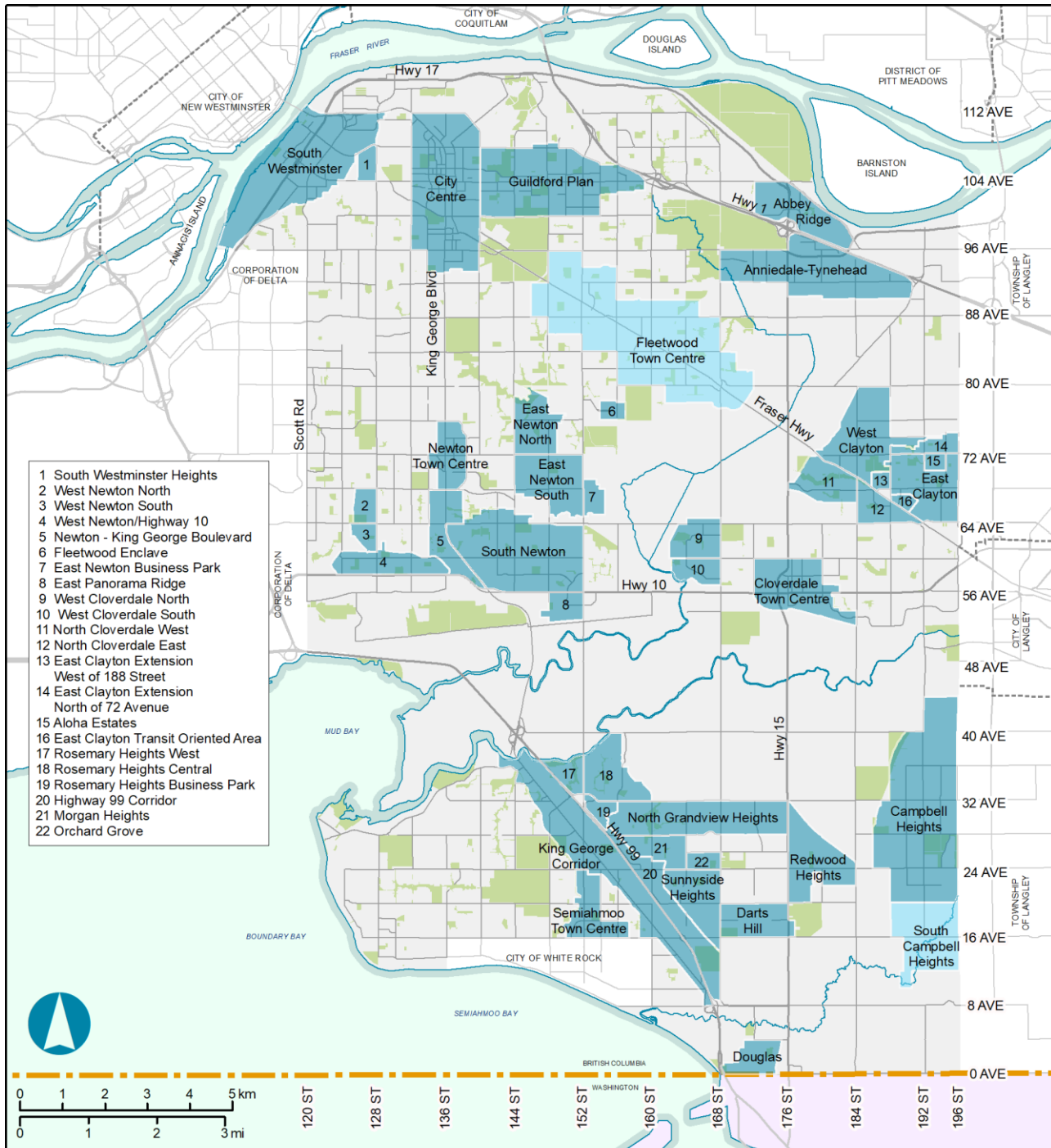
Secondary Plans are typically prepared and approved as Stage 1 and Stage 2 Plans.

Stage 1 Plans: typically include land uses and densities, transportation networks, those areas identified and required for public purposes including parks and schools, and those areas required for drainage systems such as detention ponds.

Stage 2 Plans: typically include the engineering services and utility works required to support the land use plan, detailed design guidelines and development financing strategies, including Community Amenity Contributions.

Figure 63 shows the areas in which Secondary Plans have been approved by City Council at either a Stage 1 or Stage 2 level.

Figure 63: Secondary Plan Areas



- 1 South Westminster Heights
- 2 West Newton North
- 3 West Newton South
- 4 West Newton/Highway 10
- 5 Newton - King George Boulevard
- 6 Fleetwood Enclave
- 7 East Newton Business Park
- 8 East Panorama Ridge
- 9 West Cloverdale North
- 10 West Cloverdale South
- 11 North Cloverdale West
- 12 North Cloverdale East
- 13 East Clayton Extension West of 188 Street
- 14 East Clayton Extension North of 72 Avenue
- 15 Aloha Estates
- 16 East Clayton Transit Oriented Area
- 17 Rosemary Heights West
- 18 Rosemary Heights Central
- 19 Rosemary Heights Business Park
- 20 Highway 99 Corridor
- 21 Morgan Heights
- 22 Orchard Grove

- BL 18423
- BL 18498
- BL 18787
- BL 19996
- BL 20340
- BL 20292
- BL 20554
- BL 20393
- BL 20658
- BL 20340
- BL 20658
- BL 20935
- BL 21086
- BL 21104
- BL 21183

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LEGEND

- Approved secondary plans
- Secondary plans with "stage 1 approval"*

* Approval of land use concept.

SECONDARY PLANS (cont.)

BL 18423

Secondary Plan Initiation

Secondary Plans are initiated by a resolution of Council for areas where it is determined that there is a need to review and update land use policies, provide for transit-supportive densities, revitalize and provide economic development support to areas that need new direction, or develop new urban neighbourhoods. For new neighbourhoods, Council may authorize the preparation of a Neighbourhood Concept Plan where a demonstrated level of support is evident from landowners within a defined area. This level of support is generally defined as 50% or more of landowners or owners of 70% or more of the land within the proposed plan area.

Secondary Plan Preparation

Secondary Plans are developed through an extensive and inclusive community and stakeholder consultation process. In addition to public meetings and open houses for the general public, the development of Secondary Plans includes input from Citizen Advisory Committees consisting of local land owners, residents, business representatives and interested parties from outside of the immediate study area.

Secondary Plans respond to the specific area context and generally consist of maps, statistics and policies setting out the following components:

- A land use concept or vision for the area, including maps and policy statements
- The distribution of land uses and densities, protected population and employment, and the location and type of services and amenities
- The recommended locations of schools, parks, walkways and greenways
- Urban design and place-making guidelines tailored to the plan area, including guidelines for transitions between the plan area and adjacent neighbourhoods or agricultural areas
- A transportation plan including road layouts and standards, transit services, parking strategies and facilities for pedestrians and cyclists
- A stormwater management plan and the design of water and sanitary sewer systems
- An environmental analysis, tree retention plan and a framework for the protection of significant environmental features
- A heritage assessment and recommendations for incorporation of heritage features into the plan
- A financial analysis and comprehensive strategy for the implementation and phasing of the plan, including the calculation of amenity fees
- Any other studies deemed necessary as a result of the context or special features of the secondary plan area or its impact on adjacent areas.

SECONDARY PLANS (cont.)

BL 18423

Secondary Plan Approval

Secondary Plans are approved by Council resolution. As Secondary Plans are approved by City Council, the more general Official Community Plan Land Use Designations are amended, as needed, to be consistent with the more detailed Secondary Plan. Development applications that conform to a Secondary Plan may be received and processed following Stage 1 approval but may not be completed until Stage 2 approval has been given.

Secondary Plan Amendments

Amendments to Secondary Plans may be classified as either Major Secondary Plan Amendments or Minor Secondary Plan Amendments.

Major Secondary Plan Amendments are requests to change the land use designations, density or major road patterns or policy direction in an approved Secondary Plan. Any application for a major amendment shall include a quantitative and qualitative analysis of the proposed change, including the:

- * Planning and/or engineering rationale for the proposed amendment
- * Impact of the proposed amendment on all aspects of the Secondary Plan including land use, population, densities, impact on schools, parks and amenities, services and utilities, roads and transportation, environmental considerations, funding of services and local market/economic implications
- * Impacts of the amendment in the context of the Official Community Plan
- * Precedent which may be set by the amendment and potential for additional requests for change to the Secondary Plan area
- * Public consultation to be used (which may include former Secondary Plan advisory committee members).

Major amendments require Council review and endorsement.

Minor Secondary Plan Amendments include changes such as proposed relocation of a local road not affecting the land use for overall density or minor changes to services that do not have a significant impact on land use or densities and that will not affect the serviceability or yield of properties. Any application for a minor amendment shall include a technical analysis of the proposed change, including the:

- * Planning and/or engineering rationale for the proposed amendment
- * Impacts on the Secondary Plan area and other adjacent areas and potential public concerns including consultation with all property owners impacted by the proposed endorsement.

Depending on the nature and scale of the amendment, Council endorsement may not be required.

II(b). IMPLEMENTATION INSTRUMENTS

Rezoning of Land

The Generalized Land Uses shown in Figure 3 are realized over time as land is subdivided, zoned and developed in accordance with the OCP and, where applicable, with Secondary Plans. While the OCP land use designations show Council's intention for future land uses, zoning sets out the currently permitted specific land uses and densities for each legal parcel of land. Where property is rezoned, the proposed zoning must be consistent with the OCP and, where it is not, the OCP must be amended prior to rezoning to maintain consistency.

Subdivision

Subdivision of land is subject to the approval of the City's Approving Officer consistent with the policies of this Official Community Plan, the provisions set out in Surrey's Zoning and Subdivision and Development By-laws and in accordance with the *Land Title Act*.

Density Bonusing

The *Local Government Act* (Section 482) permits municipalities to grant additional (or 'bonus') densities on parcels of land in exchange for defined Community Amenity Contributions from developers. The City of Surrey has established policies related to density bonusing in City Centre, Town Centres and in Secondary Plan areas, consistent with the Land Use and Density Section of this OCP.

II(c). IMPLEMENTATION INSTRUMENTS

Authority

The authority to issue Temporary Use Permits is granted to municipalities under Sections 492 and 493 of the *Local Government Act*.

Permit Areas

In relation to Section 492 of the *Local Government Act*, the entire City of Surrey is designated as an area within which City Council may issue Temporary Use Permits.

Permit Conditions

Temporary Use Permits may be issued by a resolution of Council for a period not exceeding three years to:

- Allow a use on a specified property that is not permitted by a zoning by-law
- Specify conditions under which the Temporary Use may be allowed
- Allow and regulate the construction of buildings or structures related to the Temporary Use.

As a condition of issuing a Permit, Council may require applicants or owners to remove buildings, restore property to a specific condition when the use ends, and/or post a security bond.

Approved Permits

Approved and finalized Temporary Use Permits are registered against the Title of a property. Permission to renew an approved Temporary Use Permit is limited to one renewal only and can only be granted by a resolution of Council.

II(d). IMPLEMENTATION INSTRUMENTS

Authority and Purpose

The *Local Government Act* (Sections 488 and 491) authorizes municipalities to issue Development Permits as a means of controlling development design and site planning in order to meet specified objectives. Where Development Permit Areas are designated, landowners are required to obtain a Development Permit prior to altering or subdividing land, or constructing, adding or altering a building or structure.

Development Permits may be required for development within specific areas for:

- * Protecting the natural environment, its ecosystems and biological diversity
- * Protecting development from hazardous conditions
- * Protecting farming
- * Revitalizing areas where commercial uses are permitted
- * Establishing objectives for form and character of intensive residential development
- * Establishing objectives for the form and character of commercial, industrial or multiple family residential development
- * Establishing objectives to promote energy conservation, water conservation or the reduction of greenhouse gas (GHG) emissions.

Where areas are designated as a Development Permit Area, the special conditions which justify the designation must be described and guidelines identifying how conditions will be alleviated and how objectives will be achieved must be specified.

Process

Depending on the circumstances for development, a property may fall within more than one Development Permit Area and in that case, only one application is required. Guidelines **from all the applicable** Development Permit Areas however, shall apply and will need to be taken into consideration for any development that occurs. Application requirements and development guidelines associated with each Development Permit Area are set out in Sub-Section VI of this OCP Implementation Section.

DEVELOPMENT PERMITS (cont.)

Development Permit Areas

The following Development Permit Areas are established for Surrey:

DP Area #1: Form and Character

DP Area #2: Hazard Lands

DP Area #3: Sensitive Ecosystems

DP Area #4: Farming Protection

BL 18423
BL 18784

DEVELOPMENT PERMITS (cont.)

BL 18423
BL 18787
BL 19363

DP1: Form and Character

APPLICATION

Local Government Act Section 488 (1), Category (f):

Form and Character of Commercial, Industrial or Multiple Family Residential Development

A Development Permit shall be required for any improvements to a property, including alterations, site design, landscaping, new construction, or subdivision, for all commercial, industrial or multiple family residential development.

AREA

The entire City of Surrey is designated as Development Permit Area #1. City-wide Development Permit Guidelines apply to all Commercial, Multi-Family, Employment or Industrial development, excluding those areas shown on Figure 64.

IMPLEMENTATION

Unless otherwise indicated, any Development Permit falling within the parameters of this Section requires approval of City Council. In accordance with the Surrey Development Permit Procedures and Delegation Bylaw, as amended, authority to issue a Development Permit for: (a) free standing signs; (b) surface parking lots (including truck parking facilities); (c) sign design packages; (d) minor amendments to a previously issued Development Permit; and (e) reissuance or extension of an expired Development Permit has been delegated to the Planning and Development Department. Where a disagreement exists between the Planning and Development Department and an applicant, authority to issue a Development Permit defaults to City Council.

JUSTIFICATION and OBJECTIVES

The OCP establishes Development Permit Areas and Guidelines for the following reasons:

A key strategy of the OCP is to create complete communities and mixed-use urban centres, providing a built environment that is safe, attractive, people-friendly and energy efficient. Surrey's increasing rate of economic, commercial and multiple residential development, often characterized by high density and intensity of uses, requires the use of design guidelines to ensure high standards are achieved in the design quality of the built environment, while also protecting the natural environment. Development Permits help to create attractive and active streetscapes that engage pedestrians and complement neighbouring properties. It also encourages the development of buildings and other development features that conserve energy and water and have reduced contributions to Surrey's greenhouse gas emissions.

For industrial lands, development is often located in highly visible areas such as along major roads, at key entrances to the City, or near residential neighbourhoods. Using Development Permits is intended to ensure that developments are of high quality and do not adversely impact the image or character of the city.

DEVELOPMENT PERMITS (cont.)

DP1: Form and Character (cont.)

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BL 21389

The following development scenarios do not require a Development Permit:

EXEMPTIONS (General):

- Internal renovations where there are no changes to the external appearance of a building
- External renovations or additions that are compatible with the existing building in terms of materials, colours, form and character
- Site improvements (e.g. landscaping and parking lot paving) where the value of the work is less than \$25,000
- Construction of (not more than) one accessory building or structure (excluding signs) with a floor area equal to or less than 10 sq. m (108 sq. ft.)
- Replacement of existing signs provided they are in full compliance with the Surrey Sign By-law (as amended) or any sign regulation in the Zoning By-law (as amended)
- Subdivision for the purpose of lot consolidation, lot line adjustment or road widening to meet City requirements
- Development of land where a Development Agreement associated with a Land Use Contract is in effect, provided the Agreement is consistent with the objectives of this Section
- Buildings for agricultural purposes.
- Duplexes only within any small-scale multi-unit housing zones.

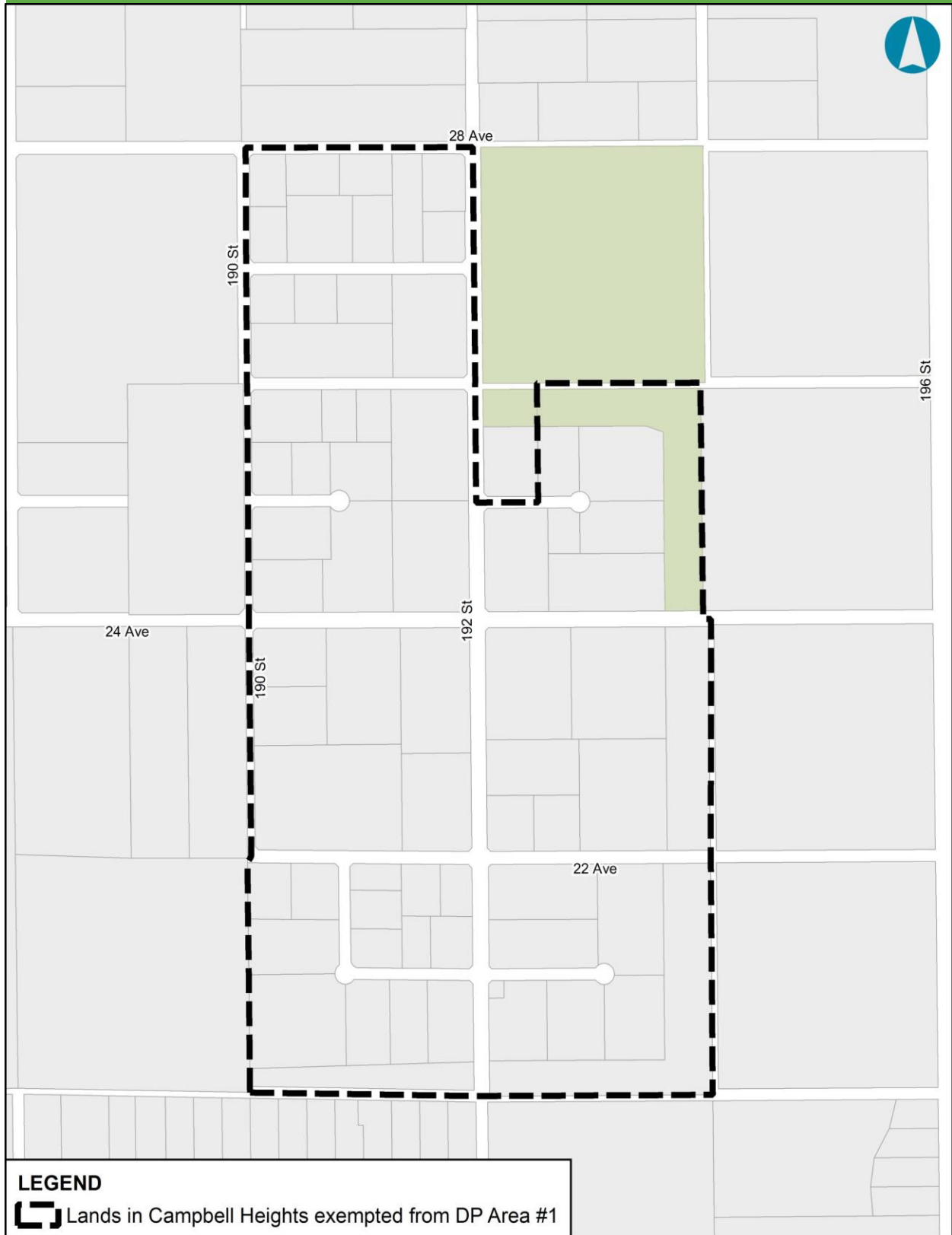
EXEMPTIONS (Commercial):

- Within an existing development already controlled by a Development Permit, new construction equal to or less than 470 sq m (5,000 sq ft) in area, or exterior building renovations, or any additions, provided that the new construction is compatible with the existing development in terms of materials, colours, form and character
- Exterior renovations, additions and/or site works located more than 100 m (328 ft) from any residential property and that is not visible from any road.

EXEMPTIONS (Industrial):

- Development including new buildings, exterior renovations, additions and/or site works that abut land only designated Industrial (and not any other designation) in the OCP (see Figure 3) AND that:
 - Ⓓ does not abut an arterial road or Provincial highway, OR
 - Ⓓ is not visible from an arterial road or Provincial highway, OR
 - Ⓓ is visible from an arterial road or Provincial highway BUT is located a minimum of 100 m away from those roads, OR
 - Ⓓ is located more than 100 m (328 ft.) from all Provincial Highways or arterial roads BUT does not fall within a business/industrial park.
- New buildings (limited to 470 sq. m (5,000 sq. ft. or less)) OR exterior renovations, additions and/or site works AND that are visible from an arterial road or Provincial highway AND that are located within 100 m from those roads ONLY IF they are compatible with existing on-site development already controlled by a Development Permit (e.g. materials, colours, and form and character).
- Lands illustrated in Figure 64 - Area of Campbell Heights Business Park.

DEVELOPMENT PERMITS
Figure 64. Campbell Heights Local Area Plan (cont.) Exemption Area



DEVELOPMENT PERMITS (cont.)

DP2: Hazard Lands

BL 18423
BL 18787

APPLICATION

Local Government Act Section 488 (1), Category (b):

Protection of Development from Hazardous Conditions

A Development Permit will be required prior to development activities for any of the following scenarios:

- ▣ Subdivision of land
- ▣ Land alteration or land clearing activity (includes, but is not limited to, removal of trees or shrubs with no limitation or specification to size, diameter or species)
- ▣ For STEEP SLOPES ONLY, construction of, addition to, or alteration of buildings or structures on all properties with a slope of twenty percent (20%) or greater, or
- ▣ For FLOOD PRONE AREAS ONLY, construction or, addition to, or alteration of buildings or structures on all properties within flood prone lands.

AREA

Development Permit Area #2, for the protection of development from hazardous conditions, is designated as follows:

- a) All parcels of property containing land with steep slopes a minimum gradient of twenty percent (20%) or more, or which are either within 10 m of the bottom or 30 m of the top of a slope that is a minimum of twenty percent (20%) gradient, as generally shown in Figure 65; and
- b) All parcels or property containing lands within the 200-year floodplain, as generally shown in Figure 66.

IMPLEMENTATION

Unless otherwise indicated, and in accordance with the Surrey Development Permit Procedures and Delegation Bylaw, as amended, authority to issue a Hazard Lands Development Permit is delegated to the Planning and Development Department for all applications except where accompanied by another application requiring City Council approval. Where a disagreement exists between the Planning Department and an applicant, authority to issue a Development Permit defaults to City Council.

JUSTIFICATION and OBJECTIVES

Lands subject to a variety of natural hazards are found within Surrey's boundaries. Principal among these hazards are steep slopes and lands possessing potentially unstable soil conditions and lands located in flood prone areas. By creating a Development Permit Area and corresponding Guidelines for these potentially hazardous areas, the City is protecting persons, property and the natural environment from hazardous conditions and is working to ensure measures are taken to address these conditions in conjunction with development.

DEVELOPMENT PERMITS_(cont.)

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DP2: Hazard Lands _(cont.)

OBJECTIVES

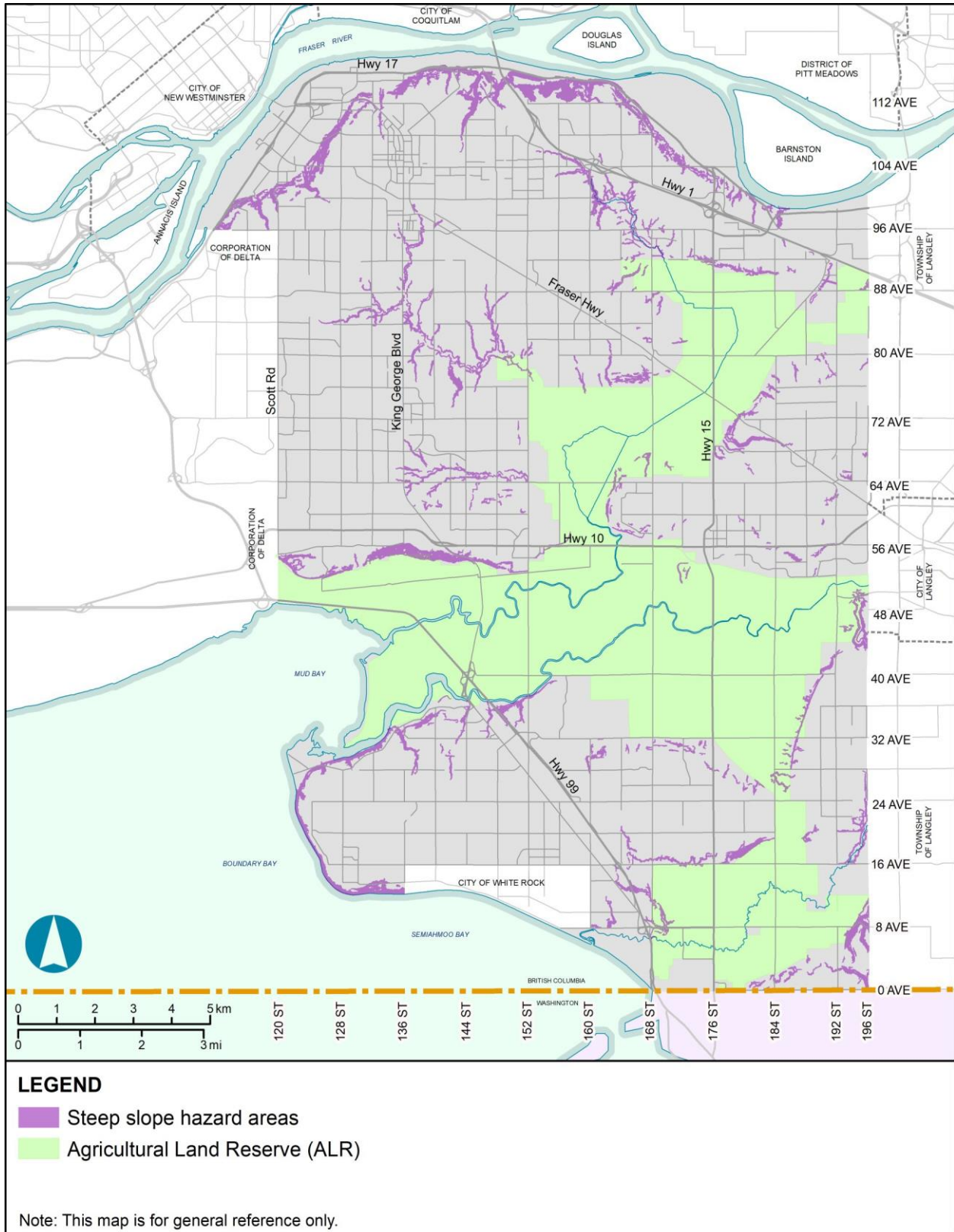
- a) The City of Surrey's objectives for establishing **STEEP SLOPE HAZARD** Development Permit Guidelines are to:
 1. Protect people, property and the natural environment from the consequences of natural hazards and from development on unsafe, unstable slopes and potentially hazardous areas in Surrey
 11. Prevent soil erosion in areas of unstable or potentially unstable slopes which pose hazards
 111. Promote development that respects the terrain, vegetation, drainage courses and constraints related to the hillside environment of the site
 - 1v. Maintain vegetative cover of steep slopes as a means to preventing erosion and unstable slopes
 - v. Promote the siting of buildings and other structures that are compatible with the steep slope context
 - v1. Ensure road design and anticipated uses provide for a safe environment and ease of on-going maintenance.
- b) The City of Surrey's objectives for establishing **FLOOD PRONE HAZARD** Development Permit Guidelines are to:
 1. Protect people, property and the natural environment from the consequences of natural hazards.

EXEMPTIONS

The following scenarios shall NOT require a Development Permit

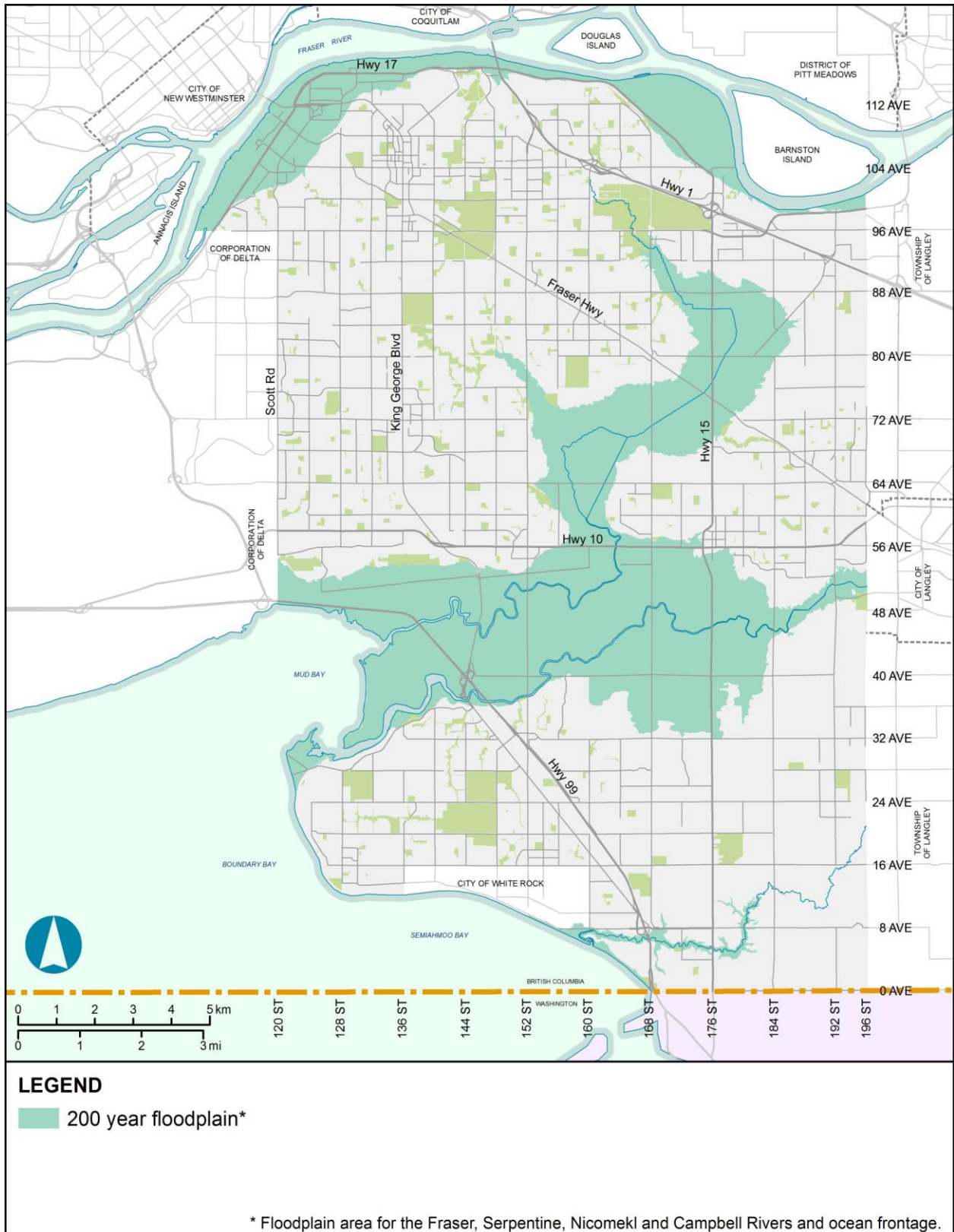
1. Emergency actions required to prevent, control or reduce an immediate threat to human life and safety and/or property
11. Interior renovations within an existing building footprint not resulting in any change to the external structure of a building or causing any disturbance to a site
111. Construction and/or maintenance of municipal operations or utility works and services undertaken or authorized by the City of Surrey
- 1v. Regular and general yard maintenance activities within an existing landscaped area, such as mowing lawns, planting vegetation, minor soil disturbances and pruning vegetation (according to *Surrey Tree Protection By-law*, as amended) that do not alter the general grade of the land or approved landscape buffers
- v. Construction of farm buildings and production of agricultural products, in accordance with normal farming practices, as defined in the *Right to Farm Act* as amended, for those lands within the Agricultural Land Reserve.
- v1. For Flood Hazards only, Single Family Dwelling construction on existing lots (excluding those with subdivision potential).
- v11. For Flood Hazards only, Single Family Dwelling construction within the Agricultural Land Reserve, where the Flood Construction Level (FCU is met and appropriately addressed with an approved Fill Permit.
- viii. Where information is provided that indicates, to the satisfaction of the General Manager, Planning and Development, that the proposed development will not disturb or affect portions of a property that are subject to hazardous conditions due to steep slopes or flood risk.

Figure 65: Steep Slope Hazard Lands Development Permit Area



BL 18787
BL 19364

Figure 66: Flood Prone Hazard Lands Development Permit Area



DP3: Sensitive Ecosystems

APPLICATION

Local Government Act Section 488, Category (a):

Protection of the Natural Environment, its Ecosystems and Biological Diversity

A Development Permit will be required prior to development activities for any of the following scenarios:

- ⇒ Subdivision of land
- ⇒ Construction, addition or alteration of a building or structure
- ⇒ Construction of roads and trails
- ⇒ Disturbance of soils, land alteration or land clearing
- ⇒ Installing non-structural surfaces with semi-pervious or impervious materials

AREA

Development Permit Area #3, for the protection of sensitive ecosystems, is designated as follows:

- all parcels of property where any portion of the property falls within 50 m of the Green Infrastructure Area, as shown in Figure 67; and
- all parcels of property where any portion of the property falls within 50 m of the top of bank of either one or both sides of a Class A, A/O or B stream containing fish habitat, as generally shown in Figure 68. Should a stream not illustrated in Figure 68 be identified, it is the responsibility of the applicant to contact the City of Surrey to determine whether or not the area in question falls within this Development Permit Area.

IMPLEMENTATION

Unless otherwise indicated, and in accordance with the Surrey Development Permit Procedures and Delegation Bylaw, as amended, authority to issue a Sensitive Ecosystem Development Permit is delegated to the Planning and Development Department except where the Development Permit application is accompanied by another application requiring City Council approval. Where the content of a Development Permit cannot be agreed upon between the Planning and Development Department and an applicant, authority to issue a Development Permit remains with City Council.

DEVELOPMENT PERMITS (cont.)

BL 18784

DP3: Sensitive Ecosystems

JUSTIFICATION

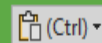
Lands of significant ecological value, including riparian areas, fall within Surrey's boundaries. The use of a Development Permit Area for these areas are justified as follows:

- a) Green Infrastructure Areas: Surrey's *Ecosystem Management Study* and *Biodiversity Conversation Strategy* (BCS) identify Surrey's sensitive ecosystem areas. These areas provide natural environment and ecosystem protection as well as biological diversity. The hubs, sites and corridors identified in the BCS provide a contiguous area of ecological importance because they function as habitat and movement systems for wildlife and other natural habitat preservation; offer increased biodiversity in urban areas; provide natural water storage and drainage; and act as natural purification systems that work to reduce erosion and sedimentation. The purpose of the Sensitive Ecosystem Development Permit Guidelines for Green Infrastructure is to ensure that the protection of these high quality plant communities and wildlife habitat areas occurs concurrently with development. Retaining large, contiguous sensitive ecosystem areas helps to retain habitat diversity and quality of life and provides for an overall healthy community for Surrey residents.
- b) Streamside Areas: Rivers, streams, lakes and ponds provide fish habitat and aquatic habitat for wildlife. Streamside areas, where in a natural state, have dense and structurally-complex vegetation that:
 - have high biodiversity and support a large number of vegetation and wildlife species;
 - contribute to the ecological health of the adjacent stream and its fish habitat;
 - prevent the loss of riparian land due to bank instability and soil erosion;
 - provide natural habitat;
 - provide natural corridor links between ecosystem hubs and sites throughout Surrey and neighbouring communities.

The purpose of establishing a Sensitive Ecosystem Development Permit Area for Streamside Areas is to:

- support development while ensuring these areas remain in a largely natural and undisturbed state;
- satisfy requirements of local government to ensure development compliance with the Riparian Areas Protection Regulation B.C. Reg. 376/2004; and
- Maintain the safe conveyance of stormwater following the City of Surrey's Natural Drainage Policy.

DEVELOPMENT PERMITS (cont.)



BL 18784

DP3: Sensitive Ecosystems

OBJECTIVES

The City of Surrey's objectives for establishing a Sensitive Ecosystem Development Permit Area for Green Infrastructure and Streamside Areas are to:

- a) Green Infrastructure Areas
 - Maintain and enhance ecosystems and critical habitat for at-risk species and generate opportunities that support and enhance biological diversity;
 - Integrate ecological systems and processes into urban areas and assess conservation values and enhancement priorities equally with land development, growth management and infrastructure planning; and
 - Identify and minimize the disturbance of development on the Green Infrastructure Network, as defined in Surrey's *Biodiversity Conservation Strategy*.

- B) Streamside Areas
 - Conserve, protect and enhance aquatic areas and associated riparian habitat;
 - Encourage ecosystem processes and biodiversity and ensure development activity does not encroach upon or alter riparian and aquatic habitats; and
 - Provide for: drainage maintenance access; potential public trails; beaver habitat accommodation; tree health; natural stream dynamics; and slope stability risk minimization.

EXEMPTIONS

The Following scenarios shall NOT require a Development Permit:

- i. Emergency actions required to prevent, control or reduce an immediate threat to human life and safety, the natural environment and/or property, as authorized by the General Manager, Engineering Department, or their designate.
- ii. Interior renovations within an existing building footprint not resulting in any change to the external structure of a building or causing any disturbance to a site.
- iii. Ecological restoration and enhancement projects authorized by the General Manager, Engineering Department or of the Parks, Recreation and Culture Department, or their designate.
- iv. Construction, maintenance or operation of municipal or public utility works and services.
- v. Regular and general yard maintenance activities within an existing landscaped area, such as mowing laws, pruning trees and shrubs (in accordance with Surrey Tree Protection Bylaw, as amended), planting vegetation and minor soil disturbances that do not alter the general grade of the land or existing approved landscaped buffers.
- vi. Permitted farm activity within the Agricultural Land Reserve (ALR).
- vii. Where there is an existing agreement with the Federal or Provincial governments and the proposed activities are consistent with those agreements.
- viii. Where information is provided, to the satisfaction of the General Manager, Planning and Development Department, that illustrates that portions of a property falling within the Sensitive Ecosystem Development Permit Area do not impact its objectives.

Figure 67: Green Infrastructure Development Permit Area

BL 18784
 BL 18787
 BL 19364
 BL 21104

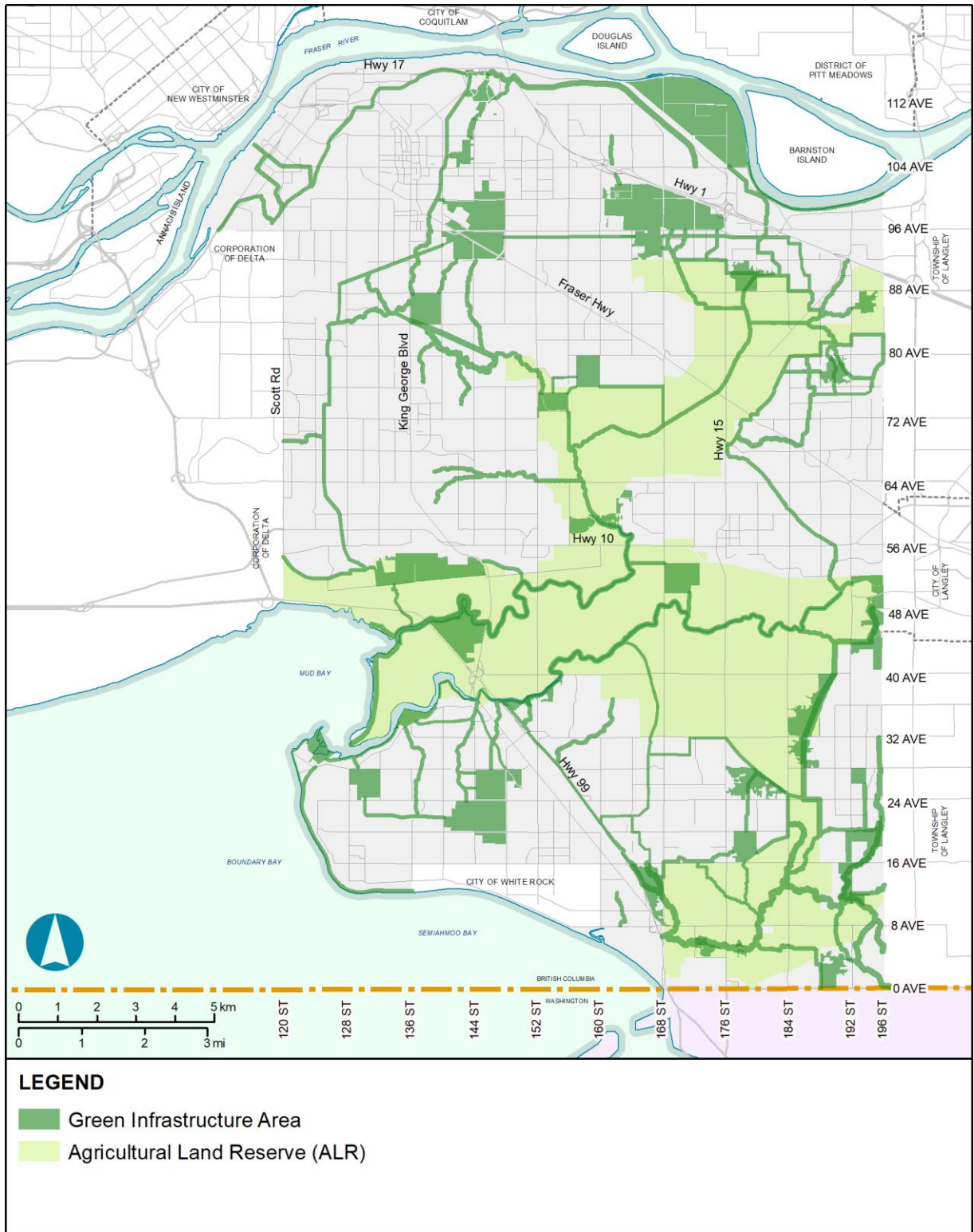
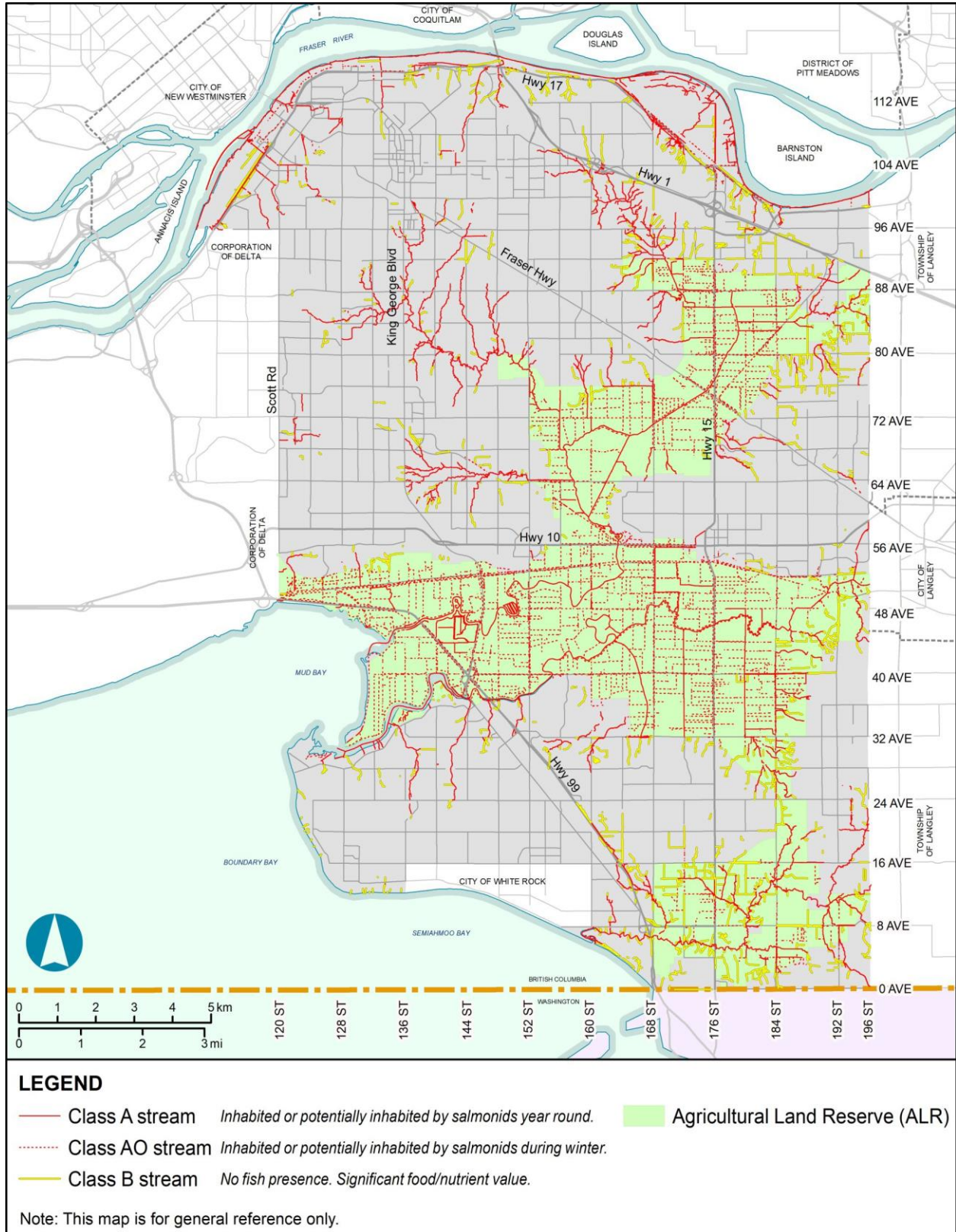


Figure 68: Streamside Development Permit Area



BL 18784
BL 19364

LEGEND

- Class A stream *Inhabited or potentially inhabited by salmonids year round.*
- ⋯ Class AO stream *Inhabited or potentially inhabited by salmonids during winter.*
- Class B stream *No fish presence. Significant food/nutrient value.*
- Agricultural Land Reserve (ALR)

Note: This map is for general reference only.

DEVELOPMENT PERMITS (cont.)

BL 18423
BL 18787

DP4: Farming Protection

APPLICATION

Local Government Act Section 488 (1), Category (c):

Protection of Farming

A Development Permit shall be required for any improvements to a property, including new construction, building alterations, site works, landscaping or subdivision, for all commercial, industrial, institutional or residential development.

AREA

Any commercial, industrial, institutional or residential development within Surrey that directly abuts land within the Agricultural Land Reserve (ALR) and/or that falls within 50 m of the ALR boundary, (see Figure 69), is designated Development Permit Area #4 for the protection of farming in Surrey.

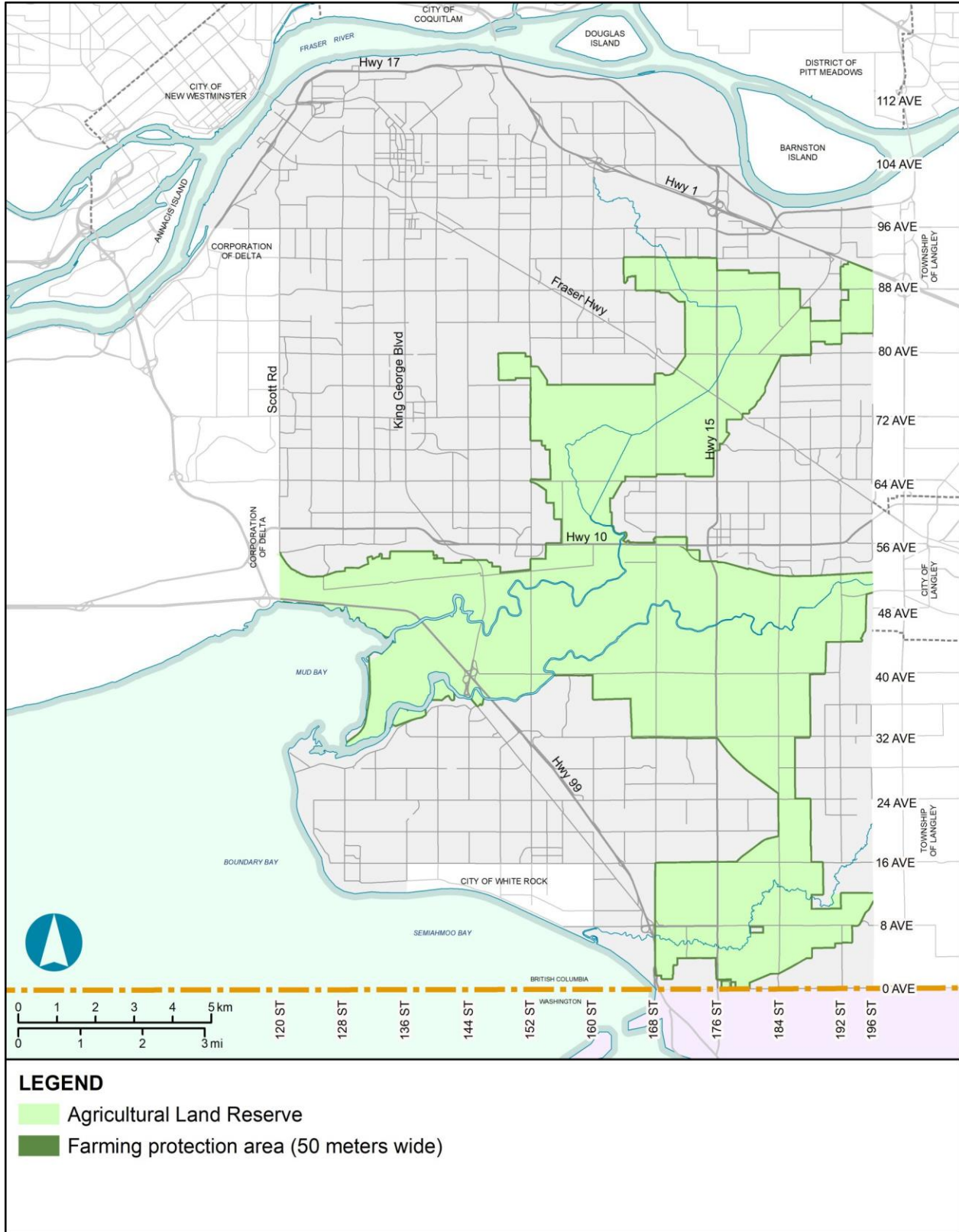
IMPLEMENTATION

Unless otherwise indicated, and in accordance with the Surrey Development Permit Procedures and Delegation Bylaw, as amended, authority to issue a Farm Protection Development Permit is delegated to the Planning and Development Department for all applications, except where accompanied by any other type of development application. Where a disagreement exists between the Planning and Development Department and an applicant, authority to issue a Development Permit defaults to City Council.

JUSTIFICATION

Agriculture is a prominent land use in Surrey, is a vital, sustaining component of the local economy, is part of Surrey's heritage and future, and accounts for a significant portion of BC's agricultural output. Lands falling within the Agricultural Land Reserve are deemed to be appropriate for agricultural production based on soil suitability, topography, and other factors. Agricultural lands are at risk from encroachment by non-agricultural uses on adjoining or nearby lands and the continued urban growth of Surrey creates the potential for land use conflicts along urban/agricultural boundaries. By creating a Development Permit area along the boundary of the Agricultural Land Reserve, the City is supporting the minimization of conflicts between urban areas and farming activities along the agricultural-urban interface.

Figure 69: Farming Protection Development Permit Area



BL 18423
BL 19364

DEVELOPMENT PERMITS (cont.)

BL 18423

DP4: Farming Protection (cont.)

OBJECTIVES

The City of Surrey's objectives for establishing Agricultural Development Permit Guidelines are to:

- a) Encourage the development of effective landscape buffers along the boundary of Agricultural land Reserve (ALR) designated lands
- b) Adjust urban densities and buildings along the urban side of the (ALR) border to allow for better management and maintenance of Agricultural buffers.
- c) Encourage open space creation, wildlife management and habitat protection along the urban side of urban-agricultural interface areas to minimize the impact on farm activities.

EXEMPTIONS

The following development scenarios do not required a Development Permit:

- a) Replacement or addition to a building where it does not *adversely impact* (to the satisfaction of the General Manager of Planning and Development) agricultural uses or normal farm practices (existing or future) of ALR areas

NOTE: *Adverse Impacts* can include, but are not limited to, the following:

Drainage—when development creates flooding, erosion or siltation damage and reduced groundwater levels that prohibit agricultural irrigation

Irrigation—when development contaminates water supplies used for agricultural irrigation

Transportation and Traffic—when development restricts access to farmland or prevents farm vehicle movement on local roads

Land Uses—when development adds sensitive uses adjacent to agricultural land without proper attention to screening or buffering

- b) Replacement of a building that has been destroyed by fire or natural causes, in cases where the replacement building is identical to the original in location, floor area and height
- c) Interior/exterior building alterations that do not expand the existing building foundation
- d) Construction, additions or alterations not exceeding 30 sq. m (323 sq. ft.) where no variances of the Zoning By-law are required
- e) Landscaping and normal yard maintenance that does not affect a landscape buffer planted in accordance with these Development Permit Guidelines
- f) Where information is provided, to the satisfaction of the General Manager of the Planning and Development Department, that illustrates that the proposed development will not adversely impact farming or farming buffers used to protect farming.

II(e). IMPLEMENTATION INSTRUMENTS

BL 18423

Authority and Purpose

The *Local Government Act* (Sections 484—487) (as amended) authorizes municipalities to designate Development Approval Information Areas within its Official Community Plan in order to gather or obtain information on the anticipated impact of a proposal on the community. Development Approval Information may be collected for: Rezoning, Subdivision, Official Community Plan Amendments, Temporary Use Permits and/or Development Permits Applications.

Area

The following Development Approval Information Areas are established for Surrey:

DAI Area #1:

The entire City of Surrey is designated as Development Approval Information Area #1 for the purposes of obtaining information related to the anticipated impact of development on the City's infrastructure, transportation systems, ecosystems, community facilities and community services.

Objectives

Development Approval Information can be requested by the City to ensure that:

- * Appropriate studies and technical information are provided by the applicant in order for the City of Surrey to evaluate the development and
- * New development meets the intent of the Official Community Plan's objectives and policies and other City Bylaws.

Process

Development Approval Information Areas can be used for development based the policies established in this Section. Reports or studies required by the City shall be prepared at the applicant's expense and by an appropriately Qualified Professional. The City of Surrey reserve the right to require a peer review of a Qualified Professional's report.

*

DEVELOPMENT APPROVAL INFORMATION AREAS (cont.)

BL 18423

Policies

The types of plans and studies that may be required for development in Surrey include, but are not limited to, the following:

- * Land use plan
- * Environmental assessment and mitigation plan (prepared by a Qualified Environmental Professional)
- * Traffic Impact Assessment (prepared by a Professional Engineer) referencing roads, transit, cycle and pedestrian routes, and parking areas and identifying any transportation improvements or necessary mitigative measures
- * Hydrological assessment of drainage patterns and proposed stormwater management facilities (prepared by a Professional Engineer)
- * Geotechnical Stability Assessment (prepared by a Qualified Professional Engineer or Geoscientist)
- * Engineering studies identifying infrastructure requirements and impacts and, if necessary, any proposed improvements to this infrastructure
- * Studies on the impacts of public facilities and community services (e.g. schools, libraries, parks, recreation centres, fire protection, police offices, emergency services, etc.)
- * Assessment of impacts on adjacent areas or public lands, lighting, noise and visual impacts and proposed means of buffering or mitigating impacts
- * Plans indicating proposed land uses, including the allocation of uses, density, parks, open space or other amenity contributions and the proposed sequence of development, among parcels where multiple parcels and/or owners are involved in a comprehensive development
- * Assessment of economic costs and benefits, including employment benefits, Development Cost Charges and tax impacts, public infrastructure costs and benefits of any proposed development and may include a market analysis to demonstrate the demand for and feasibility of the development
- * Studies required to address other issues that may be identified by Surrey City Council.

II(f). IMPLEMENTATION INSTRUMENTS

BL 18423

Overview

The City of Surrey develops and maintains Master Plans and Strategies on a wide range of issues and subjects to provide focused policy in various aspects of community development. These Master Plans and Strategies contain policy and action recommendations that are consistent with the Official Community Plan and that implement the general directions of the OCP in a more detailed manner.

Current Council-approved Master Plans and Strategies include, but are not limited to:

- * Crime Reduction Strategy
- * Agricultural Protection and Enhancement Strategy
- * Employment Lands Strategy
- * Environmental Management Strategy
- * Biodiversity Conservation Strategy
- *
- * Walking Plan
- * Cycling Plan
- * Greenways Master Plan
- * Parks, Recreation and Culture Strategic Plan
- * Cultural Plan
- * Public Art Master Plan
- * Heritage Strategic Review
- * Plan for the Social Well-Being of Surrey Residents
- * Affordable Housing Strategy
- * Community Energy and Emissions Plan
- * Surrey Climate Adaptation Plan

Engineering Services

Surrey maintains an *Engineering Servicing Plan* (as amended) that identifies engineering infrastructure projects required to support the growth and development envisioned in the OCP over a 10-year period. The *Servicing Plan* identifies the estimated capital funding required to undertake these projects as well as the classification of relative timing within the 10-year period. The City's Development Cost Charges By-law, as amended, identifies the source of capital funding required to implement the 10-year *Servicing Plan*, including the amount and percentage of project funding derived from Development Cost Charges levied on development.

II(g). IMPLEMENTATION INSTRUMENTS

Design guidelines included in the Development Permit Form and Character Guidelines of this Official Community Plan may be supplemented by additional design guidelines established in Secondary Plans, including, but not limited to, Neighbourhood Concept Plans, Town Centre Plans and Local Area Plans.

The Guidelines documents referenced in this Section are to be applied to development in addition to the Development Permit Guidelines established within this Official Community Plan.

Supplementary Areas

Supplemental design guidelines to this OCP included within separate publications include:

- * Form and Character Guidelines for the Crescent Beach Commercial Area
- * Cloverdale Town Centre Plan
- * Newton Cultural Commercial District
- * Douglas
- * Fleetwood Town Centre Plan
- * Campbell Heights Industrial Park Design Guidelines.

III. OCP MONITORING AND REVIEW

BL 18423

The City of Surrey reviews and evaluates progress towards achieving the goals and objectives of the OCP on an on-going basis by using annual reports on key indicators and major 5-year reviews and amendments, as needed.

Annual Report

The OCP Annual Report updates Council on key measures including:

- * Population growth and demographic changes
- * Housing development and availability
- * Commercial, industrial and mixed-employment development
- * Land supply and development capacity in Secondary Plan areas
- * Economic, social and environmental indicators
- * Sustainable development indicators (see Figure 70)
- * Services and infrastructure development.

Major Five-Year Review

Every five years, a major review of the Official Community Plan will be initiated, involving extensive public consultation. This review may result in amendments to the Plan to meet emerging trends and planning issues. The next major review of this OCP is expected to take place in 2018.

Incremental Amendments

This OCP may be amended by By-law as needed, in response to senior government requirements, proposed land use changes related to development applications, or to maintain consistency with approved Secondary Plans, Council Policies, Master Plans and Strategies and/or capital works programs.

Applications for an amendment to the OCP, including the Land Use Designation Map (see Figure 3), shall indicate which significant community benefit will be established as a result of the OCP changing to accommodate a development proposal. Significant community benefit is defined as: an immediate, tangible benefit such as affordable housing, community amenity, environmental protection or enhancement, park land or significant public space, or any other community benefit identified by Council. These items exclude from consideration increases in property assessment of taxes and private economic benefits associated with new development.

IV. SUSTAINABLE DEVELOPMENT INDICATORS

The Annual Report on OCP Implementation includes key indicators related to the sustainable development of the City. The purpose of reporting on these indicators is to monitor and track progress toward the achievement of key goals and objectives of the OCP.

Examples of key indicators that are linked with elements of the OCP Vision Statement are shown in Figure 70.

Figure 70: Sustainability Development Indicators

Compact and Connected City

1. Population density in Urban, Multiple Residential, Commercial, Town Centre and CBD designations
2. Percent multi-family housing units (annual and cumulative)
3. Percent new development in Town Centres, City Centre and Frequent Transit Development Areas (FTDA)
4. Percent transportation mode for trips by walking, cycling and transit

Complete City

5. Percent dwelling units within 400 m of parks and greenways
6. Percent agricultural land in production
7. Percent non-residential tax base (and percent annual non-residential construction value)
8. Full Time Equivalent (FTE) attendance at post-secondary institutions in Surrey
9. Ratio of Local Jobs—to—Labour Force

10. Annual attendance at cultural facilities (library, art and heritage)
11. Percentage of all residential units located within 400 m of one, two or three of the following amenities: a) Frequent transit stop; b) Grocery store; c) Elementary school

Green and Resilient City

12. Percent of tree canopy coverage in non-agricultural designations
13. Percent of Green Infrastructure Network in public ownership
14. Per capita non-agricultural greenhouse gas emissions (compared to 2007 benchmark levels)
15. Percent waste diverted from landfill

Safe, Inclusive and Healthy City

16. Violent crime rate per 1,000 residents
17. Ratio of Median Rent—to—Median Income
18. Licensed childcare spaces per capita
19. Annual visits to City recreation centres

V. DEFINITIONS

BL 18423

Accessible Community

Refers to ensuring there is accessibility for all ages within community infrastructures and built environments, including linking with municipal governments to make sure families with young children, those living with special needs or challenging mental or physical health issues are carefully considered in community planning processes.

Adaptable Design

Refers to building features, particularly housing features intended for use by people with disabilities or those who are limited in mobility. Adapting a space beyond current Building Code requirements enables people to create livable homes where they can live and thrive independently.

Affordable Housing

Rental or owned housing provided to low income households who have an affordability problem (pay in excess of 30% of their income on housing) and earn less than the median income for Surrey. Is subsidized by senior levels of government or by a non-profit agency which enables rents to be provided below-market rates on a 'geared to income' basis. Are typically publicly owned or owned and operated by a non-profit agency. Does not include shelters or transitional housing.

Biodiversity

The variety of species and ecosystems on earth and the ecological processes of which they are a part-including natural ecosystems, living species and other components that provide genetic diversity.

Buffer

A strip of land or neutral area designed to protect the natural environment or an identified resource from undesirable encroachment such as development of recreational uses.

Connectivity

A state or measure of spatial linkage, continuity or association between two or more distinct locations.

Corridor

A band of vegetation or strip of land that provides connectivity between distinct patches of habitat on the landscape and permits the movement of plant and animal species between what would otherwise be isolated patches.

Crime Prevention Through Environmental Design (CPTED)

A pro-active approach that seeks to reduce or eliminate opportunities for crime through the incorporation of design principles at the conceptual to detailed design stages of building, site and public realm projects. (e.g. "Eyes on the Street" - refers to designing street frontages and buildings with windows to provide surveillance over spaces).

Ecosystem Management

An approach to Environmentally Sensitive Areas (ESAs) that focuses on ecological values and ensures that areas of ecological significance are protected and enhanced (e.g. wetlands, habitat reservoirs, wildlife corridors and areas with unique flora and fauna).

Environmentally Sensitive Area (ESA)

Comprised of environmentally sensitive and unique natural areas such as watercourses, cliffs, benchlands, escarpments, forests, geological features, wildlife, wetlands and grasslands.

V. DEFINITIONS (cont.)

BL 18423
BL 20026

Environmentally Valuable Resource

Where all the features, places and species present enhance the biodiversity of an area; range in size from small patches to extensive landscape features; can include rare or common habitats, plants and animals; require special management attention to protect fish and wildlife resources, other natural systems or processes and/or historical, cultural or scenic values.

Food Security

When a community's residents have access to safe, affordable, culturally-acceptable, nutritionally-adequate diets through a system that maximizes community self-reliance, environmental sustainability and social Justice

Fragmentation

A process whereby large contiguous ecosystems are transformed into one or more smaller patches surrounded by disturbed areas.

Greenhouse Gas (GHG)

Refers to an atmospheric gas that contributes to the greenhouse effect by absorbing infrared radiation produced by solar warming of the Earth's surface.

Green Infrastructure/Development

A type of infrastructure that uses elements of the natural environment to replace or supplement traditional infrastructure, such as drainage, sewers and water that contributes towards sustainable resource management; a network of facilities that can include parks, local woodlands, gardens, greenway corridors, streams, street trees and built structures such as green roofs.

Habitat

The natural abode of a plant or animal, including all biotic, climate and edaphic (plant communities distinguished by soil conditions rather than by climate) factors affecting life.

Mixed-Use Development

Development that incorporates multiple uses within a building or site; can be a combination of residential, commercial, office, institutional and low-impact industrial uses; allows for a range of needs to be met in one location reducing travel between different locations for housing, employment and services.

Natural Areas

Areas with significant flora and fauna (fish, terrestrial wildlife and bird habitats) and connecting corridors (ravines, treed areas, open fields and bodies of water)

Net-Zero

Refers to achieving an overall balance between emissions produced and emissions taken out of the atmosphere.

Pedestrian-Oriented Space

Areas which promote visual and pedestrian access onto a site and which provides pedestrian-oriented amenities and landscaping to enhance the public's use of a space for passive activities.

Performance-Based Standards

The use of goal-oriented criteria to establish review parameters for proposed land use plans or development projects that help provide flexibility to the development process.

Streetscape

The visual character of a street, as determined by various elements such as structures, greenery, open space, views, etc.

Sustainable Development

Development that balances human need with environmental protection so that human and environmental needs can be met in the present and into the indefinite future.

V. DEFINITIONS (cont.)

Transitional Housing

Short-term housing for people who, in addition to needing affordable housing, need non-housing support services including meals, housekeeping, medical care, counseling and skills training to enable them to maintain occupancy in permanent housing; may be funded by a variety of provincial programs, including health, social and corrections-based programs.

Transit-Oriented Development (TOD)

Residential, commercial and mixed-use centres designed to maximize access by transit and non-motorized transportation modes, particularly walking and cycling; ideally provides a wide range of local services in order to reduce the need for using private automobiles.

Universal Design

The principle that products or designs should be developed with built-in features and flexibility that enables use by all people regardless of age and ability, at no additional cost.

Watercourse

Includes a natural depression with visible banks, or wetlands with or without visible banks, which contain water at some time; includes any lakes, rivers, streams, creeks, springs, swamps, gulches or surface sources of water whether containing fish or not; includes intermittent streams; includes surface drainage works which are inhabited by or provide habitat for fish.

Wetlands

An area of land consisting of soil that is saturated with moisture, such as a swamp, marsh or bog.

VI. DEVELOPMENT PERMIT AREAS AND GUIDELINES

DP1

DEVELOPMENT PERMIT GUIDELINES: FORM AND CHARACTER

BL 18787
BL 19364

The Form and Character Development Permit Guidelines apply to all areas of the city.

DP1.1: COMMON GUIDELINES—are provided for **ALL TYPES** and, where applicable, additional guidelines also apply to the following specific development classifications:

- *Low Density Multiple Family*
- *Multiple Residential/Mixed-Use/Commercial*
- *Retail/Commercial Centres*
- *Mixed Employment/Industrial*
- *Low, Mid and High Rise Forms*
- *Signs*

DP1.2: GAS STATION GUIDELINES—are provided for both Residential and Commercial

Where a certain development type is not specifically covered in this Section, applicable guidelines should be combined and used collectively. Guidelines within this section may be further explained using graphic images which are indicated with the "*illustr.*" notation.



BL 19364
BL 21389

DP1.1

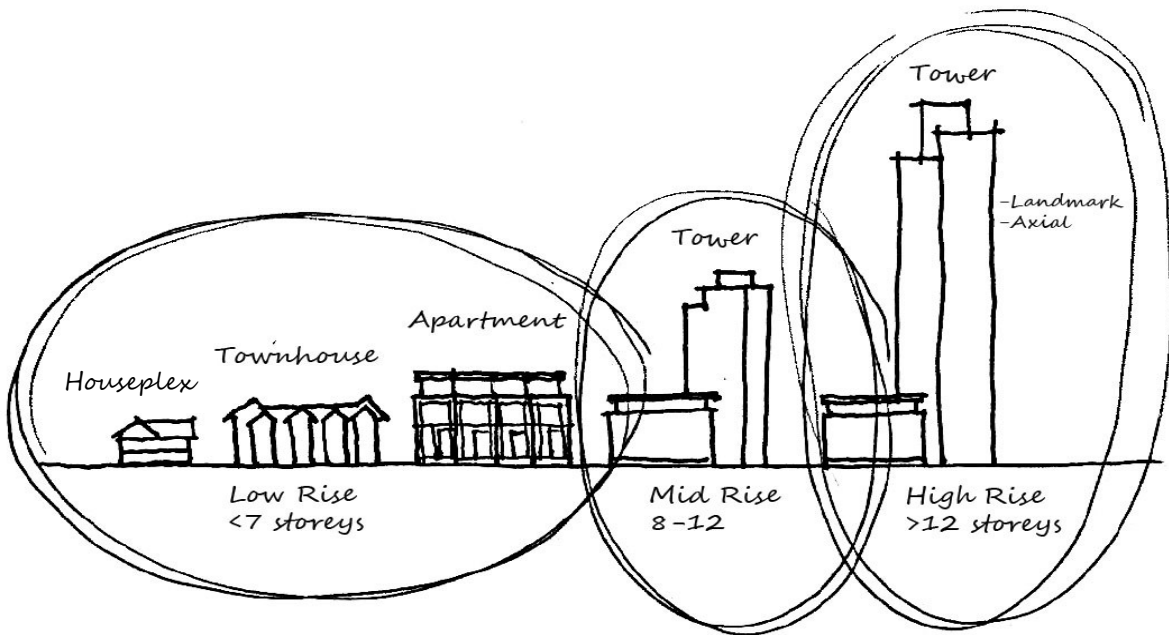
Common Guidelines: All Development Types

With specific categories generally organized as follows:

- Low Density Multiple Residential (includes Houseplexes, Townhouses, etc.)
- Multiple Residential/Mixed-Use/Commercial
- Retail/Commercial Centres
- Mixed Employment (includes Industrial, Business)
- Low-Rise (6 storeys or less), Mid-Rise (7-12 storeys) and High-Rise (more than 12 storeys)
(illustr.)

Guidelines within this section are organized into three different types:

- Site Design
- Building Form and
- Signage



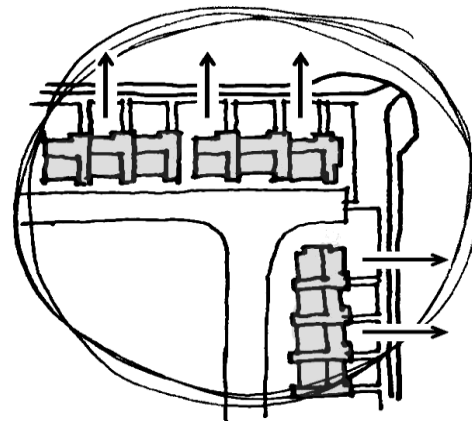
DP1.1 COMMON GUIDELINES

SITE DESIGN

General Site Design

Within a development, the following elements shall be considered in the Site Design:

1. **ALL TYPES:** Provide a transition buffer between adjoining properties containing different uses or a different intensity of uses by using design measures such as landscaping buffers and additional setbacks. *(illustr.)*
2. **ALL TYPES:** When arranging the site layout, take into consideration future intensification of densities (e.g. by clustering buildings to allow for future infill).
3. **ALL TYPES:** For any development adjacent to electrical utility wires, incorporate building setbacks that take into consideration any visual, acoustical or electromagnetic impacts while also demonstrating use of any applicable development guidelines of the utility authority.
4. **ALL TYPES:** Provide a stormwater management strategy for development sites during the early stages of design.
5. **ALL TYPES:** Locate mail facilities within amenity buildings or leave them freestanding on site near an outdoor amenity space. Incorporate weather protection using a design consistent with the overall architectural elements of the site.
6. **TOWNHOUSES:** Orient units to face the street. *(illustr.)*
7. **MIXED EMPLOYMENT:** Minimize the amount of outdoor storage.
8. **MIXED EMPLOYMENT:** Where outdoor storage cannot be avoided, locate it away from public views in rear yards with screening.
9. **MIXED EMPLOYMENT:** Consider using extensive tree coverage to screen storage areas from above views. This is particularly the case in the South Westminster area.



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DP1.1 COMMON GUIDELINES

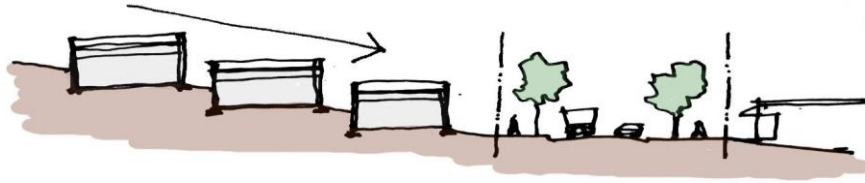
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SITE DESIGN

Site Grading

Within a development, the following elements shall be considered in the Site Design:

10. **ALL TYPES:** Incorporate topography and other significant natural features into the building, structure and overall site design. *(illustr.)*



11. **ALL TYPES:** Establish a development site grading plan at the early stages of design.
12. **ALL TYPES:** Avoid excessive disruption of the natural topography of a site by stepping buildings, sloping parking areas and providing larger setbacks to avoid the use of retaining walls. When subdividing, minimal disruption of the natural topography should be accounted for in lot layout and overall subdivision design.
13. **ALL TYPES:** Where retaining walls cannot be avoided, minimize the overall height of the wall and the height of each step. Incorporate landscaping into the design of the retaining wall and use attractive materials (e.g. brick or stone masonry or sand-blasted concrete with a reveal pattern). Avoid the use of timber retaining walls, particularly where the wall is visible from the public realm.

Circulation—Pedestrian, Active and Non-Vehicular

Within a development, the following elements shall be considered in the Site Design:

14. **ALL TYPES:** Prioritize non-vehicular modes of circulation in site design (e.g. pedestrian and cycling) above all other transportation modes.
15. **ALL TYPES:** For convenience and security, provide bicycle parking facilities at-grade, close to the main building entrance or amenity buildings, and with weather protection.
16. **ALL TYPES:** Provide multiple modes of circulation on pathways to improve natural surveillance opportunities (e.g. provide a street or lane along any publically-accessible pathway).
17. **ALL TYPES:** For circulation that is accessible to all users:
 - a) Ensure pedestrian circulation is convenient, safe and clearly identifiable to drivers and pedestrians
 - b) Use a high standard of accessible and adaptable design to accommodate the functional needs of all individuals including children, adults, seniors, and those with visual, mobility or cognitive challenges
 - c) Locate convenient universal access to buildings from parking areas by using curb let-downs or other accommodating features
 - d) Provide a minimum of 1.8 m unobstructed sidewalk width to accommodate strollers, wheel-chairs and other equipment used by pedestrians.
 - e) Clearly define accessible routes from parking stalls to building entry doors and locate accessible parking closest to the main entry of a building (surface parking) or elevator lobby entrance

DP1.1 COMMON GUIDELINES

SITE DESIGN

Circulation—Pedestrian, Active and Non-Vehicular *(cont.)*

Within a development, the following elements shall be considered in the Site Design:

18. **ALL TYPES:** Where a development directly abuts a transit stop, allow for additional building setbacks to accommodate pedestrian volumes. Create a comfortable experience for pedestrians by including amenities and features such as visual art, drinking fountains, lighting and street furniture.
19. **ALL TYPES:** Provide physical links from the development to public sidewalks, pathways and transit stops. When subdividing, these connections should be accounted for in lot layout and overall subdivision design. *(illustr.)*
20. **ALL TYPES:** Incorporate beautification and amenity features along pedestrian pathway systems (e.g. use distinctive and durable paving, special landscaping with trees, furniture and/or weather protection).
21. **ALL TYPES:** When shopping cart storage is necessary, provide for convenient use and return. Allow for clearances surrounding cart corrals for pedestrians and vehicles and design the structure to be in keeping with the character of other site structures and furnishings.
22. **ALL TYPES:** For circulation through surface parking areas:
 - a) Provide direct, functional and safe pedestrian pathways through parking areas allowing for convenient connections to the sidewalks of abutting streets
 - b) Extend contrasting, durable concrete sidewalk paving treatments where pathways extend through vehicle circulation areas
 - c) Incorporate a hierarchy of primary and secondary pathway systems throughout the development site
 - d) Provide a minimum of 3 m wide sidewalks along primary pedestrian pathways at active building frontages to allow for tree clearances and weather protection overhangs



DP1.1 COMMON GUIDELINES

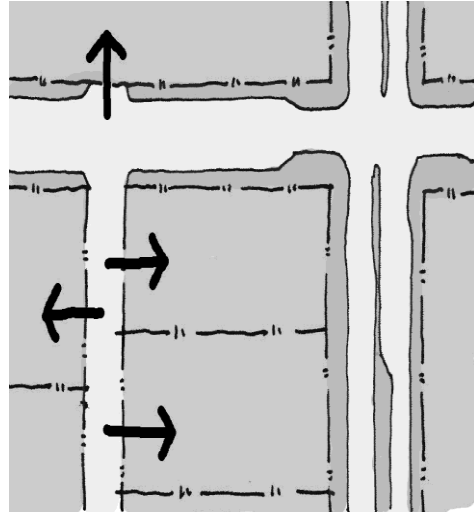
BL 20008

SITE DESIGN

Circulation—Vehicular

Within a development, the following elements shall be considered in the Site Design:

23. **ALL TYPES:** Enhance connectivity of public roads and lanes through development sites by establishing public thoroughfares for pedestrians, cyclists and vehicles.
24. **ALL TYPES:** Provide joint or shared driveway and ramp access between adjacent sites and consider driveway connections to future adjacent development sites.
25. **ALL TYPES:** Locate driveways off minor streets or lanes rather than off major streets to enhance a safe, walkable streetscape. *(illustr.)*
26. **ALL TYPES:** Where there is no alternative to providing access from a major street, locate and design driveways to minimize disruption of the pedestrian environment along the street by minimizing the number of driveways, by sharing access points and by maintaining a continuous street orientation of the development.
27. **ALL TYPES:** Where private roadways are proposed in-lieu of public streets, provide and design boulevard, landscaping and sidewalk treatments to be similar to or mimic those found along public roadways.
28. **ALL TYPES:** Consider accommodating residential and/or short-term parking along lanes rather than on streets where it tends to interrupt or interfere with pedestrian sidewalk movements.
29. **ALL TYPES:** Create markers at site perimeter driveway entrances.
30. **ALL TYPES:** For vehicular circulation in surface parking areas:
 - a) Provide vehicle connections between adjacent surface parking areas from neighbouring sites
 - b) Ensure parking is located away from street frontages or from street corners
 - c) Divide large surface parking areas into smaller sections defined by buildings or driveways with a sidewalk and landscaping on each side
 - d) Orient parking rows perpendicular to main entrances with a driveway along the building frontage for safer pedestrian movements
 - e) Locate accessible parking spaces close to main building entrances and/or elevator lobbies
 - b) Ensure accessible parking spaces are barrier-free by:
 - Including curb letdowns in practical locations
 - Locating any pole-mounted signage so it does not obstruct vehicle loading
 - Providing clearance from landscaping so it does not obstruct vehicle loading



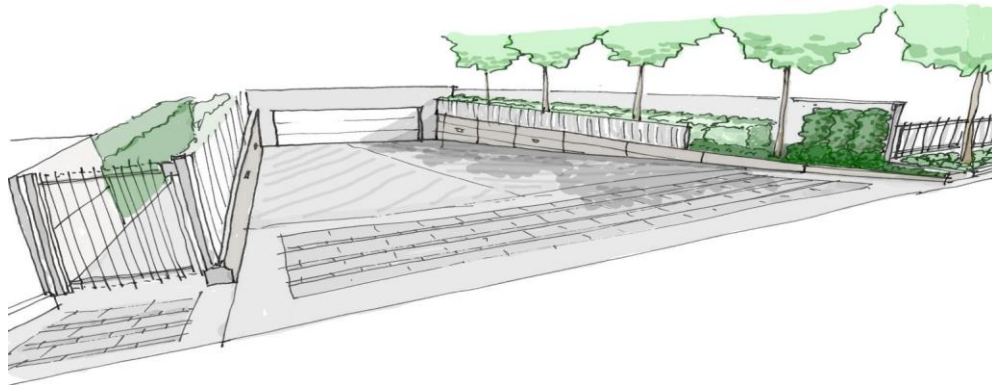
DP1.1 COMMON GUIDELINES

SITE DESIGN

Circulation—Vehicular (cont.)

Within a development, the following elements shall be considered in the Site Design:

31. **ALL TYPES:** For parking ramps *(illustr.)*:
- Where ramps cross sidewalks, provide a maximum five percent (5%) slope for the first 6 m of the ramp
 - Provide specialty paving for the first 6 m
 - Set parking ramps back from side property lines by a minimum of 1.5 m to allow for a land-scaped buffer
 - Treat sidewalls of ramps with specialty details and finishing (e.g. sand blasting with a reveal pattern).



32. **ALL TYPES:** For site loading:
- Locate surface loading spaces away from public views; screen and pave with specialty materials
 - Loading may need to be fully enclosed to reduce acoustical impacts on adjacent residential areas
 - Gates may be necessary for closure during non-business hours
 - Maneuvering for loading should be provided and accommodated for completely on-site, not on streets.
33. **TOWNHOUSES:** Screen apron parking that is visible from the public realm.
34. **TOWNHOUSES:** Use side-by-side parking as the main provision for parking and specifically minimize the use of tandem parking.
35. **MULTIPLE/MIXED/COMMERCIAL:** For corner store residential units, locate surface residential parking close to the residential entrance for convenience and security.
36. **INDUSTRIAL and BUSINESS:** Reduce conflicts between heavy vehicles and traffic from visitors and employees.
37. **INDUSTRIAL and BUSINESS:** Locate movements for heavy vehicles away from residential interfaces, where possible, and incorporate features to mitigate noise, fumes and visual intrusion; in some cases, an impact study may be required.

DP1.1 COMMON GUIDELINES

SITE DESIGN

Site Landscaping

Within a development, the following elements shall be considered in the Site Design:

- 38. **ALL TYPES:** Identify, preserve and incorporate healthy stands of mature trees into the overall site landscaping design. *(illustr.)*
- 39. **ALL TYPES:** Retain unique or culturally or historically important trees or vegetation and nesting areas on the development site.
- 40. **ALL TYPES:** Provide a coordinated group of furnishings to match the overall character of the development and site features, (e.g. bike racks, signs, garbage enclosures, benches, waste receptacles and tree grates).
- 41. **ALL TYPES:** Site landscaping design should consider the context of surrounding properties where there is a dominant pattern along the street (e.g. provide tree rows and landscaping consistent with the established or emerging standards along the street or within an adjacent neighbourhood).
- 42. **ALL TYPES:** Use landscaping and landscape materials to conform to the most current version of the British Columbia Society of Landscape Architects/British Columbia Landscaping and Nursery Association's *British Columbia Landscape Standards* (as amended).
- 43. **ALL TYPES:** Ensure sufficient in-ground planters are provided for full, healthy tree growth. *(illustr.)*
- 44. **ALL TYPES:** Locate trees no closer than 2 m from a building face, building foundation or retaining wall. Use only small growing tree species between 2 m to 3 m from the building setback; use medium growing tree species between 3 m and 4 m; and use large growing tree species beyond 4 m.



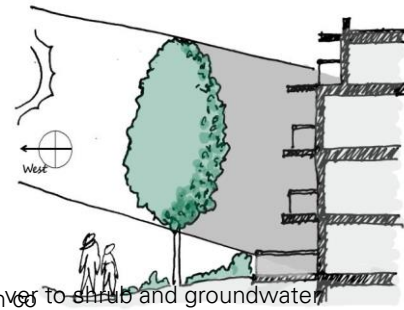
DP1.1 COMMON GUIDELINES

SITE DESIGN

Site Landscaping (cont.)

Within a development, the following elements shall be considered in the Site Design:

45. **ALL TYPES:** Site landscaping should incorporate environmentally sustainable features:
- Minimize stormwater runoff and reduce heat island effects (e.g. increase the amount of land-scaped areas on-site and decrease the amount of impervious paved surfaces)
 - Take energy efficiency into consideration in the overall design of landscaped areas. Use designs that moderate the effect of wind and solar impacts on buildings (e.g. use deciduous trees along western and southern building frontages to address passive solar gains and allow for winter gains) (*illustr.*)
 - Minimize high water use mown turf areas. Strive to limit mown turf areas to a maximum of 25-50% of the total landscaped area of one site
 - Reduce evaporation from soil by providing mulches to shrubs and groundwaters
 - Use the highest quality plant material possible and in a scale and massing that will have an obvious impact; maximize the function and aesthetic qualities of landscaping for open spaces
 - Select plants that require less need for chemical interventions in pest and weed control.
46. **ALL TYPES:** For landscaping in surface parking areas:
- Locate curbed landscape islands throughout parking areas in order to define parking clusters, visually break-up and screen the parking area, highlight pedestrian routes, increase human comfort, provide shade and manage stormwater opportunities
 - Locate landscaped islands as such: between, and at the end of, parking rows; one landscaped island along the side of a parking space, at the end of every six spaces in a single row; one land-scaped island, at a minimum of 3.6 m in length, located along the end of each group of 6 paired spaces (e.g. 12 spaces in two adjoining rows of 6 spaces, each front to front)
 - Cluster small car spaces with left over areas used for a landscaped island
 - Provide a minimum of 1.5 m radius for tree root balls in landscaped islands. Where a 1.5 m radius cannot be provided, the minimum landscape strip width should be not less than 1 m. Structural soil should be used under paved areas for the smaller landscaped strip to protect the roots from vehicles
 - Incorporate wheel stops or other devices in parking spaces to prevent landscaping and tree trunks from vehicle damage
 - Maximize tree spacing appropriately to correspond with the mature size of the tree species being used; allow for at least one tree in each island
 - Use single stem deciduous shade trees with a minimum of 5 cm caliper or larger, with tree canopies that begin no less than 2 m above grade
 - Use a mix of #1, #2 and #5 plant pot sizes for shrubs in landscaped islands as well as a mix of evergreen trees or plant material that will provide for year-round variety
 - Allow for visual surveillance through the site between the low shrubs screening the bottom half of the vehicles and the bottom of the tree canopy
 - Locate lower shrub plantings at crossing points to ensure visibility between vehicles and pedestrians at crossings.



DP1.1 COMMON GUIDELINES

BL 21389

SITE DESIGN

Site Landscaping (cont.)

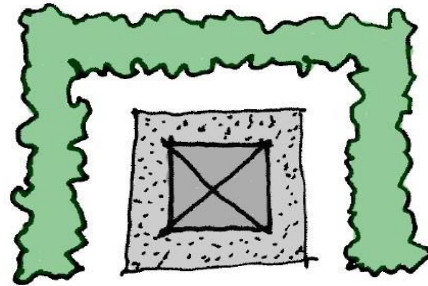
Within a development, the following elements shall be considered in the Site Design:

- 47. **TOWNHOUSES:** Provide landscaping at each individual entrance and between parking along private driveways.
- 48. **INDUSTRIAL and BUSINESS:** Locate a stormwater management swale in the front yard setback with associated planting.
- 49. **INDUSTRIAL and BUSINESS:** Provide sufficient landscape buffering where adjacent to residential development, include features such as berms, solid residential-type fencing where it will be visible and dense conifer planting.
- 50. **INDUSTRIAL and BUSINESS:** Where security fencing is necessary, avoid barbed or razor wire; work to integrate hedging along a fence line.

Site Services

Within a development, the following elements shall be considered in the Site Design:

- 51. **ALL TYPES:** Provide screening of and locate electrical kiosks and gas meters away from the visible public realm. *(illustr.)*



- 52. **ALL TYPES:** Where refuse containers cannot be located in underground facilities, locate enclosures away from the visible public realm.

- 53. **ALL TYPES:** Design refuse enclosures to coordinate with the overall design of the development and use the same high quality, durable materials as the rest of the site. Refuse container storage should have secure gates and a roof.



- 54. **ALL TYPES:** Provide a room or area large enough to allow for multiple types of refuse containers. Ensure sufficient room is created on-site to accommodate composting and recycling facility as well as garbage disposal.

- 55. **ALL TYPES:** Locate underground parking mechanical vents away from the visible public realm and make flush with grade.

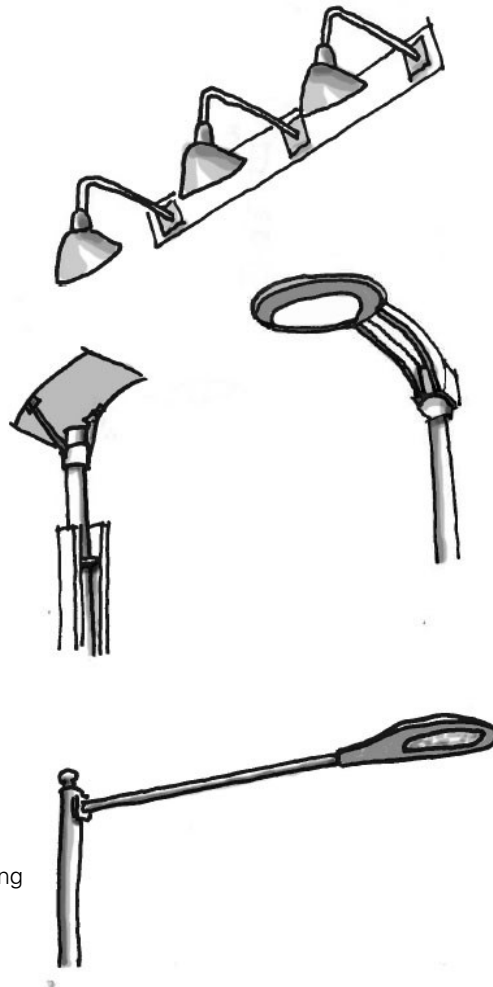
DP1.1 COMMON GUIDELINES

SITE DESIGN

Site Lighting

Within a development, the following elements shall be considered in the Site Design:

- 56. **ALL TYPES:** Incorporate neighbourhood character elements into on-site lighting design.
- 57. **ALL TYPES:** Provide a hierarchy of different lighting types with a coordinated appearance (e.g. lower- scale pedestrian pathways, parking areas, drive aisles, building and site entrances, and larger-scale parking lot lighting).
- 58. **ALL TYPES:** Coordinate the location of lighting with other landscape elements (e.g. trees).
- 59. **ALL TYPES:** Ensure overall site lighting enhances public safety and accommodates for safe ease of pedestrian movements to and from the development site.
- 60. **ALL TYPES:** Incorporate energy efficiency into the overall lighting plan.
- 61. **ALL TYPES:** Design and install lights that are vandal proof. Take maintenance accommodations into consideration when designing development site lighting plans.
- 62. **ALL TYPES:** Provide exterior street lighting that follows the *International Dark Sky Model* (as amended) in order to limit light pollution and to reduce light overspill into any adjacent residential areas. (*illustr.*).
- 63. **COMMERCIAL:** Incorporate lighting poles fitted for hanging baskets with associated irrigation and banners.
- 64. **INDUSTRIAL and BUSINESS:** Locate lighting to assist visual surveillance including site security (e.g. monitored cameras).



DP1.1 COMMON GUIDELINES

BL 19946

SITE DESIGN

Shared Amenity Space

Within a development the following elements shall be considered in the Site Design:

- 65. **ALL TYPES:** Provide sufficient outdoor amenity space at a scale consistent with the size of the development and in consolidated, usable spaces.
- 66. **ALL TYPES:** Provide a covered, weather protected outdoor seating area.
- 67. **ALL TYPES:** Provide shade trees at seating areas for summer comfort.
- 68. **ALL TYPES:** Provide a variety of passive and active program spaces.
- 69. **ALL TYPES:** Provide opportunities and support facilities for urban agriculture (e.g. provide a water source, tool storage and composting facilities).
- 70. **ALL TYPES:** Locate outdoor amenity spaces near indoor amenity rooms, entrances or staff areas, (e.g. lunch rooms) to encourage the observation and engagement of outdoor spaces.
- 71. **ALL TYPES:** Provide forms of hard and soft landscaping (e.g. constructed planters, gazebos, trellises and pergolas) to enhance the usability of decks, balconies and other outdoor amenity spaces.
- 72. **ALL TYPES:** Use roof tops as an additional outdoor amenity space.
- 73. **RESIDENTIAL:** Secure play areas for children that are located where they can be easily observed and that allow for a variety of play experiences. Children play areas should be landscaped and paved for surface play.
- 74.1 **TOWNHOUSES:** Outdoor amenity space should be useable for play and activities; and for townhouse developments less than 25 units, should not include tree protection areas.
- 74.2 **RESIDENTIAL:** Provide a minimum of one indoor amenity per residential building.
- 74.3 **MID-RISE RESIDENTIAL:** Provide a minimum of one indoor amenity per residential building or per 80 grouped residential units, whichever is less.

Publically Accessible Open Space

Within a development the following elements shall be considered in the Site Design:

- 75. **ALL TYPES:** Create publically accessible open spaces on private property along streets, lanes and public thoroughfares to add amenity spaces and enjoyment to urban areas. *(illustr.)*
- 76. **ALL TYPES:** The amount and scale of open space provided should reflect both the density of the neighbourhood and of the immediate area (e.g. on larger sites in denser neighbourhoods, larger open spaces should be provided).
- 77. **ALL TYPES:** Publically accessible open space should be added, in addition to any shared outdoor amenity space



DP1.1 COMMON GUIDELINES

SITE DESIGN

Publically Accessible Open Space (cont.)

Within a development the following elements shall be considered in the Site Design:

78. **ALL TYPES:** Open spaces should be defined as to its purpose and function by:
 - a) Incorporating passive activities in the design (e.g. providing seating as a basic function)
 - b) Contributing to placemaking by creating a focal point for visual interest
 - c) Enhancing the green, natural experience by incorporating stormwater management features into the design
 - d) Incorporating other functions into larger open spaces (e.g. playground areas).

79. **ALL TYPES:** Open spaces should be located with consideration given to the surrounding area and site by:
 - a) Linking with other open spaces in the area
 - b) Taking advantage of any unique vegetation, views or riparian areas
 - c) Responding to access for all seasons (e.g. lunch time for area workers)
 - d) Responding to wind (e.g. by providing shelter)
 - e) Reducing the impact of noisy locations (e.g. by mitigating the negative impacts of traffic by using water features)
 - f) Shaping open spaces to respond to a surrounding context (e.g. by widening a promenade in order to reinforce a street or by extending a corner plaza to link with a main building entrance)
 - g) Maximizing public access to open spaces by locating them along at least two public streets
 - h) Locating rest areas adjacent to a greenway and along highly visible locations (e.g. at intersections with connecting streets or other greenways).

80. **ALL TYPES:** Incorporate detailed design and landscape elements:
 - a) Define public, private and semi-public spaces adjacent to open spaces by using low landscaping
 - b) Incorporate local character, cultural themes or art work as a focal point or as an integral part of the overall design
 - c) Plant large trees, in clusters if possible, that have adequate growing medium and space, particularly where the trees are located on parking structures
 - d) Add shade trees suitable to prevent summer overheating
 - e) Create opportunities for open space uses, especially for children to engage in nature within an urban setting
 - f) Use trees, shrubs, ground covers, vines and flowers in a variety of colors and textures to reflect seasonal changes and variation
 - g) Install lawns to help visually soften the surrounding urban environment and to serve as an inviting and effective dry-weather seating area
 - h) Consider adding electrical and water access points for events held in larger open space areas
 - i) Use surface materials that allow for universal accessibility, particularly allowing for easy access by the elderly or disabled
 - j) Provide integrated stormwater features within landscaped areas
 - k) Ensure material selection, design, layout and function of the space is done with the objective of maintaining long term attractiveness
 - l) Incorporate subtle pedestrian-scale lighting into the character and overall design of the open space.

DP1.1 COMMON GUIDELINES

SITE DESIGN

Publically Accessible Open Space (cont.)

Within a development the following elements shall be considered in the Site Design:

81. **ALL TYPES:** Program the open space to be active and safe by:
- a) Designing the open space to connect with people moving through the area, including along de- sire lines
 - b) Maximizing surveillance of open space areas, (e.g. by orienting main entrances, townhouses or shop fronts onto the open space and by avoiding interfacing open spaces with parking lots)
 - c) Encouraging indoor uses to spill out onto open spaces (e.g. by creating informal outdoor seating areas and by creating space for the commercial display of goods)
 - d) Providing good visibility from surrounding public areas or streets by: minimizing walls, minimiz- ing raised planters, locating plazas at street level, creating seamless connections to streets and by creating clear sightlines across the open space (*illustr.*)



- e) Considering all users of a space (e.g. consider how unintended users such as skateboarders, may conflict with passive users)
- f) Considering the quality of the open space at different times of the day and week and for different seasons (e.g. by using lighting to adjust comfort in an open space).

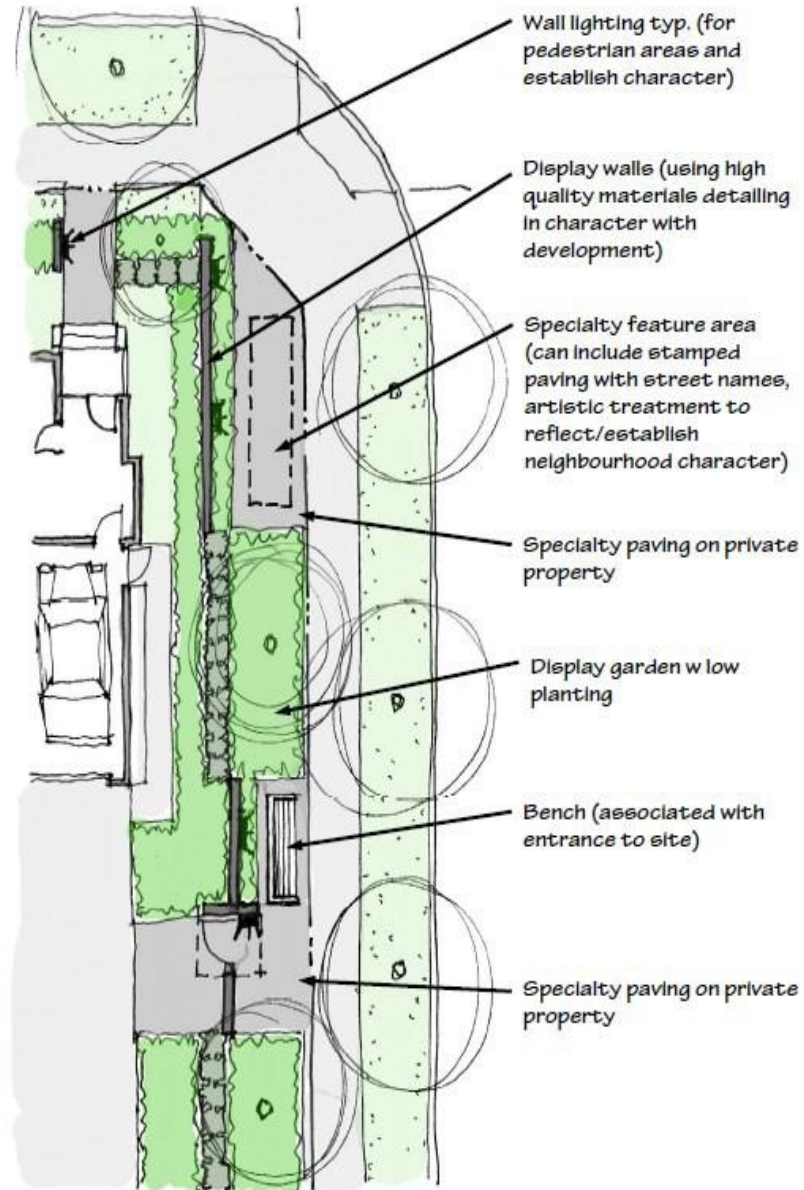
DP1.1 COMMON GUIDELINES

SITE DESIGN

Publically Accessible Open Space (cont.)

Within a development the following elements shall be considered in the Site Design:

- 82. **TOWNHOUSES:** Create a publically accessible open space at street corners. *(illustr.)*



DP1.1 COMMON GUIDELINES

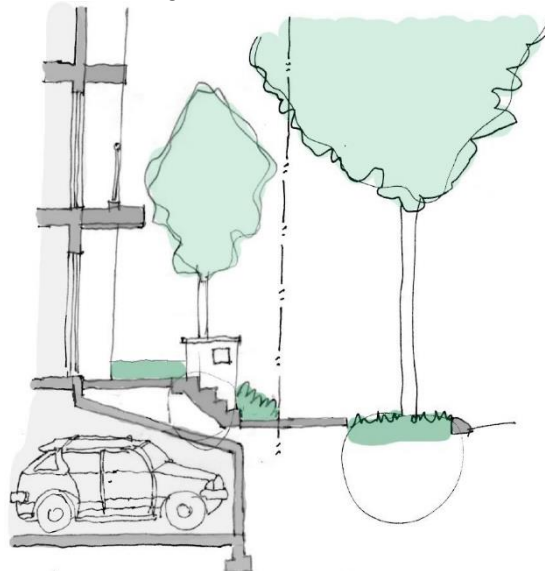
BL 19765

SITE DESIGN

Public Realm and Street Interface

Within a development, the following elements shall be considered in the Site Design:

- 83. **ALL TYPES:** Enhance beautification opportunities along prominent transportation routes, heritage streets, boundary streets and City gateways.
- 84. **ALL TYPES:** To better coordinate the interface between public and private property, include information on design drawings about off-site and adjoining public property elements (e.g. show sidewalks and boulevard details designed to City standards for reference). Set the ultimate grade requirements of adjacent public property at the early stages of design.
- 85. **ALL TYPES:** Coordinate continuous front yard setbacks along streets, particularly where buildings abut one another with no side yard, such as in Town Centres or along prominent shopping streets.
- 86. **ALL TYPES:** Enrich the public realm and street interface with distinctive character elements such as art features and cultural and historical references.
- 87. **ALL TYPES:** Underground parking should be set back and lowered, especially where visible in the public realm, so as to not project above grades; include allowances for tree roots, soil and paving depths.
- 88. **ALL TYPES:** Trees should be set back 0.5 m from any public thoroughfare property line to facilitate access, maintenance and root growth.
- 89. **ALL TYPES:** Where driveway entrances divert to each side, consider the axial view down the driveway by providing a landscaped feature or marker at the end of the driveway.
- 90. **ALL TYPES:** Screen all parking areas visible from the street with a landscaped buffer a minimum of 3 m wide. Provide a minimum of a double staggered row of evergreen shrubs, with a minimum height of 1 m, and deciduous trees with canopies starting at a minimum of 2 m above grade, spaced to meet the mature tree size. Landscaped buffers for shared vehicle parking stalls should also be a minimum of 3 m wide but landscaping should not obstruct visibility of the stalls from the street or public walkways. Provide low, open, decorative fencing with solid posts using durable materials such as masonry, and incorporate it to better define parking areas and focused pedestrian routes throughout the site.
- 91. **RESIDENTIAL:** Step planters up to raised patios with a maximum of 0.6 m wall height with high quality material facing such as masonry (e.g. stone or brick) or specialty concrete and low, layered planting in front of the wall. *(illustr.)*
- 92. **RESIDENTIAL:** Enhance each individual entrance with a tree planted in ground and specialty treatments as gate markers.
- 93. **RESIDENTIAL:** Enhance the progression from the sidewalk to the front door by lining up stairs straight



DP1.1 COMMON GUIDELINES

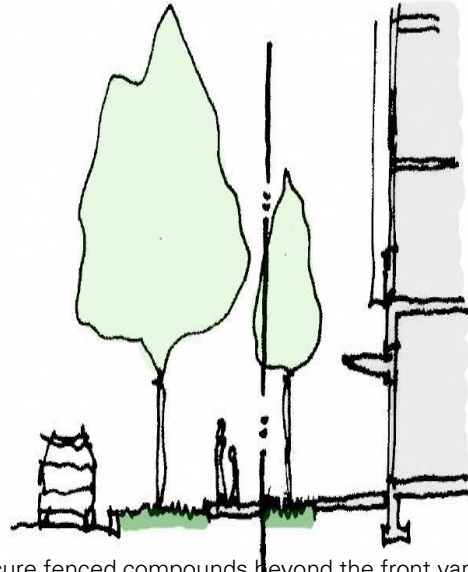
SITE DESIGN

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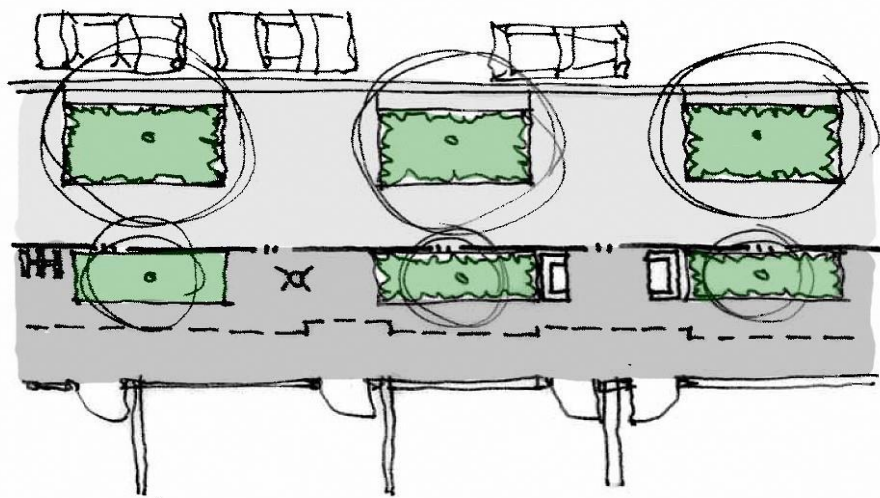
Public Realm and Street Interface *(cont.)*

Within a development the following elements shall be considered in the Site Design:

- 94. **COMMERCIAL:** Incorporate the following elements *(illustr.)*:
 - a) Seamless interfaces should be created across public and private property without using steps or retaining walls
 - b) In-ground planting should be provided along street edges; avoid the use of raised planters
 - c) Reinforce a formal tree-lined promenade along a street by providing an inside row of trees, on private property, where there is a setback of 2.5 m or more.



- 95. **INDUSTRIAL and BUSINESS:** Define and secure fenced compounds beyond the front yard to avoid having a fenced front yard.
- 96. **INDUSTRIAL and BUSINESS:** Provide a setback from the property line to allow for landscaped buffers along the building. *(illustr.)*



DP1.1 COMMON GUIDELINES

BUILDING FORM

Building Ground Plane Interface

Within a development the following elements shall be considered in the Building Form:

- 97. **ALL TYPES:** Step building ground floor levels to match the adjacent sidewalk grades on sloping sites. *(illustr.)*
- 98. **ALL TYPES:** Maximize the number of individual entrances to the street and public areas. *(illustr.)*



- 99. **ALL TYPES:** Create a strong sense of main entrance that is architecturally integrated with the building and that is facing the primary street.
- 100. **ALL TYPES:** Set main building entrances at the sidewalk grade without the need for transitions such as steps or ramps. Incorporate steps or ramps inside the main entrance lobby.
- 101. **ALL TYPES:** Locate active uses to face streets and non-active uses to face away from streets to avoid blank walls facing the public realm.
- 102. **ALL TYPES:** Provide a setback from the property line to allow for a landscape buffer along the building. Use facade variations, textured surfaces, architectural detailing or graphics and colours to reduce visual impact of blank walls.
- 103. **ALL TYPES:** Incorporate lighting into the building to enhance entrances, adjacent streets and public spaces for pedestrians.
- 104. **ALL TYPES:** Establish natural surveillance opportunities along publically accessible circulation routes by encouraging active uses to locate adjacent to the edge.
- 105. **RESIDENTIAL:** Specific to residential interfaces, the following elements should be applied:
 - a) Orient the main floor levels with the adjacent sidewalk grade, setting at a minimum of 0.6 m and a maximum of 1.2 m above grade
 - b) Express a strong individual entry porch at the street level with weather protection over each entrance
 - c) Orient front doors and front porches to face the street.
- 106. **TOWNHOUSES:** Incorporate a habitable room associated with the front entrance for units facing a street or public thoroughfare.

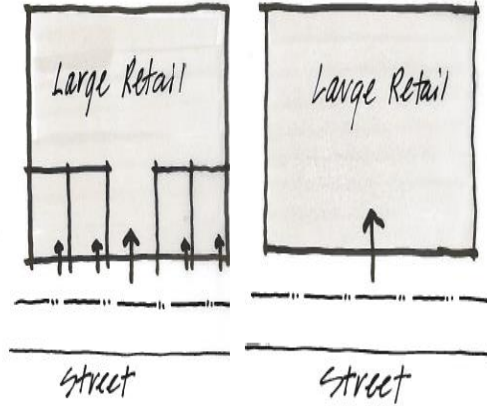
DP1.1 COMMON GUIDELINES

BUILDING FORM

Building Ground Plane Interface (cont.)

Within a development the following elements shall be considered in the Building Form:

- 107. **COMMERCIAL:** Incorporate the following elements:
 - a) Maximize active uses (e.g. shop fronts) along the public interface, including along streets and public thoroughfares
 - b) Orient primary retail or commercial unit entrances towards the street rather than to parking areas
 - c) Locate large retail units away from street edges. Where large retail buildings along street edges cannot be avoided, infill smaller retail units along these frontages (*illustr.*)
 - d) Maximize retail/commercial glazing at the street frontage and avoid overhanging building arcades.



- 108. **COMMERCIAL:** The following elements should be applied (*illustr.*):
 - a) Express the individuality of each unit with unique features to break down the scale of long facades
 - b) Provide a finer grain of detailing of ground level frontages to add interest and character
 - c) Provide deeper weather protection on buildings that are adjacent to transit stops.



- 109. **INDUSTRIAL and BUSINESS:** Locate loading doors away from public realm interfaces.
- 110. **INDUSTRIAL and BUSINESS:** Where there is no alternative to locating doors away from facing streets, provide specialty design of loading doors and integrate design into the architectural elements of the overall building.
- 111. **INDUSTRIAL and BUSINESS:** Express the ground floor along the street architecturally to establish scale.

DP1.1 COMMON GUIDELINES

BUILDING FORM

Building Massing

Within a development the following elements shall be considered in the Building Form:

112. **ALL TYPES:** Create a comparable building scale and massing with the established and/or known future neighbourhood.

113. **ALL TYPES:** Follow incremental development of an area over time to avoid sudden large-scale changes.

114. **ALL TYPES:** Orient and shape buildings to reduce shadow impacts on outdoor spaces. *(illustr.)*

115. **ALL TYPES:** Create building forms with a strong street enclosure, particularly at corners.

116. **ALL TYPES:** Locate higher building forms along more prominent streets and at corners.

117. **ALL TYPES:** Address prominent and axial sites by shaping buildings for their visual prominence and potential as reference points or landmarks.

118. **ALL TYPES:** Reduce the visual impact and massing of enclosed elevator shafts with architectural treatments.

119. **ALL TYPES:** Express vertical circulation (e.g. stairs and atria) as an architectural element.

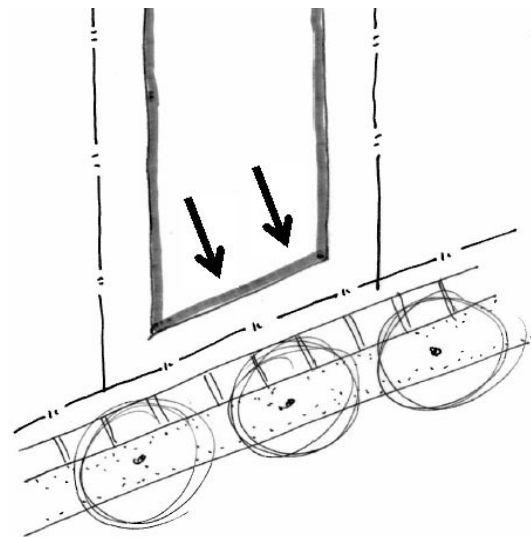
120. **ALL TYPES:** Enhance large, flat expanses of roof (whether actively used or not) with texture, colour and/ or landscaping.

121. **ALL TYPES:** Orient buildings to line streets including angled and curved streets. *(illustr.)*

122. **ALL TYPES:** For wider streets (e.g. arterials), create a higher podium to respond to street enclosure proportion.

123. **ALL TYPES:** Where higher density forms are proposed adjacent to or across the street from single family areas, design and scale the building size and massing down to that of the single family forms.

124. **ALL TYPES:** Locate elevators internal to the building and incorporate the mechanical penthouse into roof forms.



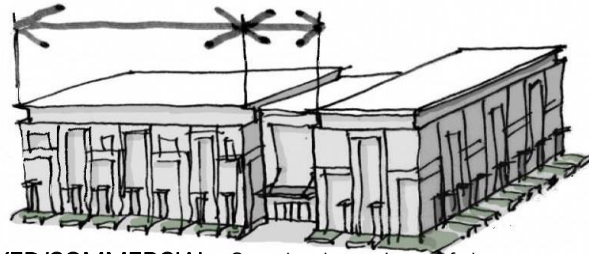
DP1.1 COMMON GUIDELINES

BUILDING FORM

Building Massing (cont.)

Within a development the following elements shall be considered in the Building Form:

- 125. **TOWNHOUSE:** Scale townhouse buildings with multiple units to a maximum footprint of 600 sq. m or six units.
- 126. **RESIDENTIAL:** Articulate larger roof forms by varying height and using dormers.
- 127. **MULTIPLE/MIXED/COMMERCIAL:** Visually scale down the length of the podium massing to 50 m by stepping down the roof forms and indenting the facade. *(illustr.)*

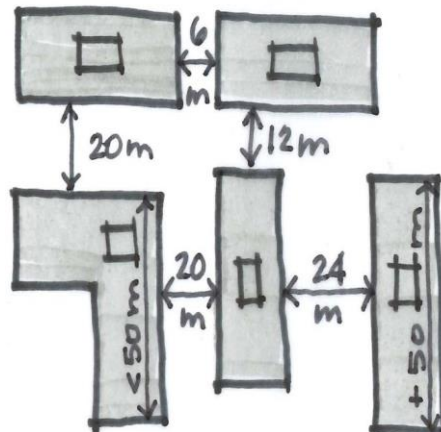


- 128. **MULTIPLE/MIXED/COMMERCIAL:** Step back portions of the upper storeys for buildings higher than 3 storeys. The lower storeys should maintain street enclosure. Where townhouses are incorporated into the base of the building, express the 2 storey form in the lower tower floors. *(illustr.)*

- 129. **RETAIL and COMMERCIAL:** Provide an enclosed service corridor at the rear of multi-tenant buildings to avoid the need for an outdoor walkway, particularly where visible to the public realm or adjacent to residential uses.

- 130.a **LOW RISE FORMS:** Optimize views, light and air for units in low rise buildings by providing minimum separations between buildings to achieve the following:

1) between a building side and another building side—minimum of 6 m; 2) between a building side and a building front—minimum of 12 m; between a building front and another building front—minimum of 20 m when one of the buildings is 50 m wide or less and a minimum of 24 m wide when one of the buildings is more than 50 m wide. *(illustr.)*



- 130.b **MID RISE FORMS:** Tower separations from corner-to-corner should be a minimum of 20 m and from face-to-face a minimum of 30 m.

- 131. **HIGH RISE FORMS:** Cluster high-rise towers with the highest forms in the core and taper out to lower forms at the outside of the cluster.

- 132. **HIGH RISE FORMS:** Minimize the east-west dimension width to 28 m for shadowing.

BL 19364
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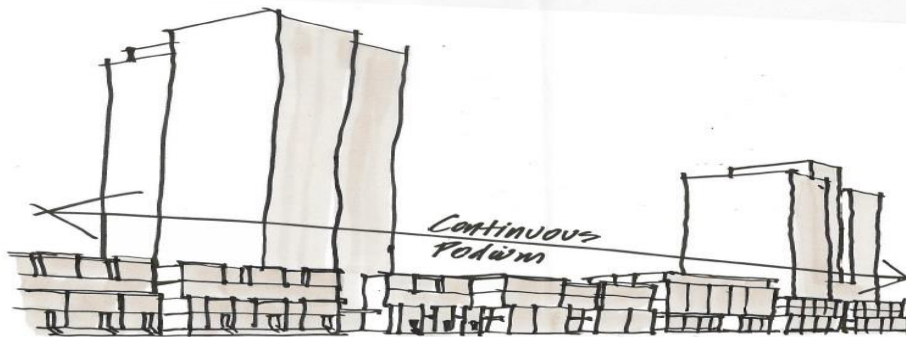
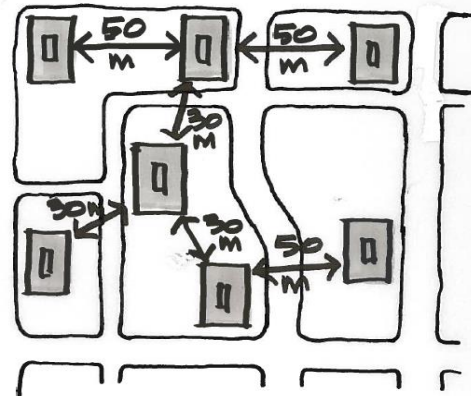
DP1.1 COMMON GUIDELINES

BUILDING FORM

Building Massing (cont.)

Within a development the following elements shall be considered in the Building Form:

- 133. **HIGH RISE FORMS:** Scale residential floors relative to tower height with high-rise floor plates at a: 1) maximum of 600 sq. m for buildings up to 30 storeys; 2) maximum of 650 sq. m for buildings between 31—40 storeys; 3) maximum 700 sq. m for buildings 41—50 storeys; 4) and maximum 750 sq. m for buildings 51 storeys and higher.
- 134. **HIGH RISE FORMS:** Set tower orthogonal to the street for the majority of sites.
- 134.a **HIGH RISE FORMS:** Tower separation is a minimum of 30 m from corner-to-corner and a minimum of 50 m from face-to-face. *(illustr.)*
- 135. **HIGH RISE FORMS:** Locate towers to allow views through from surrounding sites and from within the site for multi-tower sites.
- 136. **HIGH RISE FORMS:** Incorporate features which reduce the impacts of wind (e.g. balconies and articulation on tower forms which capture and slow the wind at upper levels).
- 137. **HIGH RISE FORMS:** Reinforce street enclosure by setting towers close to the street with approximately 4 m set back from the podium face and integrating the tower as it meets the podium form.
- 138. **HIGH RISE FORMS:** Form tower tops into an expression of the roof function (e.g. roof gardens).
- 139. **HIGH RISE FORMS:** Create a 4-6 storey podium as a base to the higher tower forms and reinforce street enclosure. *(illustr.)*



- 140. **INDUSTRIAL and BUSINESS:** Locate buildings on sites to allow views through from upland residential areas and to screen views of any outdoor storage areas.
- 141. **INDUSTRIAL and BUSINESS:** Express the different functions of the building (e.g. entrances, second floor office areas, and warehouses) as distinct forms by varying the parapet heights and by stepping forms rather than using an arbitrary application of building variations.
- 142. **INDUSTRIAL and BUSINESS:** Ancillary or secondary buildings should be designed to the same architectural level as the principal building.

DP1.1 COMMON GUIDELINES

BUILDING FORM

Architectural Character, Treatment and Materials

Within a development the following elements shall be considered in the Building Form:

143. **ALL TYPES:** Select exterior building materials for their functional and aesthetic quality. These materials should exhibit high qualities of workmanship, durability, longevity and ease of maintenance.
144. **ALL TYPES:** Continue higher quality materials used on the principal façade around any building corner or edge which is visible to the public. (*illustr.*)



145. **ALL TYPES:** Avoid the excessive use of monotone colour palettes.
147. **ALL TYPES:** Design any visible side walls with visual interest by using such features as texture, color, graphics, wall art and lighting.
147. **ALL TYPES:** For underground parking and above-ground parkades:
- Where interconnected between sites, parking structures should be separate and secure
 - Separate large parking facilities into smaller secured areas
 - Provide direct access to the street for retail parking and access to the elevator in underground parking for visitors
 - Provide overhead gates to underground parking areas; secure residential parking from non-residential parking
 - Locate exit stairs from underground parking within the building envelope and make flush with the building face without alcoves
 - Freestanding exit/access stairs and elevators to parking should be fully enclosed, visibly open and expressed as an architectural element with vandal resistant materials
 - Design exit vestibules and elevator lobbies with clear visibility such as glass enclosures
 - Parking interiors should be painted with light colors to improve visibility and lighting efficiencies
 - Parking interiors should be visibly open with few walls, hidden corners and alcoves
 - Provide accessible common areas for storage, refuse and recycling material
 - Secure bicycle parking facilities, storage lockers and change rooms with showers
 - Above-ground parkades should be wrapped with active uses along public frontages. Where visible, design as a primary façade using durable, high quality materials with the main entrance strongly expressed
 - Control access to parkades with secured exit stairs and access to the ground level where only the parking ramp is left open allowing for tighter access control.

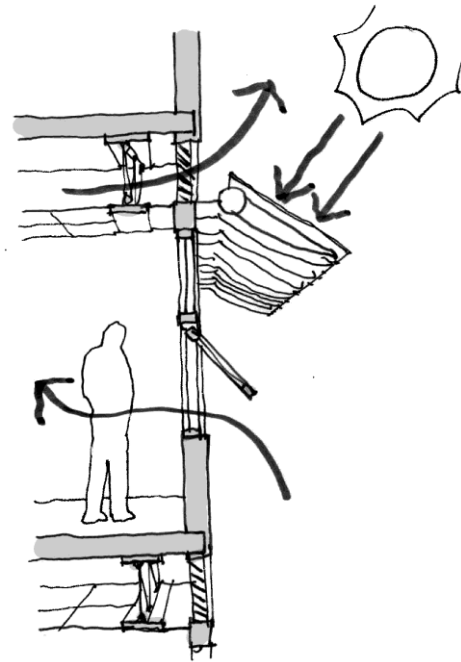
DP1.1 COMMON GUIDELINES

BUILDING FORM

Architectural Character, Treatment and Materials (cont.)

Within a development the following elements shall be considered in the Building Form:

- 148. **ALL TYPES:** Incorporate options for mounting cellular equipment and incorporate screening to avoid visual clutter on the tops of buildings, particularly for higher buildings.
- 149. **ALL TYPES:** Minimize the potential for bird collisions when siting buildings near existing landscape features and when planning new landscapes in close proximity to buildings by doing the following:
 - a) Creating visual markers on buildings by using a denser pattern of glass, applying patterns, film, decals, decorative grills and louvers to glass surfaces
 - b) Angling glass to project reflected images downward, and
 - c) Installing awnings or overhangs to cover windows on the ground floor of a building.
- 150. **ALL TYPES:** Indicate measures taken to enhance building performance and consider designing new buildings to a green building standard (e.g. LEED or equivalent).
- 151. **ALL TYPES:** Incorporate environmentally sustainable green building features such as:
 - a) Utilizing sustainable construction methods and materials, including the reuse, rehabilitation, restoration and recycling of building and/or building elements
 - b) Minimizing summer solar gain (e.g. south and west facing) by using roof overhangs, balconies, awnings, louvers, canopies and other window screening techniques *(illustr.)*
 - c) Maximizing winter solar gain, (e.g. by using solar water heating or solar mass walls)
 - d) Installing green roofs
 - e) Improving indoor air quality
 - f) Minimizing the impact of adverse weather on buildings and on the street-level microclimate (e.g. strive to reduce excessive heat by providing generous shade)
 - g) Reducing water consumption (e.g. by using reclaimed or recycled water or rainwater capture from roofs or rain barrels for outdoor use) when available, and as a substitute for potable water
 - h) Improving the energy performance and energy consumption of HVAC systems by using alternative energy supplies (e.g. photovoltaics and wind power).



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BUILDING FORM

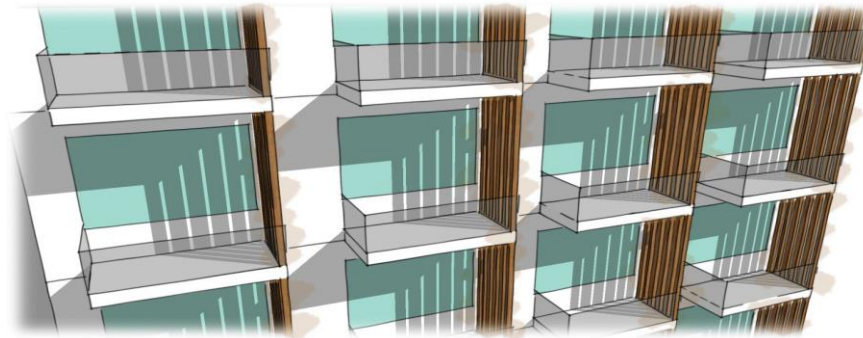
Architectural Character, Treatment and Materials (cont.)

Within a development the following elements shall be considered in the Building Form:

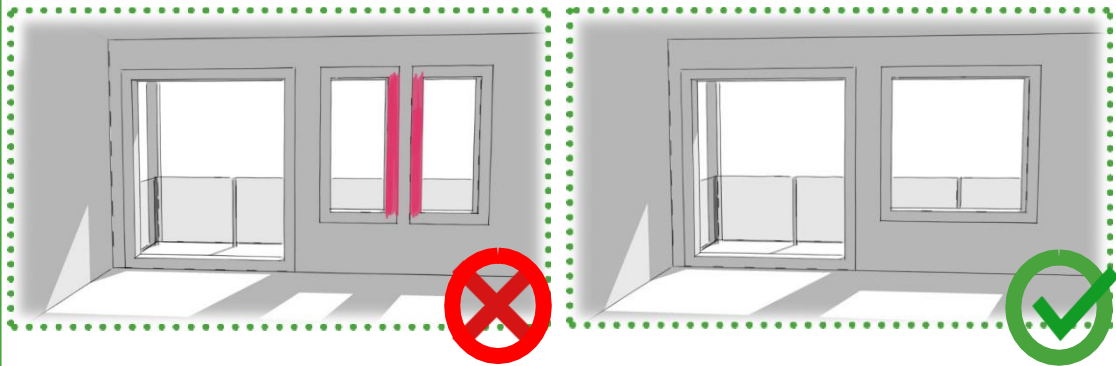
152. **ALL TYPES:** Incorporate energy efficient building design features such as:
- a) **Adjusting Compact Massing and Form**—minimize loss of heating energy by reducing the number of corners and joints use in a building by incorporating simple, solid shapes (e.g. rectangles or cubes) *(illustr.)*



- b) **Exterior Shading**—minimize unwanted solar gains during the summer (e.g. by using horizontal shading devices on the south and east facades) and optimize solar heat gain in the winter (e.g. by using vertical shading devices on west facades) *(illustr.)*



- c) **Window Frame Detailing**—mitigate heat loss by reducing the use of excessive or unnecessary design features (e.g. design buildings with fewer, larger windows that reduce excessive use of



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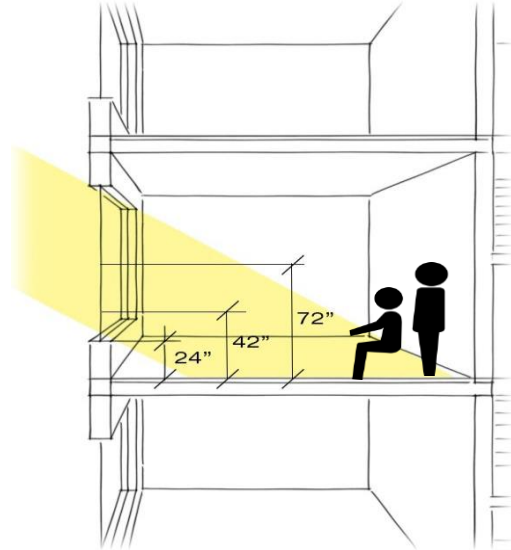
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BUILDING FORM

Architectural Character, Treatment and Materials (cont.)

Within a development the following elements shall be considered in the Building Form:

- d) **Adjusting Window Fenestration**—avoid floor-to-ceiling glazing by ensuring bottom window sill heights are placed at a minimum of 24 inches above the floor to block unnecessary solar radiation at foot level. *(illustr.)*
- e) **Thermal Bridging**—mitigate heat loss through the building envelope by constructing thermally-broken balconies from the building (e.g. ensure wall insulation is continuous, minimizing heat loss) *(illustr.)* or by eliminating exposed



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BUILDING FORM

Architectural Character, Treatment and Materials (cont.)

Within a development the following elements shall be considered in the Building Form:

153. **TOWNHOUSES:** Incorporate features on corner unit sides such as entrances, bay windows and roof articulation (e.g. gable ends). *(illustr.)*

154. **TOWNHOUSES:** Provide wider window trim and vary the type of materials used at different levels of the unit (e.g. use masonry at the base, siding on main levels and treat the gable ends with specialty materials).

155. **TOWNHOUSES:** Emphasize the vertical expression and identification of individual units.

156. **TOWNHOUSES:** Design firewalls to be incorporated into the overall building so they are not visible through roof forms.

157. **RESIDENTIAL:** Break down façades into smaller scale elements through the use of windows, bays, balconies and dormers.

158. **RETAIL and COMMERCIAL:** Pitched roof forms shall have full roof expression rather than being truncated.

159. **INDUSTRIAL and BUSINESS:** Prefabricated buildings are generally discouraged. Should they be used, significant building improvements will be required to achieve an adequate and acceptable architectural design. Incorporating extensive use of glass (e.g. transparent and spandrel) into the design may offset the solid nature of the buildings reaching a more acceptable design level.

160. **INDUSTRIAL and BUSINESS:** Consolidate roof mechanical units together and screen from views.



Residential Livability

Within a development the following elements shall be considered in the Building Form:

161. **ALL RESIDENTIAL:** For residential development directly abutting an arterial road, a report with recommendations prepared by persons trained in acoustics and current technologies of noise measurements shall be provided that demonstrates that noise levels for specific portions of a dwelling unit are not exceeded. Those noise levels and specific portions of a dwelling unit include:
- a) Bedrooms—35 decibels
 - b) Living, dining and recreation rooms—40 decibels
 - c) Kitchens, bathrooms and hallways—45 decibels.

For the purposes of this guideline, “Noise Level” is defined as the A-weighted, 24-hour equivalent (Leq) sound level defined in decibel levels.

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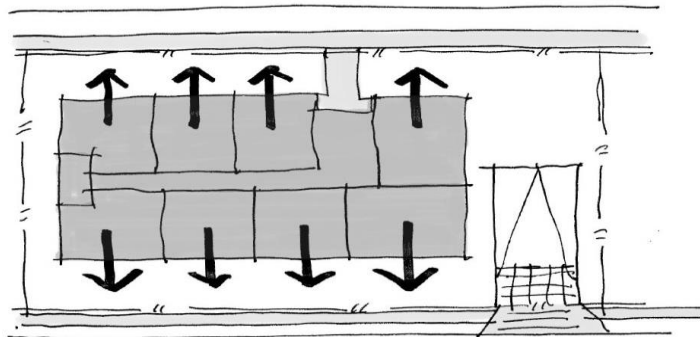
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BUILDING FORM

Residential Livability (cont.)

Within a development the following elements shall be considered in the Building Form:

- 162. **ALL RESIDENTIAL:** Take into account the negative acoustical impacts that can result when residential uses are located along roadways, (e.g. arterial roads) and when residential and non-residential uses are mixed within the same development. Use methods that will help to buffer residential units from external noises including:
 - a) Orienting bedrooms and outdoor areas away from obvious noise sources
 - b) Providing mechanical ventilation to allow for the choice of keeping windows closed
 - c) Enclosing balconies or using sound-absorptive materials and sound barriers
 - d) Using sound-deadening construction techniques or materials (e.g. concrete, acoustically-rated glazing or glass block walls)
 - e) Locating areas not affected by noise (e.g. stairwells and single-loaded corridors) between the noise source and the dwelling unit.
- 163. **ALL RESIDENTIAL:** Include a window in each bedroom to provide daylight and ventilation for occupants.
- 164. **TOWNHOUSES:** Incorporate privacy screening between patios and balconies.
- 165. **TOWNHOUSES:** Provide a minimum of 15 m for rear yard privacy between townhouse buildings.
- 166. **MULTIPLE/MIXED/COMMERCIAL:** Orient residential units to face front or rear yards, not side yards or parking ramps. (*illustr.*)



- 167. **MULTIPLE/MIXED/COMMERCIAL:** Design residential corridors with a maximum distance of 30 m from a unit to an elevator.
- 168. **MULTIPLE/MIXED/COMMERCIAL:** Provide an elevator for approximately every 70 units for convenience.
- 169. **MULTIPLE/MIXED/COMMERCIAL:** Where stairs are located at the elevator lobby and at the end of a corridor, design stairs to be visible, open and larger than the minimum to encourage walking. The number of units per elevator, as noted in DP Guideline 1.1.170, can be increased when building design encourages walking.
- 170. **MULTIPLE/MIXED/COMMERCIAL:** Where exit stairs and lobbies have exterior walls, incorporate windows for daylighting.
- 171. **MULTIPLE/MIXED/COMMERCIAL:** Locate mail boxes to be visible to residents. They should be fully visible to the residential elevator rather than located in an enclosed space.
- 172. **MULTIPLE/MIXED/COMMERCIAL:** In mixed-use developments, design buildings to ensure that each different use is self-contained and has a separate entrance with a focus on security

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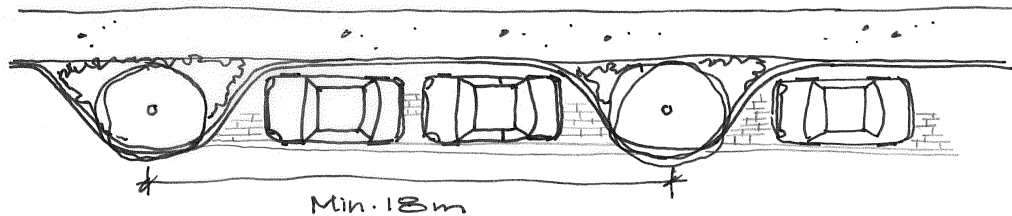
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SHARED VEHICLES

Location

Within a development, the following elements shall be considered for accommodating Shared Vehicles:

- 173. Locate shared vehicles at-grade on private property to achieve convenient access and visibility from the street or lane. Integrate shared vehicle parking stalls with landscaping without obstructing visibility.
- 174. Locate multiple shared vehicles together on each site.
- 175. Cluster parallel parking of shared vehicles, where provided in a Green Lane, to no more than two stalls unless landscaped islands are provided every 18 m. *(illustr.)*



- 176. Provide safe pedestrian access to shared stalls with a minimum 1.5 m walkway.
- 177. Ensure shared vehicle parking stalls do not impede the normal functions of a laneway (e.g. utility access, garbage collection, etc.).

Lighting

Within a development, the following elements shall be considered for accommodating Shared Vehicles:

- 178. Incorporate vandal-proof lighting in the landscaping to provide ease of access to shared vehicles with- out creating glare to adjacent residential units.

Signage

Within a development, the following elements shall be considered for accommodating Shared Vehicles:

- 179. Locate shared vehicle parking signs at the designated stall discreetly and without the use of backlit sign boxes.
- 180. When shared vehicles are located in a parking structure, provide signage at the parking structure entrance to identify where stalls are located. Sign specifications shall be in accordance with the Surrey Sign Bylaw, as amended.

Appearance

Within a development, the following elements shall be considered for accommodating Shared Vehicles:

- 181. Treat at-grade shared vehicle stalls with distinctive, durable paving (e.g. pavers) and incorporate into the site landscaping.
- 182. Minimize additional paved areas required for shared vehicle stalls by sharing drive-aisles with other parking areas and by maintaining efficient circulation.
- 183. Avoid building additional free-standing structures to accommodate shared vehicles in order to reduce building mass.

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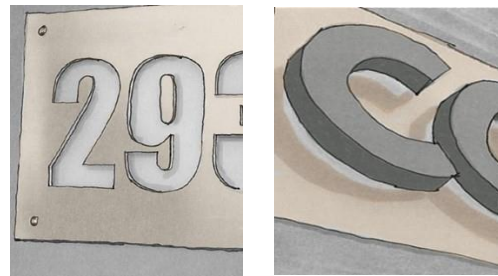
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SIGN GUIDELINES

Definitions

In conjunction with the City of Surrey Sign Bylaw, as amended, the following definitions outline and illustrate common signs related to Development Permit guidelines for Signs:

- Awning/ Canopy Sign** A sign painted on, attached to, or constructed on the surface of an awning or canopy, providing weather protection beyond a roof eave, that may include letters, characters, graphics and numbers.
- Fascia Sign** A sign displayed on a building face on a plane generally parallel to the face of a building.
- Freestanding Sign** A sign attached to the ground that is independent of any building or structure located on the same lot. Also known as Pylon Sign or Monument Sign.
- Individual Panels** On free-standing signs, the separate panels used by tenants to advertise within a multi-tenant building.
- Portable Changeable Copy Sign** Any sign not permanently attached to the ground or a building on which the information displayed can be changed manually or automatically using detachable letters, characters, numbers, pictorial panels or graphics.
- Projecting Sign** A sign attached to and projecting outward from the face of a building. Does not include wall, canopy or fascia signs. Also known as a Blade Sign. *(Illustr.)*
- Sandwich Board** Non-illuminated, portable sign having an "A" shape which is set upon the ground and has not external support elements.
- Sign Copy** Individual letters, characters, graphics or numbers on a sign either with or without lighting. Also known as Cut- Out, Channel, Stencil-Cut and/or Routed Letters. *(Illustr.)*



DP1.1 COMMON GUIDELINES

SIGN GUIDELINES

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General Design

In conjunction with the City of Surrey Sign Bylaw, as amended, the following design elements, where applicable, are required for sign installations:

- 184. All signage must conform to the regulations and standards as established in the City of Surrey Sign Bylaw, as amended.
- 185. Signs shall be made of durable, weather-resistant and high-quality materials.
- 186. All signs shall be professionally designed and fabricated with a high degree of design and aesthetics.
- 187. Signs shall consider the pedestrian realm as a priority in design, size and placement. *(Illustr.)*
- 188. A complete sign package is required to be submitted with building elevations for development permits. Sign packages should include signs that are directed toward the pedestrian realm with an appropriate scale and location suitable for comfortable pedestrian viewing (e.g. under-awning and projecting signs).
- 189. Where multiple signs exist on one property signs should have a coordinated design.
- 190. Where multiple signs exist on larger sites, sign size, location and design should ensure a coordinated hierarchy with a variety of sign types placed so as to reduce visual clutter and to reduce domination by anchor tenants.
- 191. Using contrast, or other design features, signs should be easily and universally read-able.
- 192. Building lighting bands should not dominate building architecture. *(Illustr.)*
Building lighting bands should be designed with the following features:
 - one muted colour with halo-type lighting versus using a back-lit box
 - lower level luminaries or bulbs to reduce potential glare
 - simple horizontal bands with no vertical banding
 - bands that are shorter in length and smaller in size.
- 193. All wiring should be located within an approved conduit which is integrated fully with a sign or that is completely concealed.



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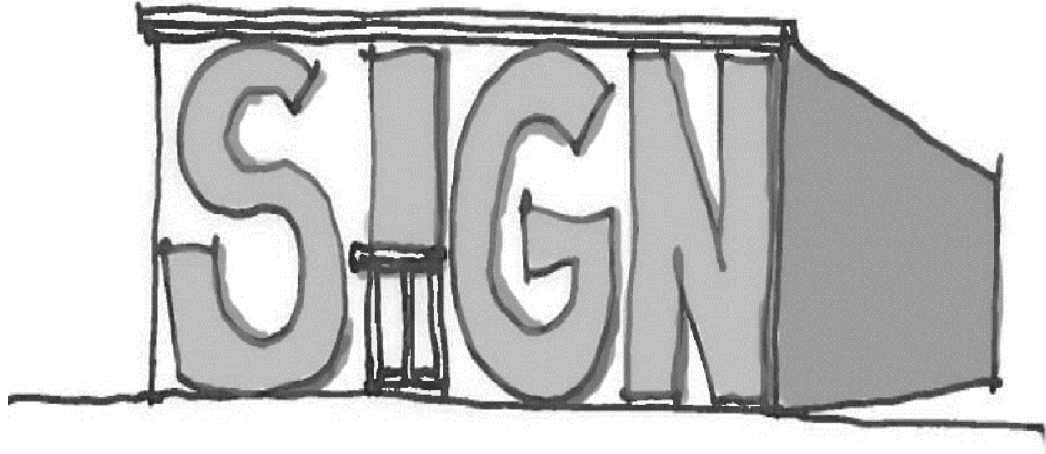
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SIGN GUIDELINES

General Design *(cont.)*

In conjunction with the City of Surrey Sign Bylaw, as amended, the following design elements, where applicable, are required for sign installations:

194. Signs should not dominate the architecture of the building. *(Illustr.)*

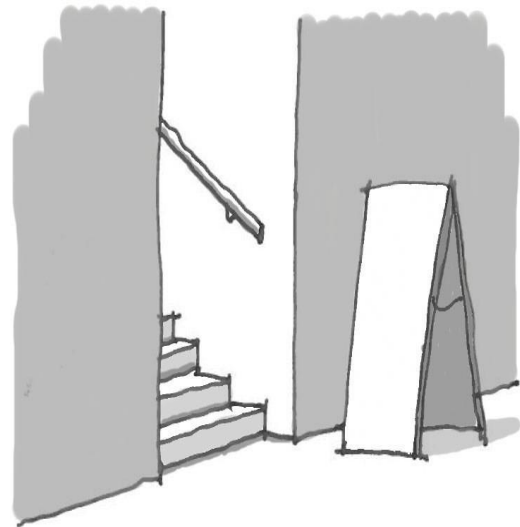


195. Sign lighting should be designed with the following features:
- cut-off shields to reduce light spill into residential areas
 - LED lighting to reduce overall glare
 - light sources that are not exposed, excludes neon tubes
 - elements that do not flash or that are animated
 - consistency with the lighting levels identified in the International Dark Sky Model
 - energy-efficient materials.

Sandwich Board

Within a development, the following elements shall be considered for Sign installations:

196. Primarily used for 2nd floor tenants located at the lower entrance areas.
197. No changeable copy signs are permitted on sandwich board signs.
198. Sandwich Board signs shall be coordinated with other on-site signage and used primarily for tenants located in upper floors. *(Illustr.)*
199. Shall be located to minimize interference with pedestrian thoroughfares, and are preferably located as close to the building as possible. Sandwich board signs are not permitted on public property.
200. Sign materials and installation shall be able to withstand the effects of wind and weather by using appropriate materials and weighting measures.



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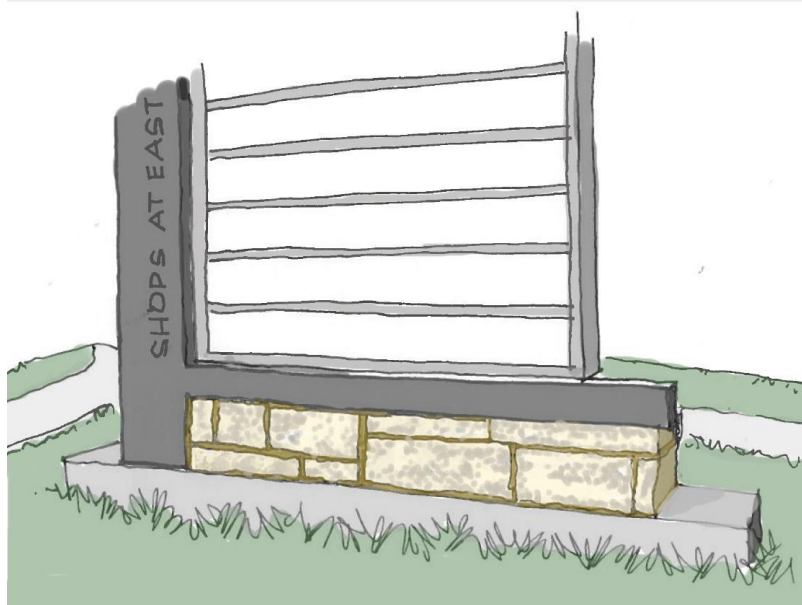
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SIGN GUIDELINES

Free-Standing

In conjunction with the City of Surrey Sign Bylaw, as amended, the following design elements, where applicable, are required for sign installations:

201. Businesses located at the rear of a property and not visible from the street shall be given signage priority in a multi-tenant freestyle sign. Where all businesses are visible, a free-standing sign is not encouraged.
202. Within the free-standing sign, the number of individual signs shall be minimized to reduce visual clutter. Six tenant signs per side should/shall be the maximum. (*Illustr.*)



203. Individual signs should be muted colour blank backing rather than white (when not leased). Bright colour schemes for individual signs shall be minimized.
204. Background colours should complement the architectural colour scheme of the building or overall development. When blank areas are present on a sign, those blank areas should also reflect the architectural colour scheme of the building or overall development rather than remaining white.
205. Design sign heights to be oriented toward pedestrian environments, particularly in areas evolving away from previously auto-oriented site designs. Signs should be designed as appropriate to the site and surrounding areas rather than automatically being constructed to meet the maximums as established in Surrey Sign Bylaw, as amended.
206. Use the top feature of a free-standing sign to tie the character of the sign with that of the architecture of the building or with other character elements of the surrounding neighbourhood.
207. Avoid the use of repeating the same style and design for sites where multiple signs are permitted. Use a variety of colours and styles to create variety, interest and uniqueness to a development.
208. Free-standing signs should be a monument style shape where the base width is consistent with the full extent of the rest of the sign and where the bottom is not narrower than the rest of the sign.

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SIGN GUIDELINES

Free-Standing (cont.)

Within a development, the following elements shall be considered for Sign installations:

- 209. Placement of the address on the top of the freestanding sign is encouraged. *(Illustr.)*
- 210. Use aesthetically-pleasing materials and detailing for sign bases such as architectural concrete or masonry.
- 211. Install landscaping around the base of free-standing signs. Landscaping should be layered, with lower plants in the front and higher plants closer to the sign base and consistent in plant species, design, layout and Best Management Practices to that used throughout the rest of the development. *(Illustr.)*
- 212. Changeable copy lettering or designs are discouraged from being used on or with free-standing signs.



Miscellaneous

Within a development, the following elements shall be considered for Sign installations:

- 213. Projecting and/or blade signs shall be two-sided and professionally designed and may be cast, routed, built up, sculpted or lettered. *(Illustr.)*
- 214. Inflatable signs are not permitted.
- 215. Sign variety is encouraged, particularly for sites with multiple buildings or for an over-all development. Signs using the following design elements, in place of back-lit, flat-face sign boxes, are encouraged:
 - non-illuminated individual letters with remote illumination
 - halo-lit individual letters
 - under-awning signage



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SIGN GUIDELINES

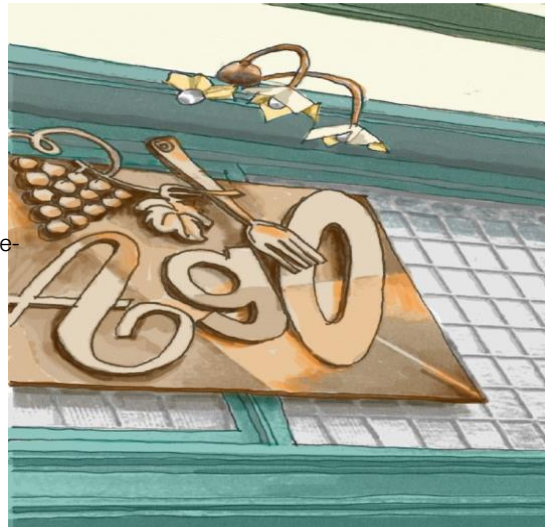
Lighting

Within a development, the following elements shall be considered for Sign installations:

- 216. Encourage architectural lighting on the face of commercial buildings and at main entrances to residential buildings to help create a sense of safety and intimacy around the building.
- 217. Gooseneck lights and sconces that are applied to fascias underneath weather protection elements are preferred for storefront lighting. *(Illustr.)*



- 218. Ensure lighting is sensitive to nearby residential uses. Avoid visible, glaring light sources by using down-lights or up-lights with cut-off shields. *(Illustr.)*
- 219. Incorporated valence lighting should be placed into canopies and up-lighting to illuminate pathways.
- 220. Encourage the use of LED lighting for storefronts.
- 221. Sign illumination shall be designed to limit light pollution and reduce light spill-over into adjacent residential areas. The International Dark Sky Model (as amended) shall be used as reference to determine appropriate lit levels for signs. Minimize sign lighting by using indirect lighting.



- 222. Avoid the use of exterior fluorescent light sources.
- 223. Install glare-free lighting into the canopy soffit. Fluorescent tube lights are not permitted for this purpose.

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SIGN GUIDELINES

Fascia

Within a development, the following elements shall be considered for Sign installations:

- 224. Individually-illuminated channel type letters including internally-illuminated or back-lit solid letters are encouraged. *(Illustr.)* Channel letters should either be flush mounted or mounted on a backer panel.
- 225. Backing for mounting channel letters should enhance the overall character of the building. Use of accent materials such as metal grills and wood should be considered as way to enhance architectural character. *(Illustr.)*



- 226. Individual cut-out signs are supported.
- 227. Backlit box signs with acrylic faces are discouraged; however, where these are used, white or contrasting background colours should be avoided.
- 228. Fascia signs are limited to the first storey only. For buildings with more than two storeys, fascia signs are only permitted on the first and top floor only.
- 229. Dominant fascia signs for a multi-tenant building should brand the building with an identifiable emblem or name of development.
- 230. All fascia signs can use a variety of fonts and styles but should be unified with other signage/building character, should be in scale with the building and should be centred on the façade.
- 231. Figurative graphics, emblems or brand graphics are encouraged singularly or as part of the sign copy. *(Illustr.)*
- 232. Window signs are not permitted above the ground floor.
- 233. Pump island fascia signs are permitted for gas stations only; however, the number of letters and words are restricted to a secondary role.
- 234. Wall plaque signage should be designed for a human scale.
- 235. For multiple tenant sites, ensure there is a coordinate approach to providing fascia signs where there is a clear hierarchy of signs but where anchor tenants do not dominate the first impression of the entire development.
- 236. Building or site addresses are encouraged to be placed at the building entrance and where visible from the street.



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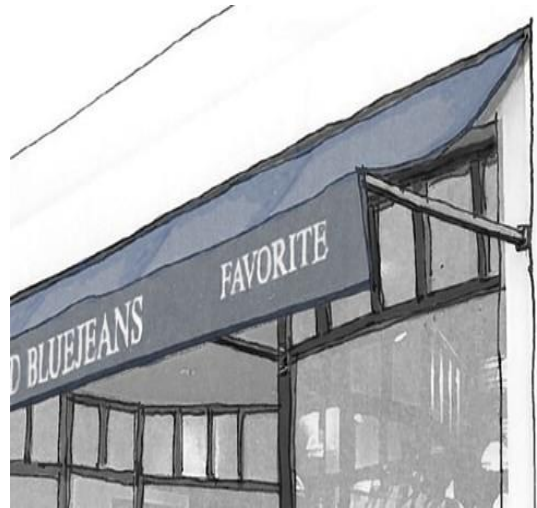
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SIGN GUIDELINES

Awning or Canopy

Within a development, the following elements shall be considered for Sign installations:

- 237. Awnings or canopies are limited to one prominent sign at unit entrances.
- 238. Awnings are preferred over canopies for use on storefronts less than 10m in length.
- 239. Three-point or four-point awnings are preferred.
- 240. Canopies are preferred over awnings for use on building frontages over 15 m long, where there is significant pedestrian activity or where people need to wait for significant durations.
- 241. Design awnings/canopies should be coordinated with and sympathetic to the style, scale and form of other on-site signage with the sign copy fitting easily onto the face of the awning/canopy for visual continuity. Canopy signs shall not repeat other on-site signs. *(Illustr.)*
- 242. Awnings, canopies and overhangs should incorporate architectural design features and the fenestration patterns (placement of windows and doors) of the buildings they extend from.
- 243. When placing awnings or canopies, the location and size shall minimally obscuring the building façade.
- 244. Transparent and translucent canopies, preferably made of wood or glass, are preferred to allow natural light to penetrate storefronts and the sidewalk.



Under-Awning or Under-Canopy

Within a development, the following elements shall be considered for Sign installations:

- 245. Signs attached to awnings or canopies shall be affixed below and permanently to the awning or canopy structure. Placing signs above the awning is discouraged. *(Illustr.)*
- 246. Under awning or projecting signs are encouraged to be more pedestrian oriented and used as an alternative to sandwich board signs.
- 247. Under awning or canopy signs should be incorporated into the building design to allow for signage flexibility.
- 248. Detailed material treatments (e.g. carvings, routing, layering or sculpting) should also be used for under awning/canopy signs.



DP1.2

Gas Station Guidelines: Residential and Commercial

Guidelines within this section are organized into three different types:

- Residential Areas
- Commercial Centres and
- Highway Commercial Zones

DP1.2 GAS STATION

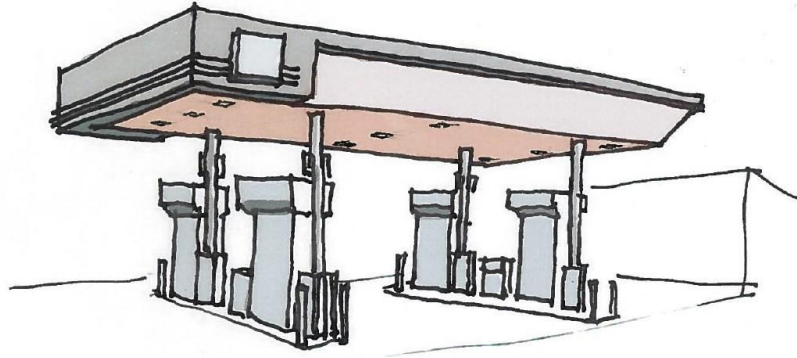
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RESIDENTIAL AREAS

Canopies

The following design elements shall be considered for Gas Stations in Residential Areas:

1. Large/high canopies should be avoided. The underside clearance of the canopy should be minimized. Canopies and any convenience stores located on site should be integrated and physically connected. The size of the canopy should be reduced to fit within a residential neighbourhood by using multiple canopies, different roof forms, off-set canopies, skylights, etc.
2. The setback of the canopy should align with the setback of the buildings on the block.
3. Lighting intensity level should be measured at the surface of the pavement area which is defined by the projection of the canopy and should not exceed 200 lux (20 footcandle). Lighting levels towards the periphery of the site should blend with the illumination level of abutting residential streets. Non-reflective materials should be used and fixtures should be recessed on the underside of the canopy to reduce glare. (*illustr.*)



4. Materials for canopies should reflect those used in the immediate residential context (e.g. cedar shingles, glass, wood, brick, etc.).
5. Proportions of the canopy-supporting structure should relate to the size of the canopy. If necessary, sections of the columns should be exaggerated to relate to the canopy.
6. The entire fascia band of a canopy should not be illuminated. Fascia height should be minimized and present a simple profile. The corporate logo should be restricted to two sides of the canopy only. Spot light fascia signage is optional.
7. The form of the canopy should reflect the residential context. Gables and sloping roof lines, thin fascia board, etc. should be used where those elements are present in the immediate residential neighbourhood.

Service Buildings

The following design elements shall be considered for Gas Stations in Residential Areas:

8. Where possible, service building side and rear yard setbacks should be increased to allow for additional landscaped buffering between adjacent properties.
9. Accessory buildings should relate to the size, form and height of the canopy. Integration of buildings is recommended.
10. The same materials should be used for accessory buildings and the canopy. Choose materials of common-use in residential areas (e.g. brick, stucco, wood, etc.). All elevations

DP1.2 GAS STATION

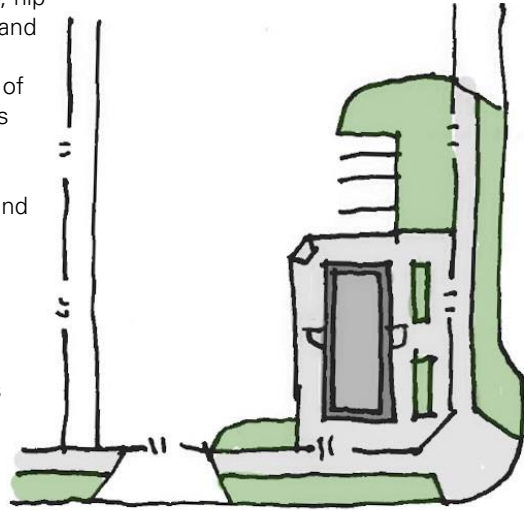
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RESIDENTIAL AREAS

Service Buildings *(cont.)*

The following design elements shall be considered for Gas Stations in Residential Areas:

11. Residential roof forms (e.g. sloping, gables, hip roofs, etc.), should be used for the canopy and service buildings. Façade treatment and materials should be consistent on all sides of the building. Use residential-style windows (e.g. mullions and trims).
12. Strategically locate buildings to maximize and facilitate casual surveillance of the site.
13. Provide clear pedestrian linkages to public sidewalks. *(illustr.)*
14. Provide weather protection for pedestrians by building overhangs and canopies. Store entrances should be recessed to allow adequate maneuvering space for wheelchairs in front of the doors.



Service Areas

The following design elements shall be considered for Gas Stations in Residential Areas

15. Garbage container enclosures should be of the same materials and finish as the rest of the building and shall have gates. The enclosure should be screened by landscaping and located out of direct view from the street and away from abutting residential properties. *(illustr.)*
16. The garbage container enclosure should be easily accessible by collection vehicles.
17. Outdoor storage is not permitted. Space should be provided inside the convenience store or in a dedicated storage area that is used and integrated with the building. Loading bays should have direct access to the storage areas and be well identified.
18. Underground placement of fuel tanks is required. The tanks should not be located within the required setback.



DP1.2 GAS STATION

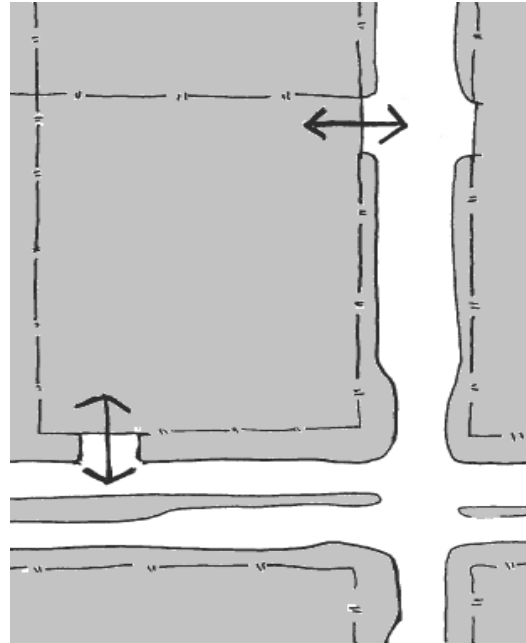
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RESIDENTIAL AREAS

Driveway Access

The following design elements shall be considered for Gas Stations in Residential Areas:

- 19. The overall site should be planned based on the functional requirements of an average-sized vehicle. Large delivery tanker trucks should be restricted to a dedicated outside lane. Service to trucks should not be provided at these gas stations.
- 20. Minimize the number and width of the entry points to the site to limit open views over the large paved areas. Access driveways to the site should be restricted to one from each of the main abutting streets (e.g. if located on a corner lot, limit to two access points only). *(illustr.)*
- 21. Raised curbs should be used to slow vehicular movement, minimize view corridors of paved areas, and narrow the gap in the continuity of the sidewalk.



Pavement Standards

The following design elements shall be considered for Gas Stations in Residential Areas:

- 22. Use concrete bands and pavers of contrasting color to break up the areas of asphalt. Areas of asphalt paving should be minimized and landscaping should be increased to be compatible with the landscaping located in the front yard in residential areas.
- 23. Use other types of paving (e.g. contrasting texture and color) to identify customer parking areas, gas pump service areas and pedestrian routes from the sidewalk to the commercial store.
- 24. Use different pavements in combination with landscaped aisles to define vehicular routes and lanes through the site (e.g. a pavement change can identify the 'tanker truck/large vehicle parking lane').
- 25. Concrete sidewalks should be identified across the entry points to the site. A level sidewalk should continue across the upper part of the driveway.

DP1.2 GAS STATION

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RESIDENTIAL AREAS

Site Lighting and Signage

The following design elements shall be considered for Gas Stations in Residential Areas:

26. Site lighting should be directed toward the interior of the site. Use landscaping to avoid the impact of reflected light from the canopy, signs and car head lamps onto adjacent sites.
27. Diffuse lighting, focused toward the inside of the site, should be used to minimize the glare towards adjacent properties.
28. One free standing sign per lot frontage will be permitted on the site and must be integrated with landscaping. The sign should be designed as a feature and incorporate materials and forms used in other structures on site. *(illustr.)*



29. Pole signs supported by one or more vertical posts and snap-on signs attached to light fixtures are not permitted.
30. Free standing and monument signs should be located a minimum of 2 m from the street property line to retain visual continuity of the landscaping. A permanent base should also be provided for changeable, temporary signs or advertisements.
31. Wall-mounted signs should be limited to a maximum of two sides of the building. Backlit Plexi-glass box signs, mounted on the fascia of the convenience store, are not permitted. Any box sign must be flush mounted.
32. Corporate logos on the canopy should be limited to a maximum of two sides. Backlit fascia signs are not permitted on any service buildings.

DP1.2 GAS STATION

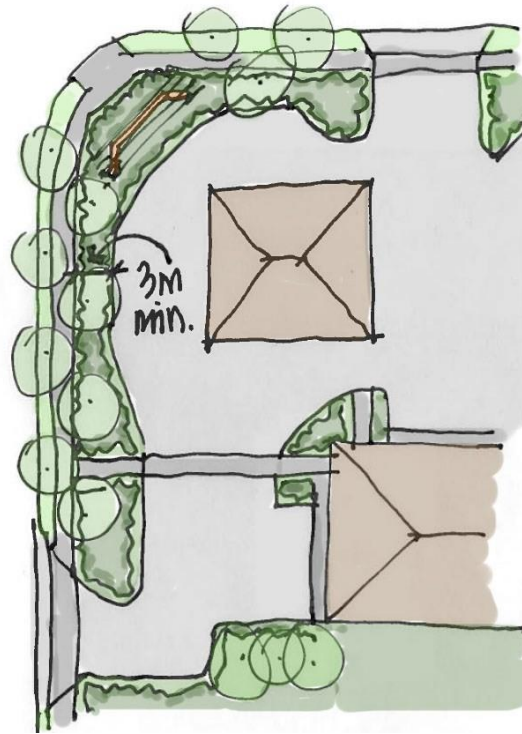
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RESIDENTIAL AREAS

Landscaping

The following design elements shall be considered for Gas Stations in Residential Areas:

33. Retain existing trees at the perimeter of the site and integrate them with the new landscaping to provide a buffer toward adjacent properties and the street.
34. A transitional landscaped area is required along the street, immediately abutting residential areas. This area should decrease in width from 7.5 m at the property line with residential uses to the required 3 m of landscaping along the street.
35. Define the edges of the site with low raised planters, berms, decorative low transparent fences and/or continuous landscaping.
36. Shrubs and trees should be provided along the street. High canopy trees are recommended along the street frontage for security reasons. These trees should be a minimum of 5 cm caliper, planted at 6 m on centre and located within the property line. Some conifers and specimen trees should be provided as accents. "Weeping tree" types are not permitted.
37. For site definition and screening, a 3 m wide landscaped area should be provided along street frontages. Existing trees within this strip should be retained. *(illustr.)*
38. All landscaped areas within the site must be defined by raised curbs. Use these landscaped areas to direct the flow of vehicles on the site and reduce total pavement coverage.
39. High hedges and conifers should be used along areas abutting residential land uses. Feature landscaping should be provided at site entrances and corners. Thorny shrubs are recommended in low visibility landscaped areas to discourage loitering and potential vandalism.



DP1.2 GAS STATION

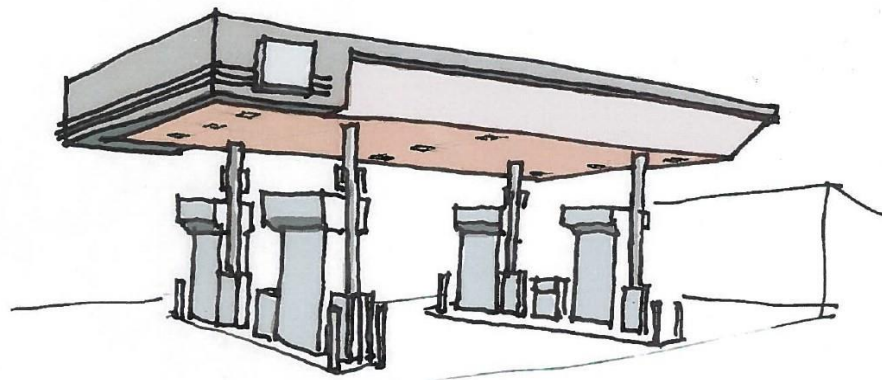
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COMMERCIAL CENTRES

Canopies

The following design elements shall be considered for gas stations in Commercial Centres:

40. Large canopies should be avoided. The use of multiple canopies, glass and thin fascias are strongly recommended to relate to the character of a commercial context. Roof lines and materials should be carefully chosen to respond to potential views from higher points.
41. Canopies should align with the building on the block. In no case should the setback for the canopy be less than 3 m.
42. The size of the canopy should be reduced to its minimum. Underside clearance of the canopy should be no more than 4.2 m and the canopy and convenience stores should be architecturally integrated. Materials should be used that reflect the immediate urban context, (e.g. use metal roofing, canopy skylights and stucco).
43. Roof styles (e.g. stepped, sloped and partial flat forms) should be used to reflect a mixed-use context and should improve potential views from above. Apparent size of the canopy structure should relate to the scale and design of the buildings in the immediate context. The use of multiple canopies, ex-posed space-frame structures and off-set canopies should be considered.
44. Properties of the support structure should relate to the apparent size of the canopy. If necessary, the support structure should be exaggerated to relate to the size of the canopy.
45. The fascia profile should be simple and its height should be minimized. Consider alternative design solutions (e.g. using a shed roof, skylights or discontinuous fascia bands). Backlighting of the entire fascia is not permitted. Spotlighting or neon signs are acceptable on a site-specific basis.
46. Lighting intensity level, measured at the surface of the pavement area defined by the projection of the canopy should not exceed 300 lux (30 footcandle). Lighting levels towards the periphery of the site should not exceed those established for the City Centre or for commercial zones. Using non-reflective materials and recessed lighting on the underside of the canopy is recommended. *(illustr.)*



DP1.2 GAS STATION

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COMMERCIAL CENTRES

Service Buildings

The following design elements shall be considered for gas stations in Commercial Centres:

47. Setbacks should be consistent with abutting land use yard requirements, for example, no side yard setback in a block of continuous commercial frontage.
48. Accessory buildings should relate to the size and height of the canopy. At certain locations, a larger and taller accessory building may be desirable to relate to the commercial context. Where appropriate, the commercial component should be located toward the corner of the site to maintain the continuity of building frontage.
49. Similar materials should be used in the canopy of service buildings. Choose materials used in commercial areas (e.g. glass, metal and stucco). All elevations should be consistent in the use of materials.
50. Commercial roof forms should be used for the canopy and the convenience store and/or service buildings. Façade treatments should be consistent on those sides of the building exposed to views. Weather protection should be provided for pedestrians around the convenience store and service buildings. Commercial window types are desirable.
51. Maximize window areas for casual surveillance and increased visibility of both staff and customers.
52. Backlit fascia signs or canopies of the convenience store and/or service building are acceptable if compatible with the immediate context.

Service Areas

The following design elements shall be considered for Gas Stations in Commercial Centres:

53. A covered enclosure, with gates, should be provided for the garage containers to avoid exposure to views from the street and from above. This enclosure should be located away from direct views from the street and should be built of the same materials and finish as the rest of the building. Landscaped screening may be appropriate. *(illustr.)*
54. Commercial and/or service buildings should include a separate room for the storage of goods with direct access from the outside. The loading and unloading area should be well identified with contrasting pavement. Outdoor storage is not permitted.
55. Storage areas and garbage container enclosures should be located for ease of access by delivery and collection vehicles.
56. Any fuel tank should be located under- ground and outside the setback area.



DP1.2 GAS STATION

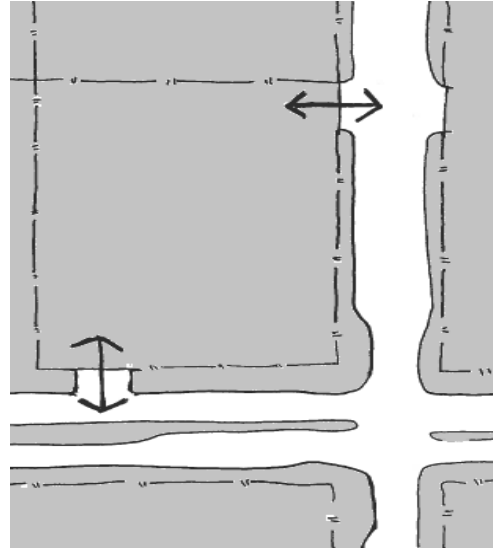
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COMMERCIAL CENTRES

Driveway Access

The following design elements shall be considered for Gas Stations in Commercial Centres:

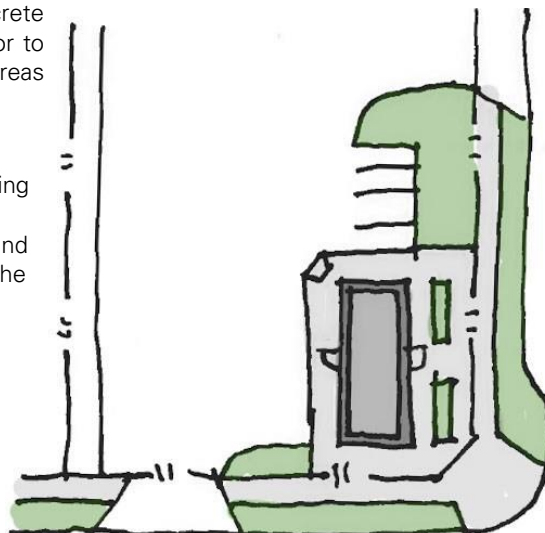
- 57. Site layout should be based on the functional requirements of an average-sized vehicle. Large delivery trucks should be restricted to a dedicated or outside lane.
- 58. Minimize the number and the width of the entry points to the site to limit open views over the large paved areas. Access drive- ways to the site should be restricted to two from the primary street and to one from the secondary street. Access drive- ways should be located as far away from the intersection as possible. *(illustr.)*
- 59. Raised curbs should be used to slow vehicles entering the site, minimize the interruption of the sidewalk and reduce views toward the paved areas.



Pavement Standards

The following design elements shall be considered for Gas Stations in Commercial Centres:

- 60. Break up the asphalt areas with concrete bands and/or pavers of contrasting color to reduce the impact of large paved areas viewed from above.
- 61. Use other types of paving (e.g. contrasting texture and color) to identify customer parking areas, gas pump service areas and pedestrian routes from the sidewalk to the commercial store. *(illustr.)*
- 62. Continuity of the public sidewalk should be maintained and reinforced across drive- ways to the site by using contrasting pavement.



DP1.2 GAS STATION

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COMMERCIAL CENTRES

Site Lighting and Signage

The following design elements shall be considered for Gas Stations in Commercial Centres:

- 63. All lighting should be directed toward the interior of the site.
- 64. It is recommended that diffused lighting, focused toward the inside of the site, be used to soften glare and spillage over adjacent properties.
- 65. One (1) monument sign should be provided for identification purposes and should be integrated with the landscaping. Signs should use the form and materials found in other structures on the site. Other signs are not permitted. *(illustr.)*
- 66. Monument signs should be located a minimum of 2 m from the street property line or at the building line, where the streetscape requires that a continuous street frontage be maintained. A permanent base should also be provided for changeable temporary signs to avoid the unplanned proliferation of spontaneous advertisement.
- 67. Corporate logos on the canopy should be limited to two sides.
- 68. Signage for the commercial/service buildings should relate to the signs used in the commercial context and shall be limited to two sides of the building. Backlit-Plexiglas wall-mounted signs are not permitted. Illuminated single channel letters, mounted directly on the fascia or wall, are recommended.



Landscaping

The following design elements shall be considered for Gas Stations in Commercial Centres:

- 69. Some of the existing trees on the perimeter of the site may be identified for retention and integrated with new tree and shrub planting along the boulevards.
- 70. A transitional area made up of a combination of soft and hard landscaping, should be provided toward the street frontage in areas abutting adjacent land uses.
- 71. At Town Centres or in the City Centre, low, raised landscaped planters and low, decorative fences, in combination with general landscaping, should be used for site definition to maintain the more formal streetscape character. In these specific areas, the width of the landscaped area may be narrower.

DP1.2 GAS STATION

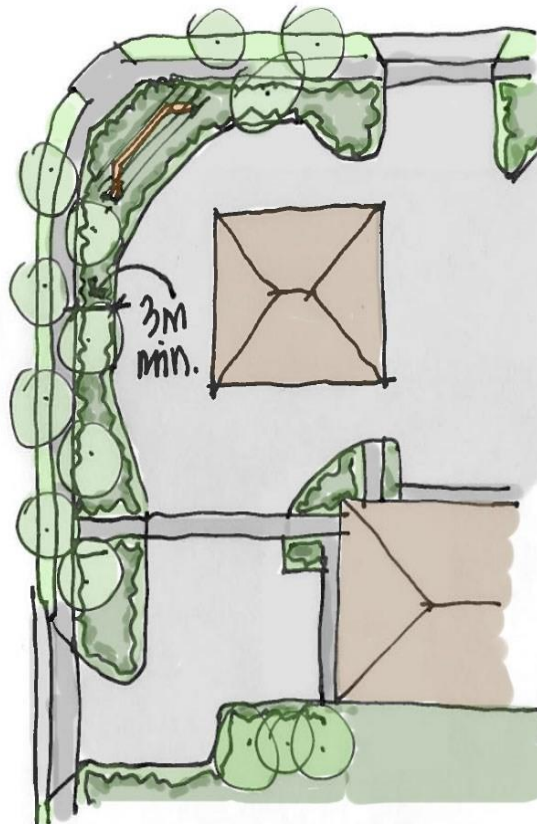
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COMMERCIAL CENTRES

Landscaping (cont.)

The following design elements shall be considered for Gas Stations in Commercial Centres:

72. Shrubs, in combination with high canopy trees, should be provided along the street frontage to complement boulevard trees. These trees should be 5 cm caliper, planted at 6 m on centre, and located within the property line. Some conifers and specimen trees should be provided as accents. "Weeping" tree types are not permitted.
73. A 3 m wide landscaped area should be provided along street frontages for site definition and screening of views toward the site. In some locations, retention of existing trees may be appropriate. *(illustr.)*
74. Landscaped areas defined by a raised curb should be provided within the site. These islands should be used to direct the flow of vehicles through the site and to soften the paved areas.
75. Landscaping should reflect the urban character of the area. Low hedging and shrubs of a regular height (approximately 1 m) is recommended along the street. Deciduous trees should be planted in groups as accents on the site. Overall landscaping concepts should be coordinated with the landscaping on abutting land uses and buildings forms (e.g. from above views).
76. If required, solid, high-quality fencing, or low walls in combination with landscaping, should be provided along the inside of property lines. Materials should be compatible and coordinated with the materials used on the buildings on-site.



DP1.2 GAS STATION

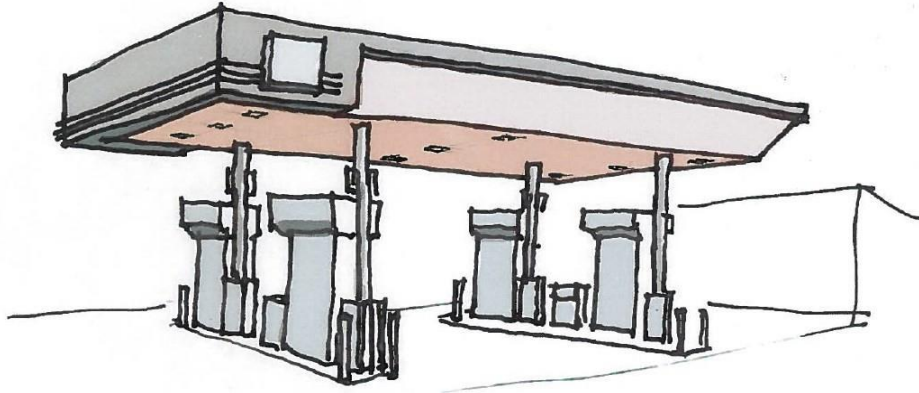
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HIGHWAY COMMERCIAL ZONES

Canopies

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

77. Support the importance of the canopy as a building landmark element by making the canopy a component of a larger commercial development. The canopy should incorporate or maintain coordination with the dominant forms of the building that define its background.
78. The minimum setback should be 7.5 m from the front property line. Reduced setbacks can be provided under a Comprehensive Development zoning supporting Highway Commercial uses.
79. Canopies and convenience stores should physically be linked and coordinated in scale with other buildings that form part of the complex.
80. Lighting intensity level, measured at the surface of the pavement area defined by the projection of the canopy should not exceed 300 lux (30 footcandle). Lighting levels towards the periphery of the site should not exceed those established for the City Centre or for commercial zones. Using non-reflective materials and recessed lighting on the underside of the canopy is recommended. (*illustr.*)



81. Roof form construction is required among the various buildings on the site. Apparent size of the canopy structure should relate to the scale and design of the buildings in the immediate context. Where appropriate, links between the canopy and the commercial component of the project should be consistent. Coordination of materials with adjacent buildings is required.
82. Proportions of the canopy support structure should relate to the apparent size of the canopy; if necessary, the support structure should be exaggerated to relate to the size of the canopy.
83. Fascia should relate to other buildings on the site. Corporate logos should be restricted to two sides of the canopy only. Backlighting the entire fascia is not permitted.

Service Buildings

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

84. Accessory buildings should relate to the size and height of the canopy and other buildings on the site. Similar materials should be used for the canopy and accessory buildings.
85. Maximize and strategically locate window areas for casual surveillance of the site. Backlit fascia/ canopies are acceptable if compatible with the immediate context.

DP1.2 GAS STATION

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HIGHWAY COMMERCIAL ZONES

Service Buildings (cont.)

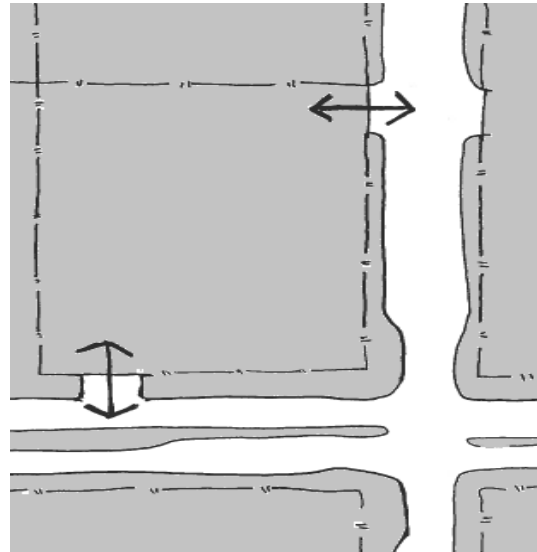
The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

- 86. Commercial roof forms should be used for the canopy, the convenience store, accessory buildings and other components of the site. Façade treatment and materials should be consistent on all sides of the buildings exposed to views. Provide weather protection for pedestrians along the convenience store frontage and other buildings on site. Covered, direct pedestrian links amongst buildings is encouraged. Recess entrance doors to the convenience/retail store for easy maneuvering of wheelchairs.

Driveway Access

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

- 87. Layout should permit easy access and egress and internal maneuvering of vehicles on the site. Service of trucks and large vehicles should be restricted to a dedicated or outside lane.
- 88. Access driveways are limited to two from the road. The site layout should aim to minimize the width and number of the entry points to the site. Access driveways should be located as far away from the intersection as possible. *(illustr.)*
- 89. Wherever possible, raised curbs should be used at the entrance to the site. Flared let-down access points are recommended where vehicular ease of access is a concern.



Pavement Standards

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

- 90. Break up asphalt areas with concrete bands at site entrances and at specific functional areas to reduce the impact of large paved areas.
- 91. Use different pavement materials to identify functional areas of the site (e.g. customer parking, pedestrian routes to retail commercial buildings and amongst buildings on the site).
- 92. Use contrasting color and textured pavement and landscaped aisles to identify vehicular and pedestrian movements through the site.

DP1.2 GAS STATION

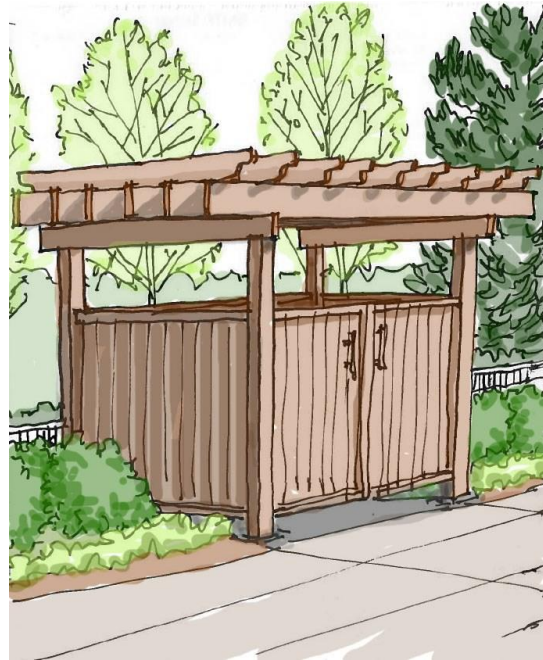
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HIGHWAY COMMERCIAL ZONES

Service Areas

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

- 94. Garbage container enclosures should be of the same materials and finish as the rest of the building and shall have gates. The enclosure should be screened by landscaping and located out of direct view from the street and away from abutting residential properties. *(illustr.)*
- 95. Outdoor storage areas are not permitted.
- 96. Fuel (including propane) tanks should not be located within the required setback. Visibility toward the tanks should be minimized by providing a substantial landscaping berm. Vertical positioning of the tanks should be avoided.



Site Lighting and Signage

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

- 97. Freestanding signs, without a single pole, should be integrated with the landscaping and provide with a strong base. Signs should use the materials and forms found in other structures on the site or in the immediate area. The use of a sign as a feature landmark along the highway is encouraged.
- 98. Signs should be located a minimum of 2 m from the front property line. A permanent base should also be provided for changeable temporary signs. Corporate logos on the canopy should be limited to two sides.
- 99. Signage for the accessory/commercial buildings on site and for gas station signage should be coordinated. Backlit Plexiglas fascia-mounted signs are not permitted unless they constitute a continuous fascia band. Backlit channel letters are recommended. Surface-mounted box signs are not permitted.
- 100. Lighting of the site should be directed toward the interior and should be consistent throughout the development.
- 101. It is recommended that diffused lighting, focused toward the interior of the site, be used to reduce glare over adjacent properties.

DP1.2 GAS STATION

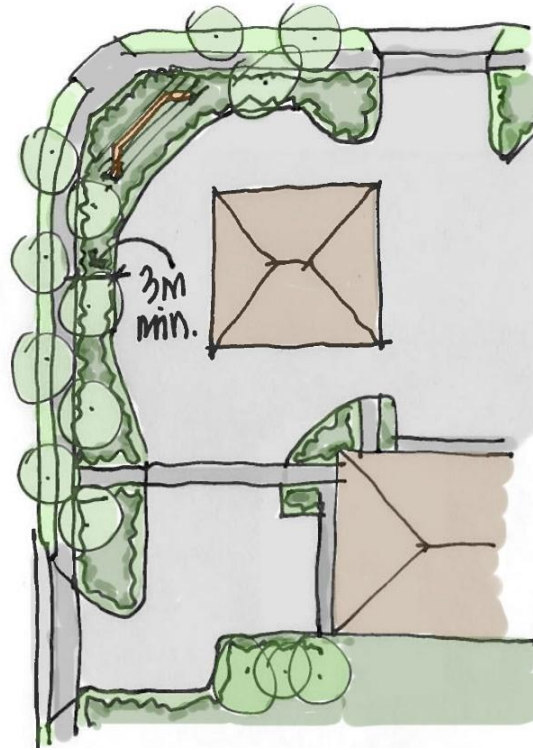
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HIGHWAY COMMERCIAL ZONES

Landscaping

The following design elements shall be considered for Gas Stations in Highway Commercial Zones:

102. For site definition and screening, a 3 m wide landscaped area should be provided along street frontages. Existing trees within this strip should be retained and incorporated into the new landscaping. *(illustr.)* Coniferous trees should be planted in this area.
103. A front yard transitional area with substantial landscaping should be provided in the area immediately abutting adjacent sites.
104. A combination of low berm or low feature wall and landscaping should be considered along the street, or highway frontage of the site. No solid fences are permitted along a street.
105. Landscaping should reflect the car-oriented character of the area. Deciduous trees, with conifer accents, should be planted along the street frontage. High canopy trees located within the property line should be considered along the highway frontage.
106. All existing trees at the perimeter of the site should be retained and integrated with new tree and shrub planting.
107. Landscaped areas provided within the site should be defined by a raised curb. These islands should be used to direct the flow of vehicles throughout the site.
108. Deciduous trees should be planted on the interior of the site and side yard to achieve a certain degree of integration with adjacent developments. Thorny shrubs should be considered in isolated areas which are hidden from surveillance and may be subject to potential vandalism.
109. No solid fences are permitted. If required, low transparent fences, in combination with landscaping, are recommended. The fence should permit pedestrian movement between sites.



DP2

DEVELOPMENT PERMIT GUIDELINES: HAZARD LANDS

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The Hazard Lands Development Permit Guidelines are intended to ensure development takes place appropriately within two distinct classifications of hazards: Steep Slopes and Flood Prone Areas.

Steep Slopes: are those determined to have a minimum of 20% grade requiring additional studies and/or limitations to ensure development is either not compromised and or that development does not compromise adjacent lands.

Flood Prone: are those determined to fall within the 200 year Flood Plain as defined by the Province of British Columbia.

In order to provide further clarification of the guidelines within this section, pictures or graphic images are provided; and where a particular guideline is graphically illustrated, the notation “(illustr)” appears.



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DP2 Hazard Lands Guidelines User Guide:

The following User Guide provides the main components required for each Hazard Land Development Permit application:

1. REVIEW and CONSIDER

- A. Definitions for Further Clarification
- B. Development Restrictions
- C. Development Guidelines

2. SUBMIT COMPLETE APPLICATION

(Exact submission requirements to be confirmed by Staff)

- D. Provide Qualifications of all Consultants
- E. Identify Hazard Areas:
 - Steep Slopes
 - Flood Prone
- F. Development Feasibility Study
- G. Stormwater Management Plan
(if required—applies to Steep Slope Areas only)

3. REVIEW ADDITIONAL REQUIREMENTS

- H. Inspections and Monitoring

NOTE: Notwithstanding the Hazard Land Guidelines listed in this section, a submission requirement may be waived, at the discretion of the General Manager, Planning and Development, if deemed unrelated to the type of development proposed in the Development Permit Application.

DP2 HAZARD LANDS

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A. Definitions

For development in the Hazard Lands Development Permit Area, **REVIEW** and **CONSIDER** the following Definitions as follows:

Steep Slopes	Flood Prone
Arborist: A professional certified by the International Society of Arboriculture.	
Gross Floor Area: All the area of the floor enclosed by the outside edge of the exterior walls of a building, including stairways, elevators and storage and mechanical rooms.	
Qualified Professional (QP): A Professional Engineer (PEng) or a Professional Geoscientist (PGeo) with demonstrated expertise and knowledge in geotechnical and/or steep slope development hazards. Must be registered with an appropriate professional organization in British Columbia; act under that association’s Code of Ethics; be subject to disciplinary action by that association; and be reasonably relied on, through demonstrated suitable education, experience, accreditation and knowledge relevant to the subject matter, to provide advice within their area of expertise.	Qualified Professional (QP): A Professional Engineer (PEng) with demonstrated expertise and knowledge in water resources, drainage and stormwater management. Must be registered with an appropriate professional organization in British Columbia; act under that association’s Code of Ethics; be subject to disciplinary action by that association; and be reasonably relied on, through demonstrated suitable education, experience, accreditation and knowledge relevant to the subject matter, to provide advice within their area of expertise.

B. Development Restrictions

For development in the Hazard Lands Development Permit Area, **REVIEW** and **CONSIDER** the following Development Restrictions as follows:

Steep Slopes	Flood Prone
1. Habitable buildings are not permitted on hazardous lands where future danger cannot or should not be mitigated.	
2. Areas subject to flooding, outlined in Figure 66 of this Official Community Plan, must remain free of development or be strictly developed, in accordance with Surrey Council Policy No. O-55.	
3. New large or greenfield developments or subdivisions are not permitted within flood prone areas of the City, as shown in Figure 66 of this Official Community Plan.	

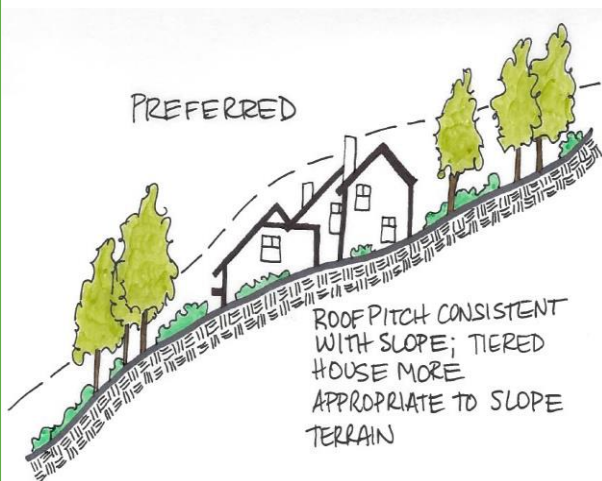
DP2 HAZARD LANDS

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C. Development Guidelines

For development in the Hazard Lands Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

Steep Slopes	Flood Prone
<p>4a. Overall Site: If roads are required to access development, ensure road construction is done safely, preserving and slopes greater than twenty-five percent (25%) as undisturbed areas.</p>	<p>4b. Overall Site: Development within flood prone areas must take a coordinated approach to ensure soil and Building Permit issues and requirements are jointly met.</p>
<p>5a. Overall Site: Incorporate required parking into the natural landscape to minimize the requirement for lot grading (e.g. avoid large, flat parking areas).</p>	<p>5b. Overall Site: In lowland areas, including agricultural lands, fill placement shall be consistent with existing Surrey Bylaws, including the Surrey Stormwater Drainage Regulation and Charges Bylaw and the Soil Conservation and Preservation By-law, as amended.</p>
<p>6. Overall Site: Protect the important ecological and aesthetic values of environmentally significant features such as wildlife trees and ecologically-rare vegetative communities as the Best Management Practice to maintaining slope stability and preventing drainage and erosion problems.</p>	
<p>7. Overall Site: Minimize cut and fill excavation to preserve the natural topography of the hillside. <i>(illustr.)</i></p>	



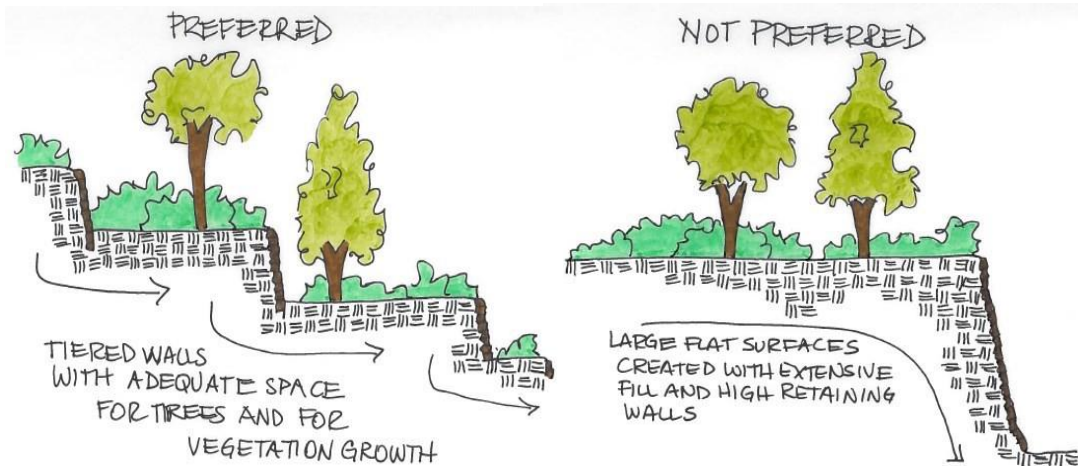
DP2 HAZARD LANDS

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C. Development Guidelines (cont.)

For development in the Hazard Lands Development Permit Area, **SUBMIT** information with the Development Permit application that details the following:

Steep Slopes	Flood Prone
<p>8a. Building and Construction: Building, structures and paved surfaces shall be sited:</p> <ul style="list-style-type: none"> a) away from areas subject to natural hazards, including, but not limited to, erosion, sloughing, rock fall, or land slippage; b) to minimize cutting into slopes; c) to avoid the use of retaining walls over 1.2 m in height. Retaining walls higher than 1.2 m shall be tiered or used with a landscaping break which complements the natural slope of the land (<i>illustr.</i>); d) to avoid encroaching into the critical root zones of those trees being retained. 	<p>8b. Building and Construction: Building, structures and paved surfaces shall be sited away from areas subject to flooding.</p>
<p>9a. Building and Construction: Ensure the location and construction of any underground structures, such as parkades, do not create conflicts with underground streams or other sub-surface water movements.</p>	<p>9a. Building and Construction: In existing areas, new development shall build to sufficiently meet the recommended Provincial Flood Construction Level (FCL) or as determined by a Qualified Professional or the City of Surrey.</p>



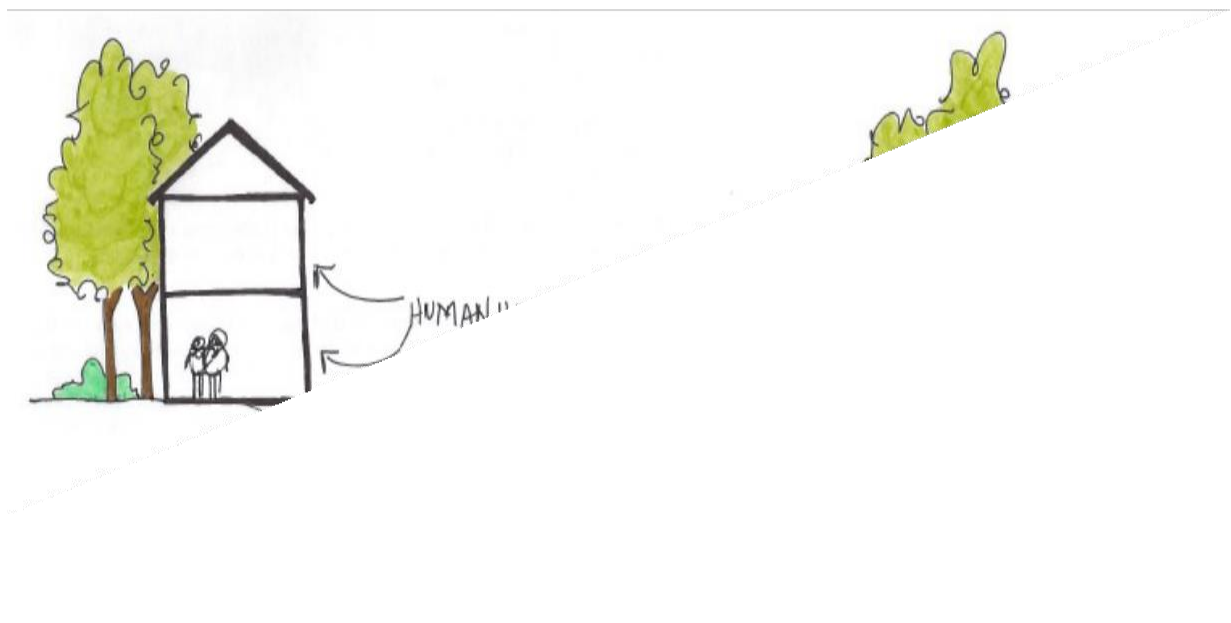
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C. Development Guidelines (cont.)

For development in the Hazard Lands Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

Steep Slopes	Flood Prone
	<p>10. Building and Construction: Minor modifications to an existing building or structure may be considered provided the area of the addition does not exceed twenty percent (20%) of the existing Gross Floor Area and the foundations within the floodplain are designed to minimize the loss of floodplain storage.</p>
	<p>11. Building and Construction: Where a building or structure intrudes partially or completely within a floodplain (<i>illustr.</i>) any modification to the building foundation should be consistent with current Surrey Bylaws and policies, as amended. Consistency with all floodplain requirements shall be met prior to the issuance of a Building Permit.</p>
	<p>12. Building and Construction: Land shall remain free of buildings and structures for human habitation except where:</p> <ul style="list-style-type: none"> a) the foundation is out of the area of the floodplain; b) those portions of a building or structure capable of being used for human habitation are located above the floodplain elevation. (<i>illustr.</i>)



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C. Development Guidelines (cont.)

For development in the Hazard Lands Development Permit Area, **SUBMIT** information with the Development Permit application that details the following:

Steep Slopes	Flood Prone
<p>13. Soils: Develop an understanding of the environmental and geological conditions of the site prior to any construction to ensure that the most appropriate methods and materials are used to develop a site.</p>	
<p>14. Trees and Landscaping: Ensure the site is re-vegetated, post construction, with species best suited to provide and support slope stabilization.</p>	
<p>15. Trees and Landscaping: Vegetate existing plant materials of significant size and/or relocate within the same development site.</p>	
<p>16a. Subdivision: Ensure hillside subdivision layouts and specifications recognize severe climate conditions, the difficulty of road maintenance, the movement of pedestrians and any other environmental factors that can contribute to an overall sustainable development.</p>	<p>16b. Subdivision: Subdivision within flood prone areas, excluding agricultural lands, shall not be allowed except where the realignment of lot lines would improve building location and lessen the impact of flooding.</p>
<p>17. Subdivision: Avoid including portions of a site, with thirty percent (30%) slope or greater, in the lot size calculation, or where sufficient land with a slope less than thirty percent (30%) is not available.</p>	
<p>18. Subdivision: Create larger lots or cluster developments to avoid significant disturbance to steep slope sites.</p>	

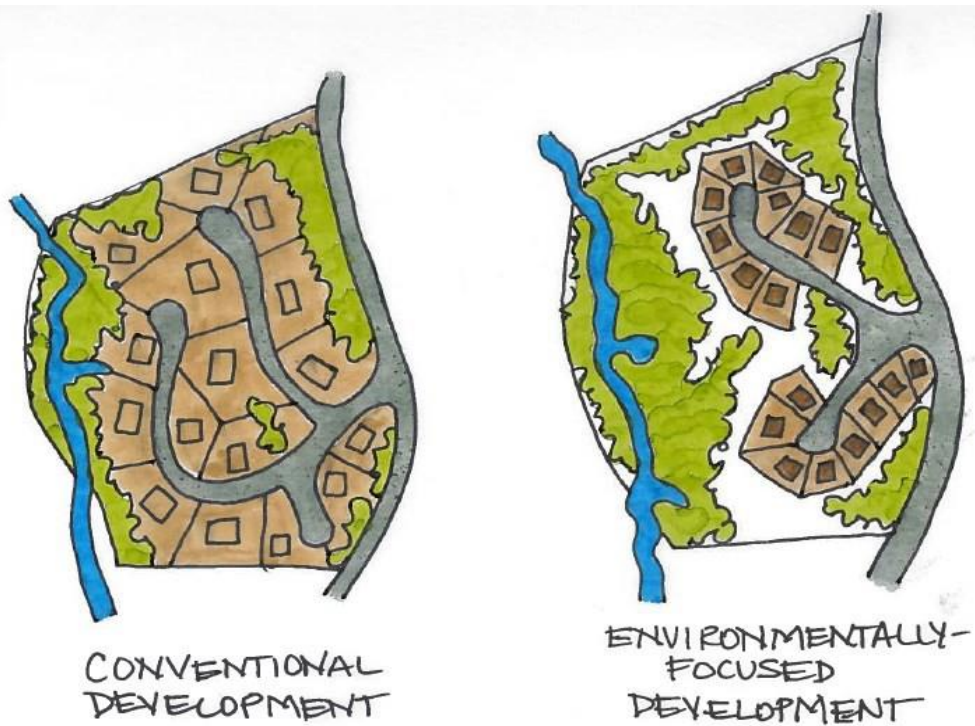
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C. Development Guidelines (cont.)

For development in the Hazard Lands Development Permit Area, **SUBMIT** information with the Development Permit application that details the following:

Steep Slopes	Flood Prone
19. Subdivision: Ensure subdivision planning and design allows for as much untouched native and natural green space as possible. <i>(illustr.)</i>	
20. Subdivision: Direct more density to less sensitive parts of a site striving to achieve a minimum of thirty-five percent (35%) of the gross site as permanent open space. <i>(illustr.)</i>	



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D. Submission Requirements—Consultant Qualifications

For development in the Hazard Lands Development Permit Area, **SUBMIT** information with the Development Permit application that provides the qualifications for each separate consultant used per development proposal, as follows:

Steep Slopes	Flood Prone
<p>21. <input type="radio"/> A list of ALL registered Qualified Professionals, with demonstrated education, expertise, accreditation and knowledge relevant to geotechnical, steep slope development hazards, water resources, drainage and/or storm-water management who will be involved with the development.</p>	
<p>22. <input type="radio"/> A written statement, including all documentation, verifying the qualifications of all Qualified Professionals (QP) responsible for preparing report submissions or involved in monitoring site conditions for Steep Slopes.</p>	
<p>23. <input type="radio"/> Where more than one Qualified Professional is needed, a written statement identifying THE PRIMARY QP for the entire development in order to ensure all required reports are prepared by qualified professionals and are coordinated both in content and in execution.</p>	

E. Submission Requirements—Identify Hazard Areas

For development in the Hazard Lands Development Permit Area, **SUBMIT** information with the Development Permit application detailing the location of the Hazard Lands in context with the development proposal:

Steep Slopes	Flood Prone
<p>24a. <input type="radio"/> Steep Slopes: Refer to this Official Community Plan, Implementation Section II (d) Implementation Instruments, Development Permits, DP2 Hazard Lands and Figure 65, Steep Slope Hazard Lands Development Permit Area to determine the location of the Steep Slope Development Permit Area in Surrey.</p>	<p>24b. <input type="radio"/> Flood Prone: Refer to this Official Community Plan, Implementation Section II (d) Implementation Instruments, Development Permits, DP2 Hazard Lands and Figure 66: Flood Prone Hazard Lands Development Permit Area to determine the location of the Flood Prone Development Permit Area in Surrey.</p>

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F. Submission Requirements—Development Feasibility Study

For development in the Hazard Lands Development Permit Area, **SUBMIT** a Development Feasibility Study with the Development Permit application that details the following:

Steep Slopes	Flood Prone
<p>25. Overall Site: Information on how site grading will provide for a smooth and stable transition between the development site and adjacent properties including site cross-sections.</p>	
<p>26. Overall Site: A plan showing what areas are to be cleared, what areas of cut and fill are proposed and the estimated sequence and timing of the clearing and re-contouring operation.</p>	
<p>27a. Building and Construction: Measures, including sequencing and timing of earthworks, that will be put into place to prevent and mitigate possible erosion, sloughing, rock falling, or land slippage during ALL (pre, during and post) stages of development for both the subject property AND any adjacent property affected by the proposed development.</p>	<p>27b. Building and Construction: Measures, including sequencing and timing of earthworks, that will be put into place to prevent and mitigate possible flooding during ALL (pre, during and post) stages of development for both the subject property AND any adjacent property affected by the proposed development.</p>
<p>28. Soils: A comprehensive geotechnical report providing soil stability analysis in relation to the slope and its ability to accommodate development. Aspects to be addressed in this report include, but are not limited to, information about soil bearing capacity, recommendations on building foundation types, provisions about permanent and/or temporary placement of soil, stipulations on stormwater management (both during and after construction) and recommendations related to accessory buildings, landscaping, vegetation and slope maintenance</p>	
<p>29. Soils: Provide a detailed report, prepared by a Qualified Professional, illustrating how the development meets a minimum level of safety for geotechnical failures, as set out in the BC Building Code or as specified in the Association of Professional Engineers and Geoscientists of BC (APEG) Guidelines for Legislated Landslide Assessments for Proposed Residential Development, or whichever is greater. The City reserves the right to modify this guideline to suit the proposed development.</p>	

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F. Submission Requirements—Development Feasibility Study (cont.)

For development in the Hazard Lands Development Permit Area, **SUBMIT** a Development Feasibility Study with the Development Permit application that details the following:

Step Slopes	Flood Prone
<p>30. Trees and Vegetation: A site report prepared by an Arborist detailing the number of trees and the species or species mix and the condition and health of existing trees with details about how tree removal will impact any remaining trees or landscaping, whether on site or immediately adjacent to the site</p>	
<p>31. Trees and Vegetation: Include a tree survey, and any related reports, as prepared by an Arborist, as part of a Building Permit application for an infill lot (e.g. any lot that is not part of a recent subdivision for which a tree survey was submitted). The tree survey shall identify trees that are protected under Surrey’s Tree Protection By-law making recommendations as to the retention or removal of trees where it is necessary to accommodate construction that cannot be located elsewhere. The Arborist must confirm that the grading and servicing plans for the development have been reviewed and that there are no conflicts with the trees proposed to be retained. Resolution of conflicts, if any exist, shall be undertaken before a Building Permit is issued.</p>	
	<p>32. Drainage: A report detailing the type of drainage system that will be used to specifically prevent erosion, protect natural watercourses and protect properties that are either above, below or adjacent to the development site.</p>

G. Stormwater Management Plan

For development in the Steep Slope Hazard Area, using plans, elevations, profiles and/or reports, **SUBMIT** a **STORMWATER MANAGEMENT PLAN** detailing, illustrating and describing the following:

33. **Drainage:** Depending on the size of the development and complexity of the site conditions, a Stormwater Management Plan may be required for the entire site and downstream drainage areas. (*illustr.*) Specific details to be addressed in the Management Plan include information about the:
- a) Hydrological conditions prior to and after development
 - b) Existing groundwater conditions, especially spring and interflow areas prior to and after development.
 - c) Protection of natural flow paths, volumes and storage resources
 - d) Impacts on trees, vegetation and other environmental features due to changes in drainage patterns
 - e) Water quality prior to, during, and after development
 - f) Sediment and erosion control
 - g) On and off-site drainage impacts (e.g. drainage from an upper lot to a lower lot).



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H. Inspections and Monitoring

For development in the Hazard Lands Development Permit Area, REVIEW additional inspection and monitoring requirements that may be required for Development Permit applications:

- 34. Retain the QP who prepared the Development Feasibility Study, to carry out field inspections during the construction of the project to confirm that the construction is in compliance with the recommendations of the Development Feasibility Study. This is a condition of the Building Permit for development within hazard lands identified in Figure 65 and 66. The QP must provide the Building Inspector with field reports related to the geotechnical aspects of the project and, at the time of final inspection of the completed building project, the QP must certify, in writing to the Building Inspector, that all aspects of the construction have been completed in compliance with their design and recommendations.
- 35. Further to DP2.1 of these Hazard Lands Development Permit Guidelines, Restrictive Covenants may be required as part of the development approval process for any hazardous lands (steep slopes or flood areas) to ensure any technical reports and specifications for the site, during and after construction, run with the lands and are followed as required by any future property owners.

DP3

DEVELOPMENT PERMIT GUIDELINES:

SENSITIVE ECOSYSTEMS

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The Sensitive Ecosystems Development Permit Guidelines are intended to be used to protect two distinct classifications of the natural environment: Streamside Areas and Green Infrastructure Areas.

Streamside Areas: represent the area adjacent to and setback from a stream that links aquatic and terrestrial ecosystems. These areas also include riparian area vegetation and the adjacent upland vegetation that exerts an influence on the stream.

Green Infrastructure Areas: are those areas identified in Surrey's *Biodiversity Conservation Strategy* as both the Biodiversity Management Areas and the Green Infrastructure Network .

In order to provide further clarification of the guidelines within this section, pictures or graphic images are provided; and where a particular guideline is graphically illustrated, the notation “(illustr)” appears.

Where a property falls within additional Secondary Plan Areas, policies and development requirements identified in those documents shall be taken into consideration in conjunction with the Guidelines listed here.



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DP3 Sensitive Ecosystem Guidelines User Guide:

The following User Guide provides the main components required for each Sensitive Ecosystem Development Permit application:

1. REVIEW and CONSIDER

- A. Definitions for Further Clarification
- B. Development Restrictions
- C. Development Guidelines

2. SUBMIT COMPLETE APPLICATION

(Exact submission requirements to be confirmed by Staff)

D. Submit an Ecosystem Development Plan

(applies to either a Green Infrastructure Area or Streamside Area)

- i) Provide Qualifications of all Professionals
- ii) Identify Protection Areas:
 - Streamside
 - Green Infrastructure
- iii) Determine On-going Safeguarding of Protection Areas
- iv) Detail Existing, Pre-Development Site Conditions
- v) Detail the Proposed Development

E. Submit an Impact and Mitigation Plan (if required)

(Applies to Streamside Areas only)

Note: Development Permit Area boundaries, objectives and exemptions are provided under Section II (d): Implementation Instruments: Development Permit Areas of this Official Community Plan.

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DP3 SENSITIVE ECOSYSTEMS

A. Definitions

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Definitions as follows:

Streamside	Green Infrastructure
Ecosystem Development Plan: A compilation of detailed reports as prepared by Qualified Environmental Professionals (QEP) which evaluates the appropriateness of a development.	
n/a	Green Infrastructure Areas: Existing or potential areas comprised of environmentally sensitive and/or unique natural areas that are consistent with the Biodiversity Management Areas and Green Infrastructure Network and corresponding conditions and recommendations required for management as identified in Surrey’s Biodiversity Conservation Strategy. (illustr.)
Impact Mitigation Plan: A compilation of detailed reports as prepared by Qualified Environmental Professionals (QEP) used in conjunction with a Development Variance Permit Application in determining the appropriateness of reducing the Streamside Protection Area as defined in Section 7a of the Zoning Bylaw, as amended.	n/a
Sensitive Ecosystem Area: Comprised of riparian or green infrastructure areas that are environmentally sensitive or unique natural areas such as streams, watercourses, cliffs, escarpments, forests, geological formations, wildlife habitat and wetlands.	
Species-At-Risk: Schedule 1, Federally-protected <i>Species-At-Risk Act</i> or Provincial Red- or Blue-Listed plant or animal species and their critical habitats and any species or habitat features identified as requiring year-round protection as identified in the <i>Provincial Wildlife Act</i> .	
Stream: Any of the following that provides fish habitat: (a) a watercourse, whether it usually contains water or not; (b) a pond, lake, river, creek, brook or a natural or channelized stream; (c) a ditch, spring or wetland, that is connected by surface flow to any item referred to in a) or b) above; or (d) a channelized, connected wetland, constructed channel or ditch, lake, swamp, gulch, ravine or natural stream.	
Streamside Areas: Existing or potential areas comprised of streambeds, water, trees, and grasses that border streams, lakes and watersheds and that link water to land and directly influence or provide fish habitat. (illustr.)	n/a



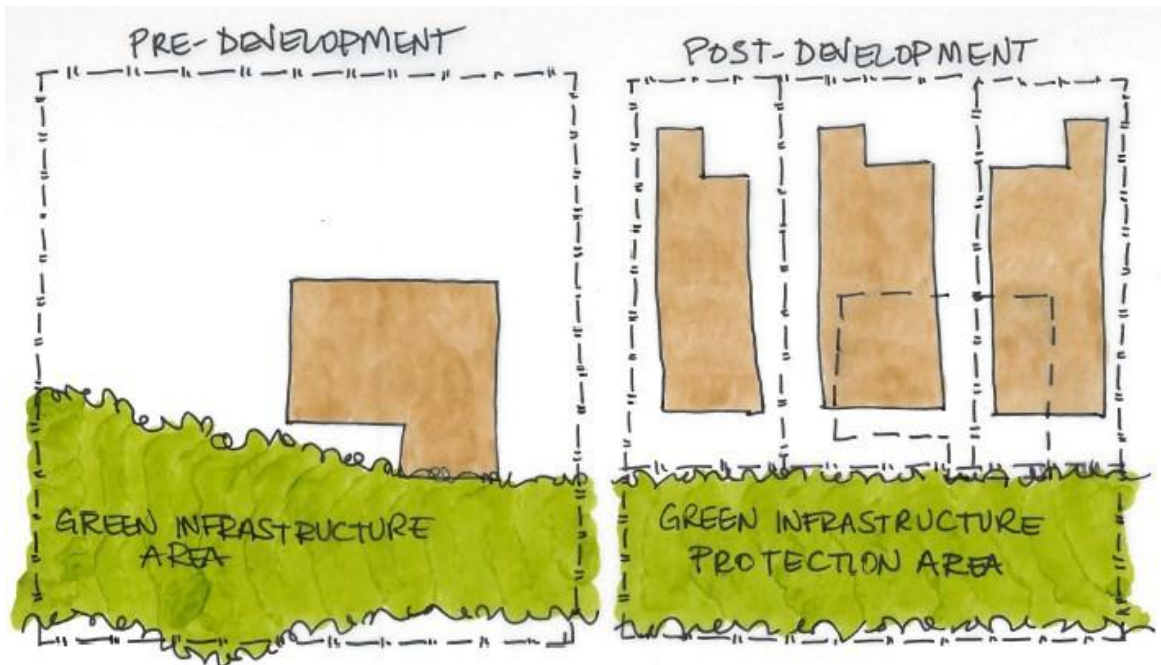
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B. Development Restrictions

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Development Restrictions as follows:

Streamside	Green Infrastructure
<p>1a. Any requirements specified in Riparian Areas Regulation, B.C. Reg. 376/2004, as amended, take precedence over any guidelines listed in this Section.</p>	<p>1b. Where a Green Infrastructure Area has been identified, development shall be adjusted to accommodate and be sensitive to the Biodiversity Management Areas, Green Infrastructure Network and the specific conditions and recommendations for protection listed within Surrey’s Biodiversity Conservation Strategy.</p>
<p>2a. Construction and post-construction access into a Streamside Protection Area is strictly prohibited, except where permitted by the City of Surrey.</p>	<p>2b. No development shall occur within a Green Infrastructure Protection Area that has not been identified, addressed and reviewed by a QEP to the satisfaction of the City of Surrey or that is not consistent with the submitted and approved Ecosystem Development Plan. <i>(illustr.)</i></p>



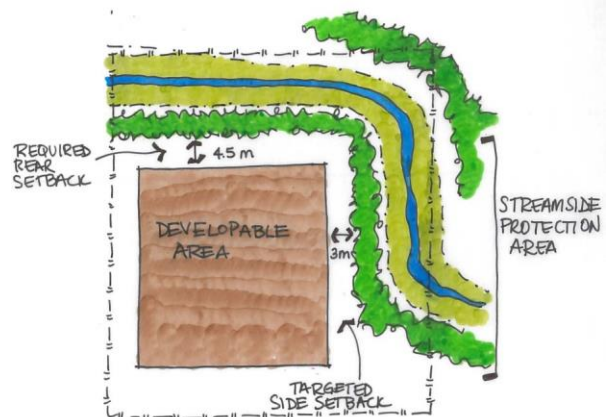
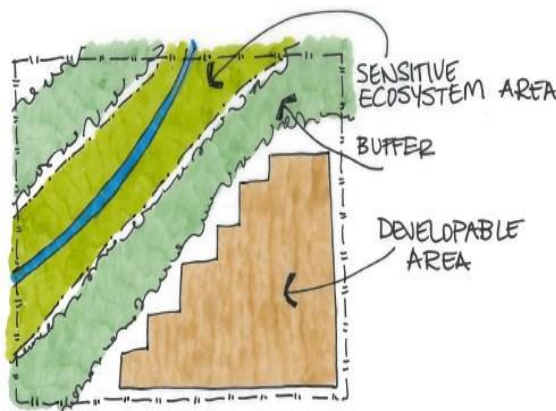
DP3 SENSITIVE ECOSYSTEMS

C. Development Guidelines

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

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Streamside	Green Infrastructure
<p>3a. Overall Site:</p> <ul style="list-style-type: none"> a) Plan and design on-site development roadways and connections to minimize habitat fragmentation and minimize disruption and impedance to wildlife travel. <input type="radio"/> b) Minimize encroachment into Protection Areas by adjusting building construction and site layouts adjacent to a Streamside or Green Infrastructure Area. c) Building, roadways, parking areas and driveways shall be located away from a Protection Area. d) Create functional yards by having a minimum of 4.5 m for rear yards and aiming for a 3.0 m side yard adjacent to a Streamside or Green Infrastructure Protection Area. 	
<p>4. Overall Site: Establish buffers large enough to protect the ecological integrity of Sensitive Ecosystem Areas. <i>(illustr.)</i></p> <p><input type="radio"/></p>	
<p>5a. Overall Site: Avoid locating infrastructure corridors adjacent to or within Streamside Protection Areas. Where it can be demonstrated that alternatives are not possible, design crossings that are narrow and perpendicular, and that minimize disturbance to Streamside Protection Areas.</p> <p><input type="radio"/></p>	<p>5b. Overall Site: Avoid locating infrastructure corridors adjacent to or within Green Infrastructure Protection Areas. Where it can be demonstrated that alternatives are not possible, design crossings that are narrow and perpendicular, and that minimize disturbance to Green Infrastructure Protection Areas.</p> <p><input type="radio"/></p>
<p>6. Overall Site: Determine buffer locations and sizes using contemporary Best Management Practices with the main goal of protecting Sensitive Ecosystem Areas from the effects of development and to retain the integrity of the Sensitive Ecosystem as a habitat and/or riparian area.</p> <p><input type="radio"/></p>	
<p>7a. Overall Site: Any development adjacent to or within (for approved works and services installations only) a Streamside Protection Area requires the monitoring of conditions, both on-and off-site, by a QEP prior to, and during construction.</p> <p><input type="radio"/></p>	<p>7b. Overall Site: Any development adjacent to or within (for approved works and services installations only) a Green Infrastructure Protection Area requires the monitoring of conditions, both on-and off-site, by a QEP prior to, and during construction.</p> <p><input type="radio"/></p>
<p>8. Building and Construction: Minimize impervious surfaces to improve stormwater absorption by using permeable materials and techniques, including permeable pavers and structural grassfields.</p> <p><input type="radio"/></p>	



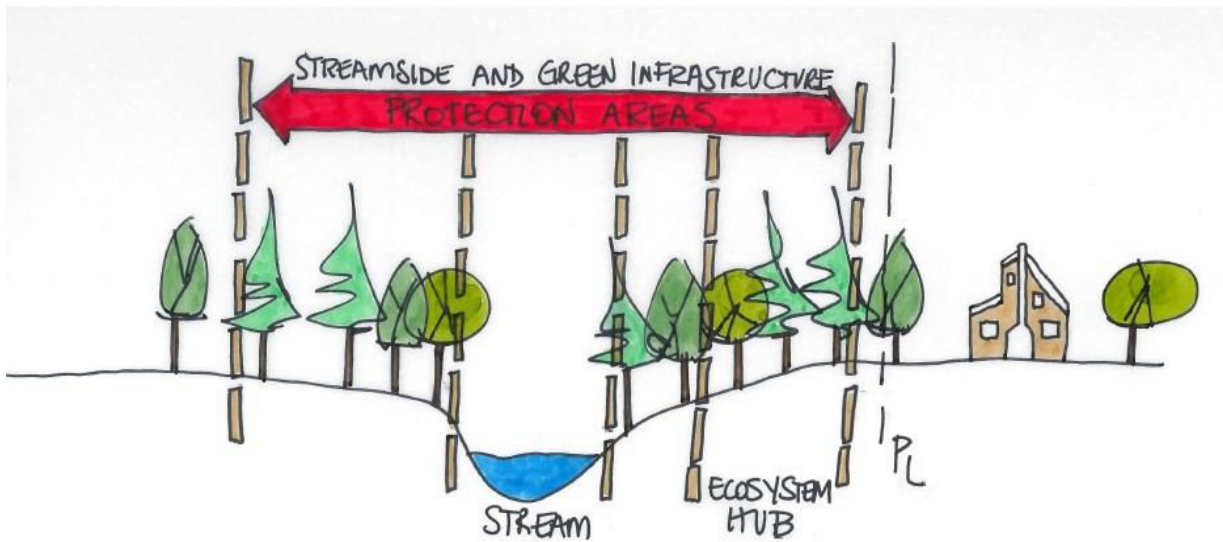
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DP3 SENSITIVE ECOSYSTEMS

C. Development Guidelines (cont.)

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

Streamside	Green Infrastructure
9. Trees and Vegetation: Create continuous vegetated corridors, wherever possible, by linking tree retention areas from within the development to that of adjacent Streamside Protection Areas or Green Infrastructure Protection Areas. (<i>illustr.</i>)	
10. Trees and Vegetation: In addition to the Areas of Protection, retain as much existing native vegetation and as many trees as possible during site development or redevelopment. Ensure tree replacement ratios are consistent with the requirements identified in Surrey’s Tree Protection Bylaw, as amended.	
11. Trees and Vegetation: Protect trees or existing vegetation within the development site and any trees on adjacent City of Surrey property and/or boulevards, before and during construction, in accordance with Surrey’s Tree Protection Bylaw, as amended.	
12. Trees and Vegetation: Landscape the setback areas created between buildings, roadways, parking and driveways and the edge of the Streamside Protection Area or Green Infrastructure Protection Area using native plantings (e.g. trees, shrubs and groundcover) where appropriate. Landscaping should be complementary and supportive of the habitat types and ecosystem values found within the adjacent Streamside Protection Area or Green Infrastructure Protection Area as documented within the Ecosystem Development Plan and/or the Impact and Mitigation Plan.	
13. Habitat: Design sites to mitigate light pollution to reduce impacts on those habitat areas reliant on darkness.	
14. Habitat: Prevent disturbance of animal and bird nesting sites and breeding areas by preserving adequate animal habitat through the retention of appropriate trees and vegetation and by timing construction activity to avoid bird breeding windows.	



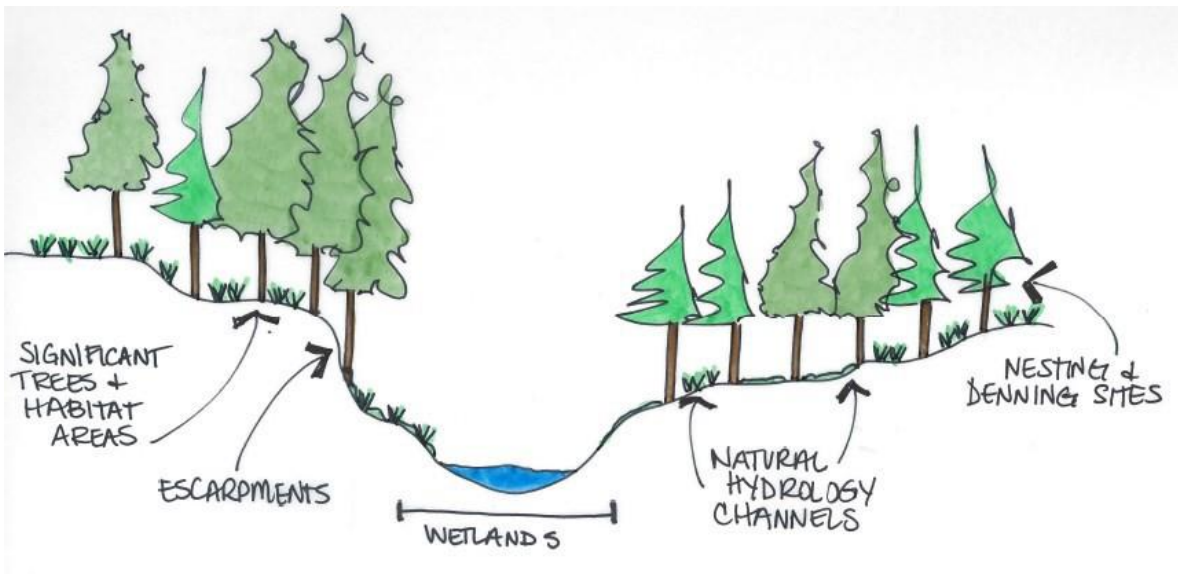
DP3 SENSITIVE ECOSYSTEMS

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C. Development Guidelines (cont.)

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

Streamside	Green Infrastructure
<p>15. Habitat: Retain intact aquatic and terrestrial ecosystems and their connectivity to other Sensitive Ecosystem Areas. Preserve large tracts of wildlife habitat or long, continuous corridors in order to facilitate safe and unencumbered movement of wildlife from one place to another. Maintain natural hydrologic cycles in wetlands, ponds, streams and seepage areas to retain biodiversity. <i>(illustr.)</i></p>	<p>16. Habitat: Reconnect fragmented ecosystems, where practical and necessary, by establishing and restoring corridors and protecting habitats for isolated species or populations, as shown in Surrey’s Biodiversity Conservation Strategy.</p>
<p>17. Habitat: Plan and design development in order to:</p>	
<p>a) Protect endangered, threatened, vulnerable species or Species-at-Risk plant and animal communities. Plan, design and implement land development and subdivisions to protect Sensitive Ecosystem Areas. Prioritize protection for habitats that provide for at-risk species, at-risk ecological communities and keystone species. Minimize impacts on significant, rare or unique vegetation, wildlife and wildlife habitat.</p>	
<p>b) Retain and enhance ecosystem features such as significant trees, hedgerows or windbreaks, with enough undisturbed space around them to protect root systems.</p>	
<p>c) Retain natural landforms such as escarpments, ravines, rock croppings, bluffs, hilltops or cliffs.</p>	
<p>d) Design and implement artificial habitat features, such as nesting boxes or spawning beds, where natural features are not present or practical.</p>	



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C. Development Guidelines (cont.)

For development in the Sensitive Ecosystem Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

Streamside	Green Infrastructure
<p>18. Drainage: At a minimum, manage stormwater so that the quantity, quality and rate of stormwater input into receiving streams does not vary from that which existed pre-development. Install both temporary and permanent sediment/pollutant containment and erosion control measures for any development or redevelopment.</p>	
<p>19. Drainage: Manage site drainage in accordance with the Integrated Stormwater Management Plan (ISMP) relevant to the site location and with Surrey’s Erosion and Sediment Control and Stormwater Drainage Regulation and Charges Bylaws, as amended.</p>	
<p>20. Drainage: Implement measures that will prevent the release of silt or sediment-laden water into streams and Surrey’s drainage network during any activity involving soil disturbance. Manage and mitigate the quality and quantity of stormwater runoff to help protect and enhance aquatic habitat.</p>	
<p>21. Drainage: Incorporate rainwater Best Management Practices into development to ensure the post-development peak flows do not exceed pre-development peak flows by treating, storing, redirecting and slowly releasing rainwater from impervious surfaces back into natural hydrological pathways as much as possible, and by using systems such as green roofs, underground rainwater storage systems, rain gardens and rain barrels. Incorporate on-site stormwater management features into the development as required by the Surrey Stormwater Drainage Regulation and Charges Bylaw, as amended.</p>	
<p>22. Drainage: Construct wetlands and detention ponds to improve the quality of rainwater through bio-filtration and to maximize habitat creation. (<i>illustr.</i>)</p>	



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DP3 SENSITIVE ECOSYSTEMS

D. Submission Requirements—Ecosystem Development Plan

For either a Green Infrastructure Area or a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **ECOSYSTEM DEVELOPMENT PLAN** which details, illustrates and describes the Development Proposal as follows:

i) Consultant Qualifications

Provide qualifications for each separate consultant used per development proposal.

Streamside	Green Infrastructure
<input type="radio"/> 23. ALL registered professionals who will be involved in the development proposal, whether a Biologist, Geoscientist, Engineer, Forester, and/or Agrologist, shall have demonstrated education, expertise, accreditation and knowledge relevant to sensitive environments, ecosystems and/or streamside management.	
<input type="radio"/> 24. ALL Arborists who will be involved in the development proposal shall be registered and certified with the International Society of Arboriculture (ISA).	
<input type="radio"/> 25. Supply a list and written statement, including all documentation, verifying the qualifications of all Qualified Environmental Professionals (QEP) and/or ISA Certified Arborists responsible for preparing report submissions or involved in monitoring site conditions for Sensitive Ecosystems Development Permit applications.	
<input type="radio"/> 26. Where more than one Qualified Environmental Professional is needed, submit a written statement identifying THE PRIMARY QEP for the entire development and acknowledging their role to ensure: all required reports are prepared by qualified professionals and are coordinated both in content and in execution.; and that all relevant Development Permit Guidelines and requirements have been met and/or addressed in the application submission.	

ii) Protection Areas

Use the following documents to determine the Sensitive Ecosystem Area to be protected.

Streamside	Green Infrastructure
<input type="radio"/> 27a. Zoning Bylaw: Part 7a, Streamside Protection of Surrey’s Zoning Bylaw is to be used to determine the Area of Protection required for development adjacent to a stream. (<i>illustr.</i>)	<input type="radio"/> 27b. Biodiversity Conservation Strategy: the Biodiversity Management Areas, Green Infrastructure Network and Appendix J of the <i>Biodiversity Conservation Strategy</i> are to be used to determine the Area of Protection required for development within a Green Infrastructure Area.



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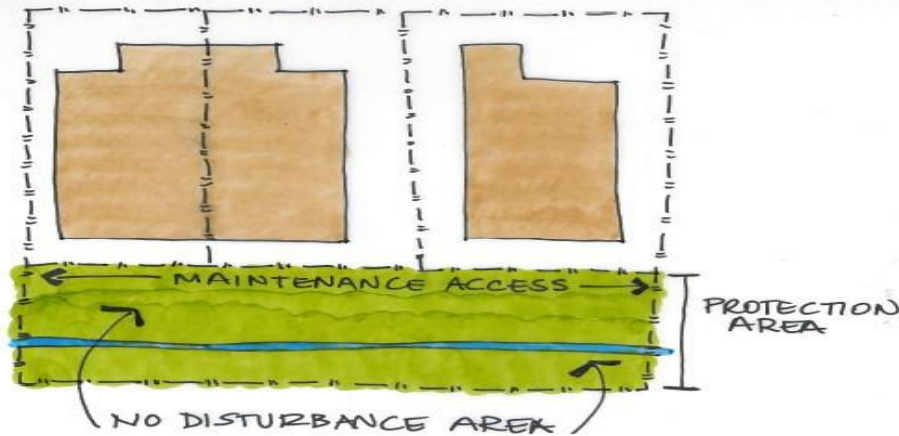
D. Submission Requirements—Ecosystem Development Plan (cont.)

For either a Green Infrastructure Area or a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **ECOSYSTEM DEVELOPMENT PLAN** which details, illustrates and describes the Development Proposal as follows:

iii) Levels of Safeguarding

Indicate which level of safeguarding is proposed to be applied to the development.

Streamside	Green Infrastructure
<p>28. Maximum Safeguarding: conveyance of the Protection Area to the City of Surrey. Where conveyance is chosen, the applicant is not responsible for the additional ecological restoration or on-going maintenance of the Protection Area as detailed and described below under the Minimum Safeguarding option.</p>	
<p>29a. Minimum Safeguarding: registration of a combined Restrictive Covenant/Right-of-Way against the property to ensure safeguarding and maintenance of the Protection Area in perpetuity that shall include:</p> <ol style="list-style-type: none"> 1) identified “no disturbance” and “maintenance access” areas; (<i>illustr.</i>) 2) provisions for post-construction on-going management of the Protection Area for a minimum of five years, including any required rehabilitation, restoration and/or conservation of any areas identified by the project-managing QEP; 3) provisions for yearly monitoring and reporting; and identification of the Principal QEP responsible for providing yearly monitoring reports during the minimum five-year maintenance period; and 4) a compensation plan and cost estimate for all items listed in this section. This will also be used to determine landscape bonding and security requirements for installation and maintenance purposes. 	<p>29b. Minimum Safeguarding: registration of a Restrictive Covenant against the property to ensure safeguarding and maintenance of the Protection Area in perpetuity that shall include:</p> <ol style="list-style-type: none"> 1) identified “no disturbance” areas; (<i>illustr.</i>) 2) provisions for post-construction on-going management of the Protection Area for a minimum of five years, including any required rehabilitation, restoration and/or conservation of any areas identified by the project-managing QEP; 3) provisions for yearly monitoring and reporting; and identification of the Principal QEP responsible for providing yearly monitoring reports during the minimum five-year maintenance period; 4) a compensation plan and cost estimate for all items listed in this section. This will also be used to determine landscape bonding and security requirements for installation and maintenance purposes.



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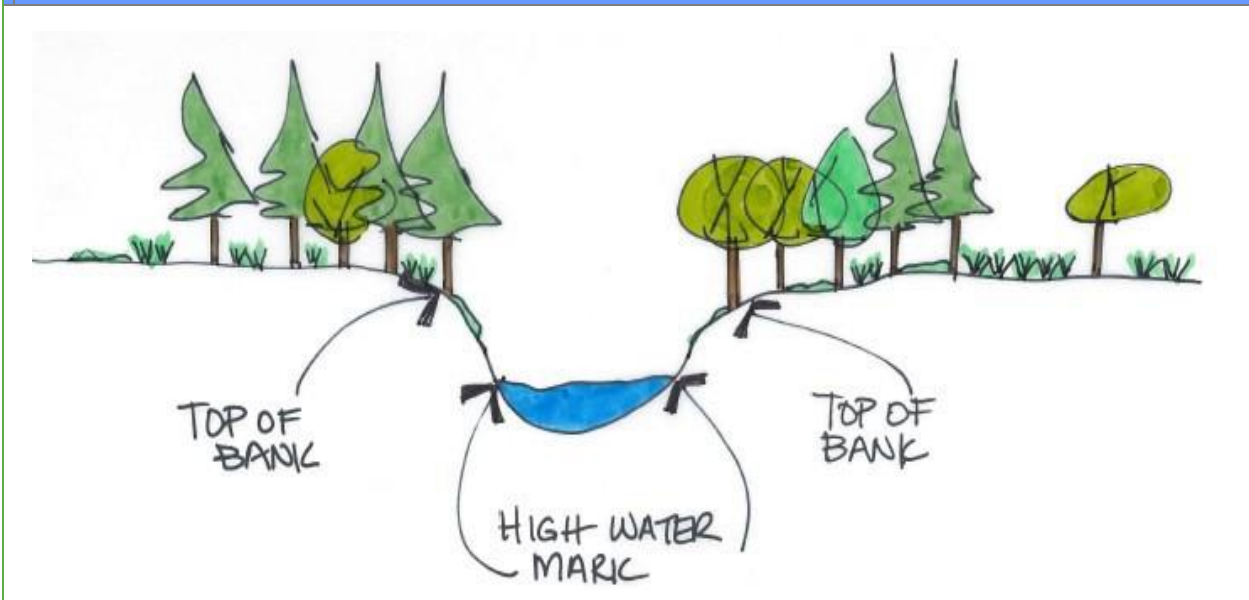
D. Submission Requirements—Ecosystem Development Plan (cont.)

For either a Green Infrastructure Area or a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **ECOSYSTEM DEVELOPMENT PLAN** which details, illustrates and describes the Development Proposal as follows:

iv) Pre-Development Site Conditions

Detail conditions of the development site prior to any development disturbance.

Streamside	Green Infrastructure
30. Building and Construction: Identify all existing on-site buildings, structures and developed areas, including paved and landscaped areas, and any other areas disturbed beyond its original condition.	
31. Soils: Perform a slope analysis and identify existing topographic features including geological and hydrogeological soil conditions, particularly areas of unstable or sensitive soils.	
32. Trees and Vegetation: Identify and detail existing vegetation and trees (including trees defined in the Surrey Tree Bylaw, as amended) and submit in an arborist’s assessment report.	
33. Habitat: Identify Schedule 1, Federally-protected <i>Species-At-Risk Act</i> or Provincial Red- or Blue-Listed plant or animal species and their critical habitats including shrub and ground cover communities and any species, or habitat feature, identified as requiring year-round protection as identified in the Provincial <i>Wildlife Act</i> .	
34. Drainage: Identify the Streamside Protection Area and stream locations, including top of bank (illustr.) and stream classification (as defined in Surrey’s Zoning Bylaw, as amended).	n/a
35. Drainage: Identify existing site drainage conditions in accordance with the Integrated Stormwater Management Plan (ISMP) relevant to the site location.	n/a
36. Drainage: Detail existing site drainage conditions including depth to ground water table, stormwater conveyance, infiltration and storage features, and stormwater channels and overland flow paths.	



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D. Submission Requirements—Ecosystem Development Plan (cont.)

For either a Green Infrastructure Area or a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **ECOSYSTEM DEVELOPMENT PLAN** which details, illustrates and describes the Development Proposal as follows:

v) Proposed Development Conditions and Specifications

Detail the proposed development and post-site conditions.

Streamside	Green Infrastructure
37a. Overall Site: The identified Streamside Protection Area and how it is situated with the development. <input type="radio"/>	37b. Overall Site: The identified Green Infrastructure Protection Area and how it is situated with the development. <input type="radio"/>
n/a	38. Overall Site: Locate development where it is most sensitive to the objectives of the Biodiversity Management Areas, Green Infrastructure Network and corresponding conditions and recommendations required for management as identified in Surrey’s Biodiversity Conservation Strategy. (illustr.) <input type="radio"/>
39. Building and Construction: Detail construction specifications including materials, timing, technologies and techniques proposed as a means to mitigate and reduce the ecological impacts of development on the identified Streamside or Green Infrastructure Protection Area. <input type="radio"/>	
40. Building and Construction: Explain how the proposed development conforms to Surrey’s Drainage Regulation and Charges Bylaw, as amended, as well as policies in Secondary Plans, Integrated Stormwater Management Plans and the Zoning Bylaw, as amended; <input type="radio"/>	
41. Building and Construction: Detail the locations of all proposed buildings, structures and impervious surfaces. <input type="radio"/>	
42. Building and Construction: Detail the timing and scheduling of all proposed development activities. <input type="radio"/>	
43. Soils: Provide site grading plans illustrating the area and extent of soil disturbance including slope grades and any proposed retaining wall heights, locations and materials used. Detail how slope or soil stability will be ensured and how erosion and increased sedimentation risks will be reduced. <input type="radio"/>	



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DP3 SENSITIVE ECOSYSTEMS

D. Submission Requirements—Ecosystem Development Plan (cont.)

For either a Green Infrastructure Area or a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **ECOSYSTEM DEVELOPMENT PLAN** which details, illustrates and describes the Development Proposal as follows:

v) Proposed Development Conditions and Specifications (cont.)

Detail the proposed development and post-site conditions.

Streamside	Green Infrastructure
44. Trees and Vegetation: Identify how existing trees, shrubs and groundcover will be retained and protected including details and specifications on the replanting, restoration and management of vegetated areas and the maintenance of short- and long-term hydraulic regimes. <input type="radio"/>	
45. Trees and Vegetation: Identify individual tree retention and removal as well as areas of structured landscaping, including plant species, size and location. <input type="radio"/>	
46. Trees and Vegetation: Provide details as to how the Streamside or Green Infrastructure Area management of protection objectives will be met and monitored following the official completion of all construction activity. Where restoration work IS NOT required, maintenance and monitoring shall be for a minimum of ONE year; and where restoration work IS required, maintenance and monitoring shall be for a minimum of FIVE years. <input type="radio"/>	
47. Trees and Vegetation: Provide a restoration, maintenance and cost estimate plan consistent with the development requirements identified in the Ecosystem Management Plan and/or Impact Mitigation Plan to be used to determine landscape bonding and security requirements for installation, monitoring and maintenance purposes. <input type="radio"/>	
n/a	48. Habitat: Articulate how the proposed development meets the Objectives and Recommendations of the Biodiversity Management Areas and Green Infrastructure Network of the <i>Biodiversity Conservation Strategy</i> . <input type="radio"/>
n/a	49. Habitat: Detail how wildlife habitat will be protected and enhanced while taking into consideration wildlife movement and connectivity to adjacent sites. Focus on Schedule 1, Federally-protected <i>Species-At-Risk Act</i> or Provincial Red- or Blue-Listed plant or animal species and their critical habitats and how they may be affected by the proposed development, and indicate how proposed Best Management Practices may be used for the protection and preservation of that habitat. <input type="radio"/>
50. Drainage: Identify post-development drainage site conditions in accordance with the Integrated Stormwater Management Plan (ISMP) relevant to the site location. <input type="radio"/>	
51. Drainage: Detail how flooding risk and water quality degradation will be mitigated including specific measures that will be taken to prevent channel erosion and prevent the fouling of streams, wetlands or drainage conveyance corridors. <input type="radio"/>	

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DP3 SENSITIVE ECOSYSTEMS

E. Submission Requirements—Impact Mitigation Plan

For development adjacent to a Streamside Area, using plans, elevations and/or profiles at an appropriate scale, **SUBMIT** an **IMPACT MITIGATION PLAN** detailing, illustrating and describing the following:

52. An **IMPACT MITIGATION PLAN** is required to determine the appropriateness of requests to reduce the Streamside Protection Area (through the use of a Development Variance Permit) as defined in Surrey’s Zoning Bylaw, Part 7a. The **IMPACT MITIGATION PLAN**, where relevant to the site (determined by City of Surrey), shall include the following:

- Overall Site:** A detailed explanation why a variance is being sought and why the required Streamside Protection Area is not being met.
- Overall Site:** A detailed Riparian assessment report, following the Provincial methods specified in the Riparian Areas Regulation, B.C. Reg. 376/2004, identifying the regulatory Streamside Protection and Enhancement Area and associated measures.
- Overall Site:** An assessment report indicating any expected changes and modifications to streams and aquatic and riparian areas, including any expected water quality reductions, water quantity changes, or fish and wildlife habitat degradation, with a particular focus on salmonid habitat and healthy tree retention, as a result of the proposed Streamside Setback Area variance.
- Overall Site:** A restoration plan and cost estimate for the items submitted with the Impact Mitigation Plan. This restoration plan shall also be used to determine landscaping bonding and security requirements for installation, monitoring and maintenance purposes.
- Building and Construction:** Details demonstrating how the City of Surrey can reasonably access the stream to maintain drainage conveyance including illustrating the location of a required minimum 5 m wide access corridor that is geotechnically-stable and located outside of the Streamside Protection and Enhancement Area, as defined in Riparian Areas Regulation, B.C. Reg. 376/2004, as amended.
- Building and Construction:** Details on how public and maintenance access will be accommodated where there is a park trail proposed within the proposed adjusted Streamside Setback Area.
- Soils:** Provide a geotechnical report that assesses the impacts of the adjustments and recommends how to protect stream bank stability. The report will provide detailed information on stream hydraulics, erosion, sedimentation, and top of bank conditions (e.g. top loading or slope undercutting effects). The report will also include a seismic assessment addressing impacts for both structural and human usage.
- Trees and Vegetation:** A vegetation accounting report including the effects of any biomass and habitat removal, or any other disturbances as a result of the proposed Streamside Setback Area variance.
- Habitat:** A riparian restoration plan specifying the following: invasive species management; abatement of hazard trees; appropriate native species proposed to be used in any restoration work; details on any removal of anthropomorphic habitat debris; and how stream complexing and drainage conveyance will be undertaken (to the satisfaction of the City of Surrey).
- Drainage:** An assessment report on any expected flooding or increase in flooding or impacts, positive or negative, affecting groundwater as a result of the proposed Streamside Setback Area variance.
- Drainage:** An assessment report of the stream and how the proposed adjustment to the Streamside Setback Area will not create a vulnerability from beaver activities for the development site.
- Drainage:** Detail and identify post-development drainage site conditions in accordance with the Integrated Stormwater Management Plan (ISMP) relevant to the site location.

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DEVELOPMENT PERMIT GUIDELINES: FARMING PROTECTION

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The Farming Protection Development Permit Guidelines are intended to protect Agriculture and Farming operations from the effects of adjacent urban development by using Vegetation Buffers and Building and Site Design specifications.

Vegetation Buffers: represent the area adjacent to the Agricultural Land Reserve (ALR) used to create a physical and visual separation between agricultural land and urban development.

Building and Site Design: building and layout specifications used to supplement the vegetation buffers to further reduce conflicts and negative impacts of urban development on agricultural land.

In order to provide further clarification of the guidelines within this section, pictures or graphic images are provided; and where a particular guideline is graphically illustrated, the notation “(illustr.)” appears.



DP4 Farm Protection Guidelines User Guide:

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The following User Guide provides the main components required for each Farm Protection Development Permit application:

1. REVIEW and CONSIDER
 - A. Agricultural-Urban Conflict Requirements
 - B. Development Guidelines

2. SUBMIT COMPLETE APPLICATION
 - C. Determine Setbacks and Buffers
 - Single Family Uses
 - Multiple Family Uses
 - Non-Residential Uses

DP4 FARMING PROTECTION

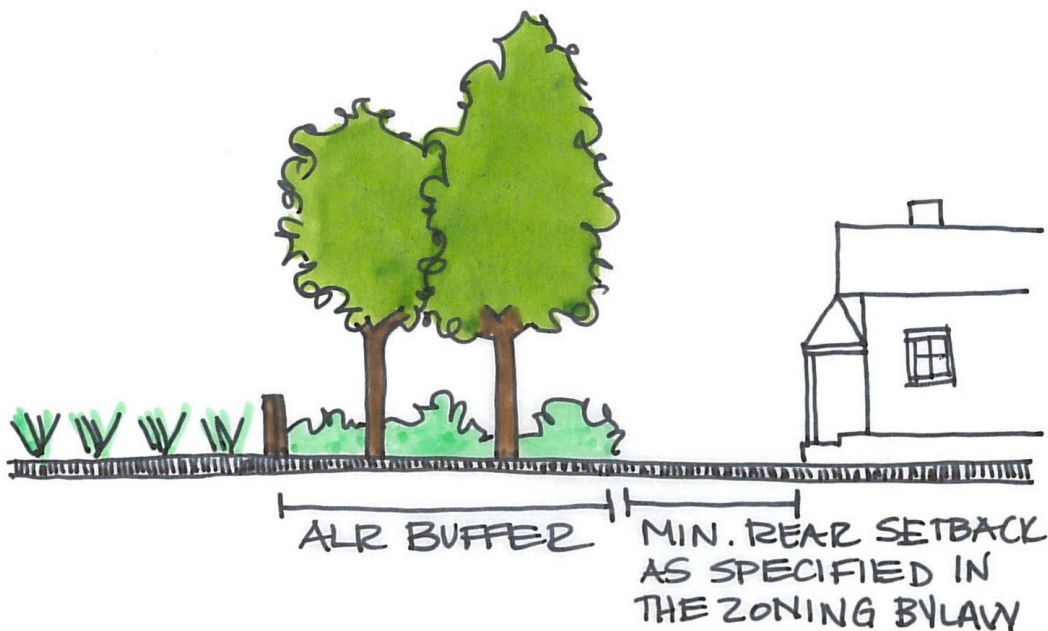
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A. Agriculture-Urban Conflict Requirements

For any development adjacent to agricultural land falling within the Farm Protection Development Permit Area, **REVIEW** and **CONSIDER** the following Agriculture-Urban Conflict Requirements:

Farm Protection

1. As part of a development process, any property that falls within 50 m of the Agricultural Land Reserve (ALR) boundary shall register a Restrictive Covenant against that property informing any future purchasers that farm operations take place in the area and that normal farm practices produce noise, odour and dust that may impact adjacent residential properties. This Restrictive Covenant shall reference a farm operation’s acceptable and required farm practices, as identified and in accordance with the Provincial *Right to Farm Act*.
2. Building setback minimums referred to and illustrated in this section are required as a way to reduce Agricultural-Urban conflicts. Where the full building setback minimum cannot be met due to hardship reasons or to protect natural features, the minimum setback requirements between the building face and the vegetated agricultural buffer shall be determined by the minimum yard setback for each applicable zone, in accordance with Surrey’s Zoning By-law, as amended. (*illustr.*)
3. Mitigate the impact of urban development on farming practices within the ALR by utilizing the following site and building design elements:
 - ⇒ Sensitive place backyards and patios away from the ALR
 - ⇒ Install double paned windows or sound-proofed windows
 - ⇒ Cluster buildings to maximize buffering between residences and the ALR



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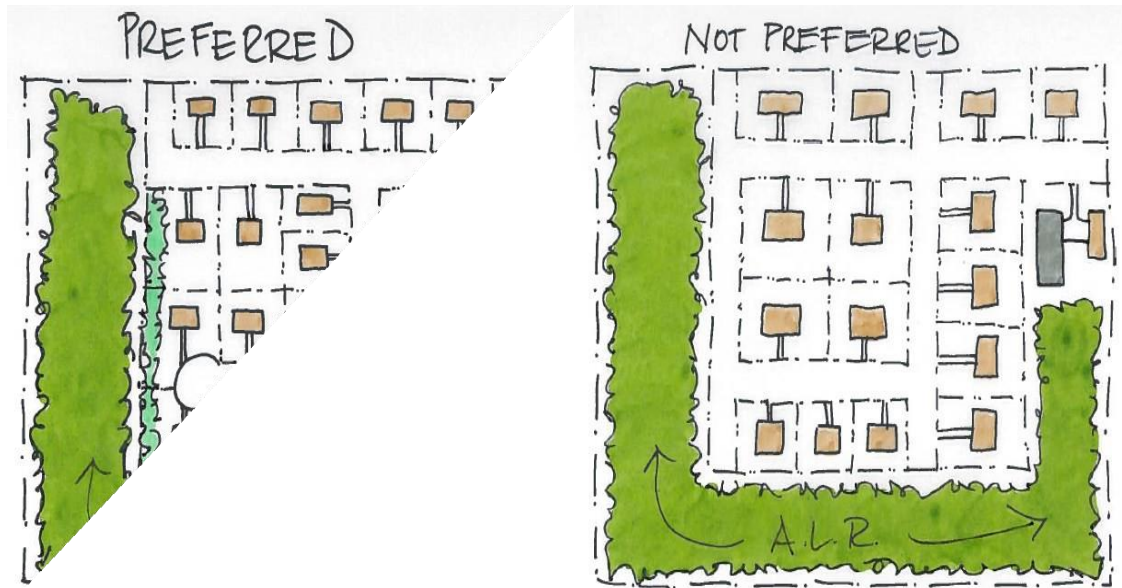
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A. Agriculture-Urban Conflict Requirements (cont.)

For any development adjacent to agricultural land falling within the Farm Protection Development Permit Area, **REVIEW** and **CONSIDER** the following Agriculture-Urban Conflict Requirements:

Farm Protection

4. Encroachment of urban uses into farming areas shall be reduced by avoiding the dead-ending of road ends adjacent or into the ALR, except where that road may be needed for access by farm vehicles. *(illustr.)*
5. Where appropriate, use roads, topographic features, watercourses, ditching, no-build areas, vegetated and fenced barriers as buffers to preserve large farm units and areas from the encroachment of non-agricultural uses. *(illustr.)*
6. Retain and enhance natural buffer features (e.g. riparian areas, ravines, woodlots, wetlands and natural vegetated areas) along the urban side of the ALR boundary. Where natural buffers are not feasible, create buffers and/or compatible uses (e.g. roads, railways, utility Rights-of-Way, berms, open spaces and rain-water management facilities and features) on the urban side of the ALR boundary. *(illustr.)*



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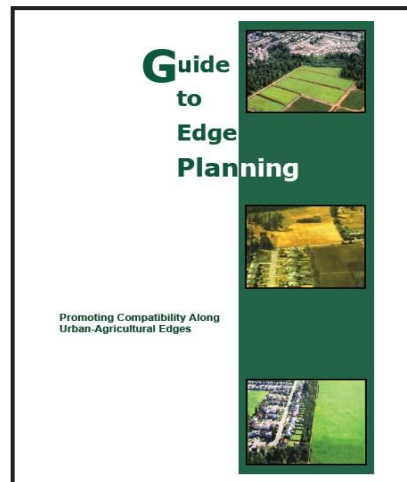
B. Development Guidelines

For any development adjacent to agricultural land falling within the Farm Protection Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines as follows:

General Buffer Design

7. Natural landscape buffer features shall be retained and enhanced along the urban side of the Agricultural Land Reserve Boundary. Buffer features include mature trees, riparian areas, ravines, woodlots, wetlands and natural vegetation. Where natural buffers do not exist, create vegetated buffers and/or compatible uses (e.g. roads, railways, utility Rights-of Ways, berms, fences, open spaces and rain-water management facilities and features) on the urban side of the ALR boundary.

8. Unless otherwise indicated, specifications for “appropriate” planting and species details and fencing should be referenced from the Ministry of Agriculture *Guide to Edge Planning*, as amended, and from the Agricultural Land Commission’s “Schedule D”, *Landscaped Buffer Specifications*, as amended.



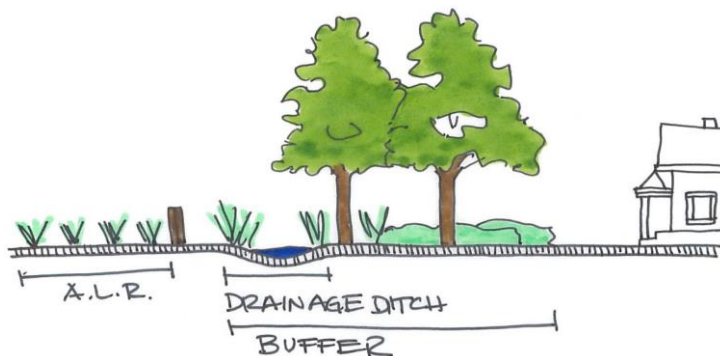
9. Ensure firm edges exist between the agricultural buffer and residential landscaped areas by planting infill trees and shrubs and plants to create a fuller buffer and by using other elements such as rock boulders and trespass-inhibiting shrubs in conjunction with an “appropriate” fence.

10. Include a mix of “appropriate” deciduous and coniferous trees a minimum of 2m—4m apart.

11. Preserve all existing, healthy and mature trees located within the agricultural buffer area.

12. Design buffers with “appropriate” native and natural agriculturally-sensitive plant species. Consider infill planting or planting older and larger trees to create a fuller vegetated buffer.

13. If planned, include rainwater management or water detention facilities in buffer areas (*illustr.*). Construct detention ponds in conjunction with local area Integrated Stormwater Management Plans (ISMP) to ensure drainage from the development does not affect the adjacent agricultural land.



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B. Development Guidelines (cont.)

For any development adjacent to agricultural land falling within the Farm Protection Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines:

General Buffer Design (cont.)	
14.	During development and construction phases, ensure any required fill deposition is handled sensitively with respect to the buffer to ensure natural soil regimes, on and off site, are maintained.
15.	Ensure public safety and security is taken into consideration in the design and development of trails and sidewalks near vegetated buffer areas.
16.	Consider the use of rock boulders and firm edges between the buffer and any developed area, particularly with single family developments.
Buffer Installation and Maintenance	
17.	Landscaped buffers and any related tree fencing (fencing used to protect existing trees from development) will be required to be installed prior to the issuance of Building Permits for new subdivisions or developments.
18.	All landscaping shall be maintained in good condition, consistent with an approved Landscape Plan. Landscaping should be consistent with the guides and specifications listed under <i>General Buffer Design</i> of this Development Permit Section.
19.	Use Statutory Easements combined with a Restrictive Covenants to ensure buffers are established, maintained and accessible by maintenance crews.
20.	For private property, either through the use of Letters of Credit or Cash Securities, deposits equal to one hundred percent (100%) + fifteen percent (15%) contingency of the total landscaping costs will be retained during all development stages. For public parkland, a Maintenance Bond is required to be provided by the developer. Partial refunds of Security Deposits or Maintenance Bonds may be allowed after a minimum of five years, provided that landscaped buffers have been substantially and satisfactorily completed. Satisfactory completion includes illustrating that trees and shrubs are established to a free-growing stage.
21.	Should fencing not be required for an existing naturally-vegetated ALR edge at the time of development, security deposits in the amount of one hundred percent (100%) + fifteen percent (15%) contingency of the total fencing costs will be required to secure future fence installations.

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B. Development Guidelines (cont.)

For any development adjacent to agricultural land falling within the Farm Protection Development Permit Area, **REVIEW** and **CONSIDER** the following Development Guidelines:

Parks and Trail Design

- 22. Where appropriate, and located where it will likely not create conflicts with farming, and where determined by and approved the City of Surrey, include passive recreation with buffers that will be conveyed as Park-land. Buffer depths shall be increased from any minimums previously stated to a minimum of 20 m wide.
- 23. Where trails will be added into agricultural buffers and maintained by the City of Surrey, the following requirements apply:
 - ⇒ Rights-of-Way protecting the trail shall be a minimum of 4 m wide and be located away from the agricultural edge
 - ⇒ Trails shall be passive in nature and not developed as multi-use pathways
 - ⇒ Trails shall be placed on the inner-most half of the buffer a minimum of 10 m away from the ALR edge
 - ⇒ Fencing shall be provided along the property line at 1.8 m high and constructed of black chain link
 - ⇒ Fencing shall also be provided between the agricultural buffer and the residential development at 1.2 m high and be constructed of black chain link.

Development and Building Design

- 24. Ensure housing units constructed adjacent to agricultural land incorporate additional building upgrades and/or design features in order to minimize noise conflicts from agricultural operations. These building upgrades or design features may include the following:
 - ⇒ Orienting bedrooms and outdoor areas away from agricultural land
 - ⇒ Providing mechanical ventilation to allow for the choice of keeping windows closed
 - ⇒ Enclosing balconies or using sound-absorption materials and other sound barriers
 - ⇒ Using sound-deadening construction materials (e.g. concrete, acoustically-rated window glazing or glass block walls)
 - ⇒ Locating areas not affected by noise (e.g. stairwells and single-loaded corridors) between the agricultural land and other areas of the residential unit
 - ⇒ Maximizing the setback between agricultural land and buildings or structures
 - ⇒ Reducing the number of doors, windows and outdoor patios facing directly onto agricultural land.

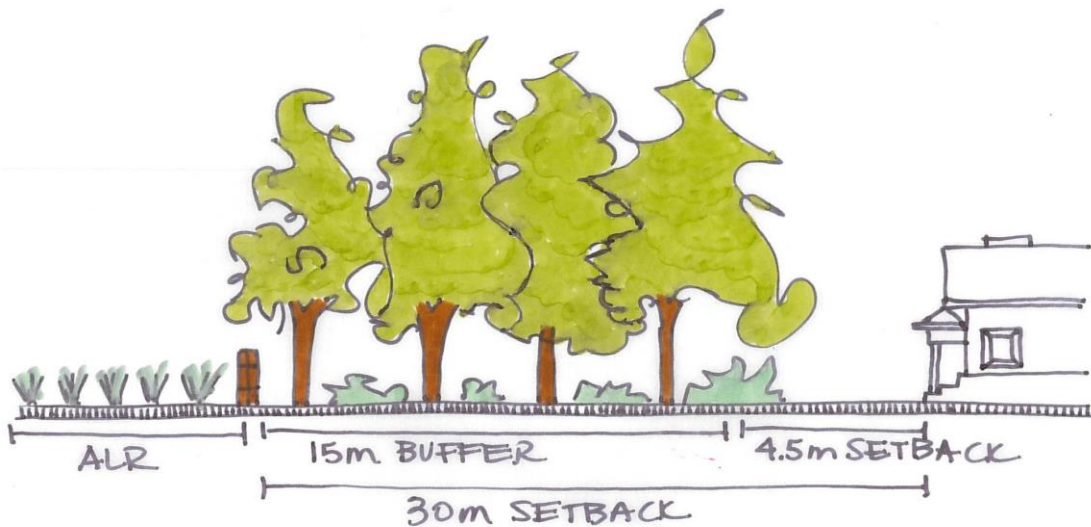
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C. Setbacks and Buffers

For development in the Farm Protection Development Permit Area, **SUBMIT** a **COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

SINGLE FAMILY USES Scenario: ALR Abutting a Yard	
<p>25. For properties with a Yard abutting the ALR (<i>illustr.</i>):</p> <ul style="list-style-type: none"> ⇒ Minimum building setback from the Agricultural Land Reserve boundary is 30 m ⇒ Increase zoning setbacks, where possible, to accommodate appropriate and effective rear yard space for buffers ⇒ Provide a minimum of 4.5 m of rear yard space between the landscaped buffer and the rear face of a single family dwelling (<i>illustr.</i>) ⇒ Minimum vegetated buffer width is 15 m ⇒ Vegetated landscape buffer remains under private ownership. 	<p>The diagram shows a vertical dashed line representing the ALR boundary. To its right is a shaded green area representing a 15m vegetated buffer. Further right is a white area representing a 4.5m setback. A building footprint is shown to the right of this setback. A horizontal dimension line at the bottom indicates a total 30m setback from the ALR boundary to the building.</p>



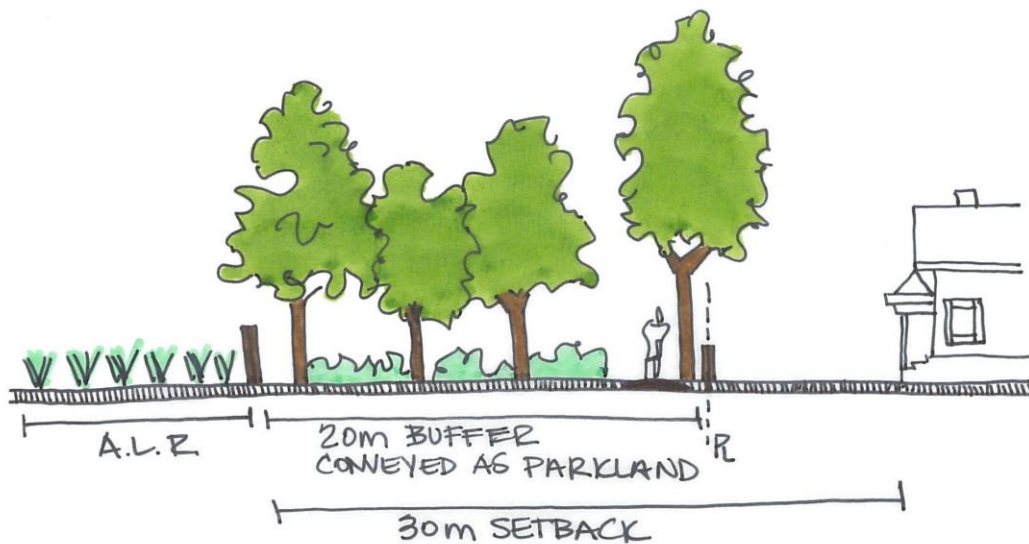
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C. Setbacks and Buffers (cont.)

For development in the Farm Protection Development Permit Area, **SUBMIT a COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

SINGLE FAMILY USES Scenario: ALR Abutting a Yard as Parkland	
26. For properties with a Yard as Parkland abutting the ALR (<i>illustr.</i>):	<p>MIN. 20m CONVEYED PARKLAND BUFFER 30m SETBACK</p>
⇒ Minimum building setback from the Agricultural Land Reserve boundary is 30 m	
⇒ Increase zoning setbacks, where possible, to accommodate appropriate and effective rear yard space for buffers	
⇒ Minimum vegetated buffer width is 20 m , particularly where there is already a natural area such as a riparian stream, ravine, woodlot or wetland	
⇒ Vegetated landscaped buffer is conveyed as public parkland.	



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C. Setbacks and Buffers

For development in the Farm Protection Development Permit Area, **SUBMIT a COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

SINGLE FAMILY USES	
Scenario: ALR Abutting Yard Under Strata Control	
<p>27. For properties with a Yard under Strata Control abutting the ALR (<i>illustr.</i>):</p>	
⇒ Minimum building setback from the Agricultural Land Reserve boundary is 30 m	
⇒ Increase zoning setbacks, where possible, to accommodate appropriate and effective rear yard space for buffers	
⇒ Minimum vegetated buffer width is 20 m	
⇒ Vegetated buffer maintained by a Strata Corporation.	



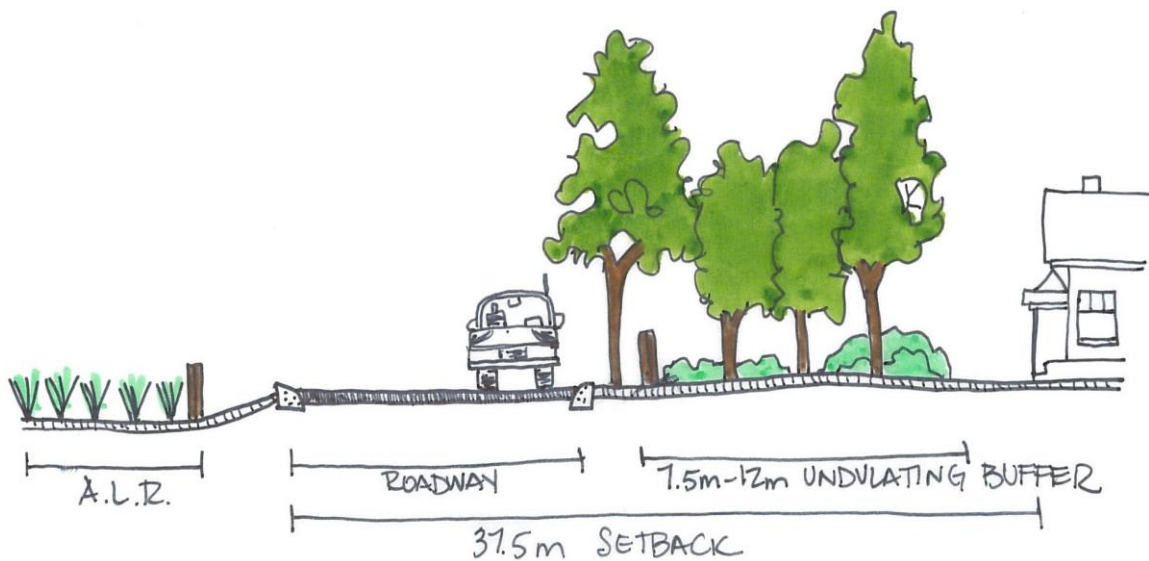
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C. Setbacks and Buffers (cont.)

For development in the Farm Protection Development Permit Area, **SUBMIT a COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

SINGLE FAMILY USES Scenario: ALR Abutting a Road	
<p>28. For properties with a Road abutting the ALR (<i>illustr.</i>):</p>	
⇒ Minimum building setback from the Agricultural Land Reserve boundary is 37.5 m	
⇒ Increase zoning setbacks, where possible, to accommodate appropriate and effective rear yard space for buffers	
<p>⇒ Undulating buffer width from a minimum of 7.5 m to a maximum of 12 m, with an overall net width of 10 m is permitted</p> <p>OR</p> <p>⇒ Straight buffer with a minimum width of 10 m</p>	
⇒ Vegetated buffer remains under private ownership.	



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C. Setbacks and Buffers

For development in the Farm Protection Development Permit Area, **SUBMIT a COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

MULTIPLE FAMILY USES Scenario: ALR Abutting a Lot

29. For properties with Multiple Family uses abutting the ALR (*illustr.*):
- ⇒ Minimum building setback from the Agricultural Land Reserve boundary is **30m**
 - ⇒ Minimum building setback, as identified in each specific zone, shall be increased where possible, to accommodate appropriate and effective rear yard space for vegetated buffers
 - ⇒ Provide a **minimum of 3.5 m of rear yard space** between multiple family dwellings and landscaped buffers



- ⇒ Minimum vegetated buffer width is **24 m**
- ⇒ Minimum vegetated buffer width shall be increased proportionally (*illustr.*) with any increase to density.
- ⇒ Buffer widths increased to accommodate additional densities shall also be developed in conjunction with parkland or passive open space
- ⇒ A community garden for the Strata's use, may be included as part of the vegetated buffer area
- ⇒ Vegetated buffer maintained by a Strata Corporation.



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C. Setbacks and Buffers (cont.)

For development in the Farm Protection Development Permit Area, **SUBMIT a COMPLETE APPLICATION** using site plans, building and street elevations, that detail the setbacks and buffers required for development adjacent to the Agricultural Land Reserve, as follows:

NON-RESIDENTIAL USES Scenario: ALR Abutting a Lot	
30. For properties with Non-Residential uses abutting the ALR (<i>illustr.</i>):	
⇒ Minimum building setback from the Agricultural Land Reserve boundary is 30 m	
⇒ Minimum vegetated buffer width is 15 m	
⇒ Vegetated buffer remains under private ownership.	

